

Indiana Department of Transportation

County Jay Route Blaine Pike Des. No. 1600965

**FHWA-Indiana Environmental Document
CATEGORICAL EXCLUSION / ENVIRONMENTAL ASSESSMENT FORM
GENERAL PROJECT INFORMATION**

Road No./County:	Blaine Pike, Portland, Jay County, Indiana
Designation Number:	1600965
Project Description/Termini:	Blaine Pike Pedestrian System Improvements, on Blaine Pike from CR 150 South, northeast to approximately 131feet south of the Norfolk Southern Railroad

After completing this form, I conclude that this project qualifies for the following type of Categorical Exclusion (FHWA must review/approve if Level 4 CE):

X	Categorical Exclusion, Level 2 – The proposed action meets the criteria for Categorical Exclusion Manual Level 2 - table 1, CE Level Thresholds. Required Signatories: ESM (Environmental Scoping Manager)
	Categorical Exclusion, Level 3 – The proposed action meets the criteria for Categorical Exclusion Manual Level 3 - table 1, CE Level Thresholds. Required Signatories: ESM, ES (Environmental Services Division)
	Categorical Exclusion, Level 4 – The proposed action meets the criteria for Categorical Exclusion Manual Level 4 - table 1, CE Level Thresholds. Required Signatories: ESM, ES, FHWA
	Environmental Assessment (EA) – EAs require a separate FONSI. Additional research and documentation is necessary to determine the effects on the environment. Required Signatories: ES, FHWA

Note: For documents prepared by or for Environmental Services Division, it is not necessary for the ESM of the district in which the project is located to release for public involvement or sign for approval.

Approval _____
ESM Signature Date ES Signature Date

FHWA Signature Date

Release for Public Involvement

N/A

ESM Initials Date

 
ES Initials Date

Certification of Public Involvement _____
Office of Public Involvement Date

Note: Do not approve until after Section 106 public involvement and all other environmental requirements have been satisfied.

INDOT ES/District Env.
Reviewer Signature: _____ Date: _____

Name and Organization of CE/EA Preparer: Brian C. Shaw – Beam, Longest, and Neff, LLC

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Part I - PUBLIC INVOLVEMENT

Every Federal action requires some level of public involvement, providing for early and continuous opportunities throughout the project development process. **The level of public involvement should be commensurate with the proposed action.**

Does the project have a historic bridge processed under the Historic Bridges PA*?	Yes <input type="checkbox"/>	No <input checked="" type="checkbox"/>
If No, then:		
Opportunity for a Public Hearing Required?	<input checked="" type="checkbox"/>	<input type="checkbox"/>

*A public hearing is required for all historic bridges processed under the Historic Bridges Programmatic Agreement between INDOT, FHWA, SHPO, and the ACHP.

Discuss what public involvement activities (legal notices, letters to affected property owners and residents (i.e. notice of entry), meetings, special purpose meetings, newspaper articles, etc.) have occurred for this project.

Remarks:

Notice of Entry

Notice of entry letters were mailed to potentially affected property owners near the project area on December 20, 2017 notifying them about the project and that individuals responsible for land surveying and field activities may be seen in the area. A sample copy of the notice of entry letter is included in Appendix G, page G-1.

Section 106

To meet the public involvement requirements of Section 106, FHWA's finding of "No Historic Properties Effected" was published in the Portland Commercial Review on March 20, 2020 offering the public an opportunity to submit comment pursuant to 36 CFR 800.2(d), 800.3(e), and 800.6(a)(4). The public comment period closed on April 18, 2020. No comments from the public or consulting parties were received by the established deadline. The copy of the public notice and the affidavit of the publication appear in Appendix D, page D-34.

Project Does Meet

The project will meet the minimum requirements described in the current *Indiana Department of Transportation (INDOT) Public Involvement Manual* which requires the project sponsor to offer the public an opportunity to submit comment and/or request a public hearing. Therefore, a legal notice will appear in a local publication contingent upon the release of this document for public involvement. This document will be revised after the public involvement requirements are fulfilled.

Public Controversy on Environmental Grounds

Will the project involve substantial controversy concerning community and/or natural resource impacts? Yes ☐ No ☒

Remarks:

No Controversy

At this time there is no substantial public controversy concerning impacts to the community or to natural resources.

Part II - General Project Identification, Description, and Design Information

Sponsor of the Project: City of Portland INDOT District: Greenfield
Local Name of the Facility: Blaine Pike

Funding Source (mark all that apply): Federal ☒ State ☐ Local ☒ Other* ☐

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PURPOSE AND NEED:

Describe the transportation problem that the project will address. The solution to the traffic problem should NOT be discussed in this section. (Refer to the CE Manual, Section IV.B.2. Purpose and Need)

Need

The existing sidewalks along Blaine Pike are not continuous between neighborhoods, and the curb ramps are not in compliance with current Public Right-of-Way Accessibility Guidelines (PROWAG) and the Americans with Disabilities Act (ADA) guidelines. The Portland Memorial Park, the Golden Age Village Adult Community, and the Green Park Cemetery are all located within the project limits and generate increased traffic along the corridor. Where sidewalks are not present, pedestrians and non-motorized vehicles use the roadway instead, causing conflicts with motorized traffic. In addition, the existing Blaine Pike roadway pavement surface exhibits major cracking.

Purpose

The purpose of this project is to separate pedestrians and non-motorized vehicles from the roadway utilized by motorized vehicles and improve the existing roadway surface along Blaine Pike. A goal of the project is to provide increased pedestrian, skater, jogger, and other non-motorized users access between neighborhoods and destinations while reducing potential conflicts with motorized traffic along the Blaine Pike corridor. Another goal is to improve the existing deteriorated condition of the roadway surface and extend the life of the pavement structure for approximately nine years.

PROJECT DESCRIPTION (PREFERRED ALTERNATIVE):

County: Jay Municipality: City of Portland

Limits of Proposed Work: CR 150 S to 175 feet south of the Norfolk Southern Railroad on Blaine Pike

Total Work Length: 1.203 Mile(s) Total Work Area: 6.1939 Acre(s)

Is an Interchange Modification Study / Interchange Justification Study (IMS/IJS) required?

If yes, when did the FHWA grant a conditional approval for this project?

Yes¹

No

Date:

¹If an IMS or IJS is required; a copy of the approved CE/EA document must be submitted to the FHWA with a request for final approval of the IMS/IJS.

In the remarks box below, describe existing conditions, provide in detail the scope of work for the project, including the preferred alternative. Include a discussion of logical termini. Discuss any major issues for the project and how the project will improve safety or roadway deficiencies if these are issues.

Location

This project is on the southwest side of Portland in Wayne Township, Jay County, Indiana. Blaine Pike is on a tangent horizontal alignment and traverses southwest to northeast. Termini for the referenced project on Blaine Pike are County Road (CR) 150 South on the south side of the intersection to approximately 131 feet south of the Norfolk Southern Railroad crossing of Blaine Pike on the north. The total project length is approximately 1.2 miles (6,500 feet). Specifically, the project crosses Sections 20, 29, and 30, Township 23 North, Range 14 East as shown on the Portland, Indiana 7.5 USGS quadrangle map (Appendix B, page B-2).

Existing Conditions

Blaine Pike is a two-lane asphalt roadway that is approximately 20-21 feet wide. The functional classification of Blaine Pike is a Minor Arterial. The roadway serves as one of the primary routes for access into the City of Portland from rural Jay County. Blaine Pike provides two travel lanes, one in each direction; lane widths are 10-10.5 feet. There are no useable shoulders and no pavement markings. A curbed parking lane affords on-street parking from 7th Street to Western Avenue on the east side of Blaine Pike; a second curbed parking lane exists on the west side of Blaine Pike at Sesame Street. Although on a diagonal, the horizontal alignment of the roadway is tangent, and the vertical alignment is at grade. The posted speed limit is 30 mph. Overhead power lines run parallel along the east side of Blaine Pike, with a secondary set of lines running along the west side of Western Avenue to the railroad tracks. The roadway also provides access to

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various special land uses including the Green Park Cemetery, Portland Memorial Park, Golden Age Adult Community, and the Jay County Humane Society.

Drainage along Blaine Pike is directed through a series of open ditches and outfalls into well-defined drainage ditches that travel east and west under the roadway. An existing corrugated metal pipe located north of 3rd Street assists in directing surface run-off away from the roadway. In several areas along the roadway, drainage appears to be captured and transported by yard inlets. It appears all surface run-off is outlet into the Salamonie River, which is located approximately one-half mile to the east and parallel to Blaine Pike.

The Norfolk Southern Railroad line crosses Blaine Pike, north of the northern project terminus. The rail crossing at Blaine Pike is in fair to good condition and has advanced signing and cross bucks with mounted flashing lights. However, there are no stop bars or pavement markings on Blaine Pike. The existing Norfolk Southern Railroad line is not active. No work will be undertaken on the rail crossing as the project will terminate approximately 131 feet south of the rail crossing.

Preferred Alternative

The project proposes the milling and resurfacing of Blaine Pike along the entire project corridor from CR 150 South to 131 feet south of the Norfolk Southern Railroad and construction of a 10-foot wide, paved shared-use path on the east side of the roadway from Bittersweet Lane, approximately 0.20 mile north of SR 150 south, to just north of West 3rd Street. The paved shared-use path will cross side roads and driveways in which the project proposes to construct crosswalks and curb ramps that meet Americans with Disabilities Act (ADA) compliance. As the paved shared-use path approaches Western Avenue, the path will turn slightly to the east and cross over Western Avenue before turning back northeast along the east side of Blaine Pike (see plan and profile sheet in Appendix B, page B-26). A mid-block crossing with solar-powered flashing beacons will be constructed just north of the intersection of Blaine Pike and Western Avenue adjacent to Memorial Park. Drainage swales will be constructed on the east side of Blaine Pike, between the roadway and the shared-use path. The existing corrugated metal pipe (CMP) north of Third Street will be extended to accommodate the new shared-use path. The location of the shared-use path is on the east side of Blaine Pike as there are existing sidewalks in place between 7th Street and Western Avenue. In addition, there is potential for more impacts along the west side of the roadway. The available area adjacent to the roadway is limited and the Green Park Cemetery, the Golden Age Village Adult Community residential area and the Portland Memorial Park are all located along the west side of Blaine Pike between CR 150 South and 7th Street.

The project is approximately 1.2 miles long. Land use surrounding the project area is primarily residential with some agricultural land use at the southern end of the project and some commercial land use at the northern end of the project. The Green Park Cemetery is located adjacent to Blaine Pike on the west side in the southwest quadrant of the intersection with Seventh Street. No impacts to the cemetery will occur. The project lies entirely within the corporation limits of the City of Portland with the downtown area located to the north. Construction is anticipated to begin in 2022.

Additional permanent right-of-way will be required for the construction of the project. It is estimated that approximately 2.59 acres of permanent and 0.003 acres of temporary right-of-way will be necessary. Excavation up to a depth of 2- feet will be necessary for construction of the multiuse path along the corridor. The roadway work includes milling and removal of the existing surface for approximately 1½ -2 inches in depth and replaced with new pavement along the entire corridor. Excavation will occur within 50 feet of the centerline of the roadway. Blaine Pike will remain open for the duration of the construction period. The pavement milling and overlay will be performed with the use of flaggers and moving construction site. At least one lane of traffic will always be maintained for access through the construction zone. Closure of the road will not be necessary. Pedestrian and other non-motorized traffic will be enhanced by the construction, providing positive separation between the roadway and those using the shared use path.

The project includes logical termini starting at the intersection of CR 150 South and Blaine Pike and moving north to approximately 131 feet south of the Norfolk Southern Railroad line. The northern termini was set during the initial coordination between the Local Agency and INDOT using the "Near Terminus" policy as defined in the Indiana Design Manual to eliminate unnecessary coordination and expenditure of funds to coordinate with an inactive railroad crossing. If project limits were extended north to Water Street, coordination with the Norfolk Southern Railroad would be

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necessary, requiring the LPA to provide PE funding (no federal funds) typically in the amount of \$15,000 to engage the railroad. As this crossing is inactive, this was deemed unnecessary. A discussion with the INDOT Rail Office confirmed the northern limits of the project will not extend across the railroad tracks (Appendix I, page I-9). As confirmed at the preliminary field check, there will be no construction within 131 feet of railroad tracks. The logical termini for the shared use path start at the residential area at the intersection with Bittersweet Lane and moves north along the east side of Blaine Pike to the residential areas just north of West Third Street. The shared use path will allow residents to travel on the path to Portland Memorial Park, the Golden Age Village Adult Community, and the Green Park Cemetery.

OTHER ALTERNATIVES CONSIDERED:

Describe all discarded alternatives, including the Do-Nothing Alternative and an explanation of why each discarded alternative was not selected.

“Do Nothing” Alternative

This alternative would not require an expenditure of funds; however, it would not address the purpose of the project which is to provide a shared-use facility for pedestrians and keep non-motorized vehicles separate from the roadway utilized by motorized vehicles. This alternative would also correct the deteriorated condition of the existing pavement. With the “Do Nothing” alternative, the existing roadway will continue to deteriorate and require constant maintenance. The “Do Nothing” alternative does not address the purpose and need of the project and was dismissed from further consideration.

The Do-Nothing Alternative is not feasible, prudent or practicable because (Mark all that apply):

- | | |
|--|-------------------------------------|
| It would not correct existing capacity deficiencies; | <input type="checkbox"/> |
| It would not correct existing safety hazards; | <input type="checkbox"/> |
| It would not correct the existing roadway geometric deficiencies; | <input type="checkbox"/> |
| It would not correct existing deteriorated conditions and maintenance problems; or | <input checked="" type="checkbox"/> |
| It would result in serious impacts to the motoring public and general welfare of the economy. | <input type="checkbox"/> |
| Other (Describe) It would not provide improved non-motorized access between neighborhoods and destinations | <input checked="" type="checkbox"/> |

ROADWAY CHARACTER:

Blaine Pike

Functional Classification:	<u>Minor Arterial</u>			
Current ADT:	<u>1825</u>	VPD (2022)	Design Year ADT:	<u>2196</u>
				VPD (2042)
Design Hour Volume (DHV):	<u>220 V.P.H.</u>	Truck Percentage (%)	<u>4.5</u>	
Designed Speed (mph):	<u>30</u>	Legal Speed (mph):	<u>30</u>	

	Existing		Proposed	
Number of Lanes:	2		2	
Type of Lanes:	Travel lanes		Travel lanes	
Pavement Width:	10	ft.	10	ft.
Shoulder Width:	0	ft.	0	ft.
Median Width:	N/A	ft.	N/A	ft.
Sidewalk Width:	4	ft.	10	ft.

Setting:	<input checked="" type="checkbox"/>	Urban	<input type="checkbox"/>	Suburban	<input type="checkbox"/>	Rural
Topography:	<input checked="" type="checkbox"/>	Level	<input type="checkbox"/>	Rolling	<input type="checkbox"/>	Hilly

If the proposed action has multiple roadways, this section should be filled out for each roadway.

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DESIGN CRITERIA FOR BRIDGES: N/A

Structure/NBI Number(s): N/A Sufficiency Rating: N/A
(Rating, Source of Information)

	Existing	Proposed
Bridge Type:		
Number of Spans:		
Weight Restrictions:		
Height Restrictions:		
Curb to Curb Width:		
Outside to Outside Width:		
Shoulder Width:		
Length of Channel Work:		

Describe bridges and structures; provide specific location information for small structures.

Remarks: **No presence**
No bridges or small structures are located within the project area or are proposed as part of this project.

Drainage improvements to alleviate standing water in property owner yards includes a combination of grading open swales, installation of trench drains across drives and the shared-use path, installation of yard drains in low lying areas as well as installation of enclosed 12-inch concrete sewer pipes that ties into existing sewers.

Will the structure be rehabilitated or replaced as part of the project?

☐ **Yes**
 ☐ **No**
 ☒ **N/A**

If the proposed action has multiple bridges or small structures, this section should be filled out for each structure.

MAINTENANCE OF TRAFFIC (MOT) DURING CONSTRUCTION:

	Yes	No
Is a temporary bridge proposed?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Is a temporary roadway proposed?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Will the project involve the use of a detour or require a ramp closure? (describe in remarks)	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Provisions will be made for access by local traffic and so posted.	<input checked="" type="checkbox"/>	<input type="checkbox"/>
Provisions will be made for through-traffic dependent businesses.	<input checked="" type="checkbox"/>	<input type="checkbox"/>
Provisions will be made to accommodate any local special events or festivals.	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Will the proposed MOT substantially change the environmental consequences of the action?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Is there substantial controversy associated with the proposed method for MOT?	<input type="checkbox"/>	<input checked="" type="checkbox"/>

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Remarks: The MOT for the project will require Blaine Pike to remain open for the duration of the construction period. The pavement milling and overlay will be performed with the use of flaggers and a moving construction site. At least one lane of traffic will always be maintained for access through the construction zone. Full closure of the road will not be necessary.

Sidewalk is present from West 7th Street to Southwestern Avenue. Accessible accommodations will be provided by closing the existing parking lane for use as temporary sidewalks while the new path is constructed. Protection from construction and traffic will be provided along the temporary path as well as temporary signing. The lane restrictions will pose a temporary inconvenience to traveling motorists including school buses and emergency services. Delays would occur during construction however no significant delays are anticipated and will cease with project completion.

ESTIMATED PROJECT COST AND SCHEDULE

Engineering: \$ 368,400 (2019) Right-of-Way: \$ 494,600 (2021) Construction: \$ 1,615,000 (2022)

Anticipated Start Date of Construction: Spring 2022

Date project incorporated into STIP July 2, 2019

Is the project in an MPO Area? ☐ Yes ☒ No

If yes,

Name of MPO N/A

Location of Project in TIP N/A

Date of incorporation by reference into the STIP N/A

RIGHT OF WAY:

Land Use Impacts	Amount (acres)	
	Permanent	Temporary
Residential	2.59	0.003
Commercial	0.00	0.00
Agricultural	0.00	0.00
Forest	0.00	0.00
Wetlands	0.00	0.00
Other:	0.00	0.00
Other:	0.00	0.00
TOTAL	2.59	0.003

Describe both Permanent and Temporary right-of-way and describe their current use. Typical and Maximum right-of-way widths (existing and proposed) should also be discussed. Any advance acquisition or reacquisition, either known or suspected, and there impacts on the environmental analysis should be discussed.

Remarks: **Right-of-way (ROW) required**
The project requires approximately 2.59 acres of permanent right-of-way from residential properties adjacent to Blaine Pike. The project also requires approximately 0.003 acres of temporary right-of-way from residential properties adjacent to Blaine Pike. The existing right-of-way width varies throughout the project. The minimum proposed R/W width is approximately 20 feet and the maximum proposed width is 63 feet.

If the scope of work or permanent or temporary right-of-way amounts change, the INDOT Environmental Services Division (ESD) and the INDOT District Environmental Section will be contacted immediately.

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Part III – Identification and Evaluation of Impacts of the Proposed Action

SECTION A – ECOLOGICAL RESOURCES

	<u>Presence</u>	<u>Impacts</u>	
		<u>Yes</u>	<u>No</u>
Streams, Rivers, Watercourses & Jurisdictional Ditches			
Federal Wild and Scenic Rivers			
State Natural, Scenic or Recreational Rivers			
Nationwide Rivers Inventory (NRI) listed			
Outstanding Rivers List for Indiana			
Navigable Waterways			

Remarks:

No presence, no impact

Based on a desktop review, a site visit on July 11, 2019 by Little River Consultants, LLC staff, the aerial map of the project area (Appendix B, page B-3), and the water resources map in the Red Flag Investigation (RFI) report (Appendix E, page E-10) there are seven (7) streams, rivers, watercourse or jurisdictional ditches segments within the 0.5 mile search radius. There are no streams, rivers, watercourses, or jurisdictional ditches present within or adjacent to the project area. Therefore, no impacts are expected.

Waters Report

A *Waters of the U.S. Determination/Wetland Delineation Report* was completed for the project on July 22, 2019, as part of the scope of work developed for the project to confirm if any resources are present in the project area. Please refer to Appendix F-1 for the *Waters of the U.S. Determination/Wetland Delineation Report*. It was determined that there are no streams, rivers, watercourses, or jurisdictional ditches present within or adjacent to the project area. The U.S. Army Corps of Engineers (USACE) makes all final determination regarding jurisdiction.

Early Coordination

Early coordination letters were sent on June 12, 2018. IDEM responded on June 12, 2018 with the standard coordination response (Appendix C, page C-12). IDNR Division of Fish and Wildlife responded on July 6, 2018 (Appendix C, page C-10) indicating that formal approval by the Department of Natural Resources under the regulatory programs administered by the Division of Water is not required for this project. The project appears to meet the conditions of the USFWS Interim Policy (2013). All applicable agency recommendations are included in the Environmental Commitments section of this CE document.

	<u>Presence</u>	<u>Impacts</u>	
		<u>Yes</u>	<u>No</u>
Other Surface Waters			
Reservoirs			
Lakes			
Farm Ponds			
Detention Basins			
Storm Water Management Facilities			
Other: _____			

Remarks:

No presence, no impact

Based on a desktop review, a site visit on July 11, 2019 by Little River Consultants, LLC staff, the aerial map of the project area (Appendix B, page B-3), and the water resources map in the RFI report (Appendix E, page E-10), There are twelve (12) lakes within the 0.5 mile search radius. The nearest lake is approximately 0.04

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mile southwest of the project area. No impact is expected. There were 6 NWI-points located within the 0.5-mile search radius. The nearest point is approximately 0.18 mile southeast of the project area. No impact is expected. No other surface waters are present within the project area; therefore, no impacts are expected.

Waters Report

A *Waters of the U.S. Determination/Wetland Delineation Report* was completed for the project on July 22, 2019. Please refer to Appendix F-1 for the *Waters of the U.S. Determination/Wetland Delineation Report*. It was determined that there are no other surface waters present within or adjacent to the project area. The U.S. Army Corps of Engineers (USACE) makes all final determinations regarding jurisdiction.

Early Coordination

Early coordination letters were sent on June 12, 2018. IDEM responded on June 12, 2018 with the standard coordination response (Appendix C, page C-12). IDNR Division of Fish and Wildlife responded on July 6, 2018 (Appendix C, page C-10) indicating that formal approval by the Department of Natural Resources under the regulatory programs administered by the Division of Water is not required for this project. The project appears to meet the conditions of the USFWS Interim Policy (2013). All applicable agency recommendations are included in the Environmental Commitments section of this CE document.

	<u>Presence</u>	<u>Impacts</u>	Yes	No
Wetlands	<input type="checkbox"/>		<input type="checkbox"/>	<input type="checkbox"/>
Total wetland area: <u>0</u> acre(s)		Total wetland area impacted: <u>0</u> acre(s)		

(If a determination has not been made for non-isolated/isolated wetlands, fill in the total wetland area impacted above.)

Wetland No.	Classification	Total Size (Acres)	Impacted Acres	Comments

	<u>Documentation</u>	<u>ES Approval Dates</u>
Wetlands (Mark all that apply)		
Wetland Determination	<input type="checkbox"/>	<input type="text"/>
Wetland Delineation	<input type="checkbox"/>	<input type="text"/>
USACE Isolated Waters Determination	<input type="checkbox"/>	<input type="text"/>
Mitigation Plan	<input type="checkbox"/>	<input type="text"/>

Improvements that will not result in any wetland impacts are not practicable because such avoidance would result in (Mark all that apply and explain):

- Substantial adverse impacts to adjacent homes, business or other improved properties;
- Substantially increased project costs;
- Unique engineering, traffic, maintenance, or safety problems;
- Substantial adverse social, economic, or environmental impacts, or
- The project not meeting the identified needs.

Measures to avoid, minimize, and mitigate wetland impacts need to be discussed in the remarks box.

Remarks: **No presence, no impact**
 Based on a desktop review of the National Wetlands Inventory (NWI) online mapper, the USGS topographic map (Appendix B, page B-2), a site visit on July 11, 2019 by Little River Consultants, LLC staff, the aerial map of the project area (Appendix B, page B-3), and the water resources map in the RFI report (Appendix E, page E-10), there are sixteen (16) NWI-wetland polygons located within the 0.5 mile search radius. The nearest polygon is approximately 0.05 mile east of the project area. No impact is expected. No wetlands are present within or adjacent to the project area, therefore, no impacts are expected.

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Waters Report

A *Waters of the U.S. Determination/Wetland Delineation Report* was completed for the project on July 22, 2019. Please refer to Appendix F-1 for the *Waters of the U.S. Determination/Wetland Delineation Report*. It was determined that there are no wetlands present within or adjacent to the project area. The U.S. Army Corps of Engineers (USACE) makes all final determinations regarding jurisdiction.

Early Coordination

Early coordination letters were sent on June 12, 2018. IDEM responded on June 12, 2018 with the standard coordination response (C-12). IDNR Division of Fish and Wildlife responded on July 6, 2018 (Appendix C, page C-10) indicating that formal approval by the Department of Natural Resources under the regulatory programs administered by the Division of Water is not required for this project. All applicable agency recommendations are included in the Environmental Commitments section of this CE document.

Terrestrial Habitat

Unique or High-Quality Habitat

Presence

X

Impacts

Yes NO

X	

Use the

remarks box to identify each type of habitat and the acres impacted (i.e. forested, grassland, farmland, lawn, etc).

Remarks:

Presence, With Impact

Based on a desktop review, a site visit on May 1, 2018 by BLN staff, and the aerial map of the project area (Appendix B, page B-3) there are wooded acreages adjacent to Blaine Pike near Bittersweet Lane and south of the southern terminus. The wooded areas are small and generally dominated by small deciduous scrub species. The predominant land use along the corridor is single family residential subdivisions. Agricultural land is present beyond the project corridor to the south, east, and west. The City of Portland is located to the north of the project area. Animal species expected to be present in the area include but are not limited to the following: white tail deer, ground squirrels, rabbits, chipmunks, raccoons, opossums, groundhogs, foxes, coyotes, and various native songbird species. No amphibians, reptiles, birds, mammals, or aquatic organisms were observed during the site visit. It is not anticipated that there would be impacts to the wooded areas. North of Bittersweet Lane, additional right-of-way would be acquired on the east side of Blaine Pike. South of Bittersweet Lane, no additional right-of-way is required and only milling, and resurfacing of the pavement is proposed. Therefore, no impacts are expected.

Total area of ground disturbance which excludes including milling and resurfacing of Blaine Pike is approximately 2.59 acres. There is a potential to impact bat habitat as the project will require that 22 trees be removed due to conflicts with the proposed shared-use path. The number and size of impacted trees include the following: 12 6-inch trees, 5 10-inch trees, 4 18-inch trees and 1 30-inch tree. There are several existing mature trees located along the west side of Blaine Pike in the area of the Green Park Cemetery, between and Blaine Pike and the cemetery. The scope of work for the roadway in this area includes only milling and resurfacing of Blaine Pike. There will be no clearing of trees along the west side of the roadway or impacts to the existing mature trees in the area of the Green Park Cemetery. Additional information on the bats is in the Threatened and Endangered Species section of this document.

Early Coordination

Early coordination letters were sent to the Indiana Department of Natural Resources (IDNR) on June 11, 2018. The agency issued recommendation to minimize adverse impacts on terrestrial resources. In their letter dated July 6, 2018 (Appendix C, page C-10), the IDNR recommended that all bare and disturbed land is re-vegetated as soon as possible upon project completion and that appropriate design erosion control measures be implemented and maintained for the duration of the project. All applicable IDNR recommendations are included in the Environmental Commitments section of this CE document.

If there are high incidences of animal movements observed in the project area, or if bridges and other areas appear to be the sole corridor for animal movement, consideration of utilizing wildlife crossings should be taken.

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Karst

Is the proposed project located within or adjacent to the potential Karst Area of Indiana?
Are karst features located within or adjacent to the footprint of the proposed project?

Yes	No
<input type="checkbox"/>	<input checked="" type="checkbox"/>
<input type="checkbox"/>	<input checked="" type="checkbox"/>
<input type="checkbox"/>	<input type="checkbox"/>

If yes, will the project impact any of these karst features?

Use the remarks box to identify any karst features within the project area. (Karst investigation must comply with the Karst MOU, dated October 13, 1993)

Remarks:

Outside Karst Area

Based on a desktop review, a site visit on May 1, 2018 by BLN staff, and the project is located outside the designated karst region of Indiana as outlined in the October 13, 1993 Memorandum of Understanding (MOU). According to the topo map of the project area (Appendix B, page B-2), the RFI report (Appendix E, page E-1) there are no karst features known to exist within or adjacent to the project area. In the early coordination response, the Indiana Geological Survey (IGS) did not indicate that karst features may exist in the project area (Appendix C, page C-7). Response from IGS has been communicated with the designer on March 6, 2020. No impacts are expected.

Rock and Mineral Resources

The Early Coordination response from IGS did indicate that abandoned industrial mineral resources extraction sites for sand and gravel exist in the general project area (Appendix C, page C-7). Based on the RFI report (Appendix E, page E-1) and site visit by BLN staff on May 1, 2018, no surface or underground mines were present within or adjacent to the project area.

Threatened or Endangered Species

Within the known range of any federal species
Any critical habitat identified within project area
Federal species found in project area (based upon informal consultation)
State species found in project area (based upon consultation with IDNR)

Presence

<input checked="" type="checkbox"/>
<input type="checkbox"/>
<input type="checkbox"/>
<input type="checkbox"/>

Impacts

Yes	No
<input checked="" type="checkbox"/>	<input type="checkbox"/>
<input type="checkbox"/>	<input type="checkbox"/>
<input type="checkbox"/>	<input type="checkbox"/>
<input type="checkbox"/>	<input type="checkbox"/>

Is Section 7 formal consultation required for this action?

Yes	No
<input type="checkbox"/>	<input checked="" type="checkbox"/>

Remarks:

Based on a desktop review and the RFI (Appendix E-1), completed by BLN personnel on May 1, 2018, the IDNR Jay County Endangered, Threatened and Rare (ETR) Species List has been checked and is included in (Appendix E, page E-14). The highlighted species on the list reflect the federal and state identified ETR species located within the county. According to the IDNR-DFW early coordination response letter dated July 6, 2018 (Appendix C, page C-10), the Natural Heritage Program's Database has been checked. To date, no plant or animal species listed as state or federally threatened, endangered, or rare have been reported to occur in the project vicinity.

Indiana Bat and Northern Long-Eared Bat

Bats, Programmatic Informal Consultation – Not Likely to Adversely Affect

Project information was submitted through the USFWS's Information for Planning and Consultation (IPaC) portal, and an official species list was generated (Appendix C, page C-17). The project is within the range of the federally endangered Indiana bat (*Myotis sodalis*) and the federally threatened northern long-eared bat (*Myotis septentrionalis*). No additional species were found within or adjacent to the project area other than the Indiana bat and northern long-eared bat.

The project qualifies for the *Range-wide Programmatic Informal Consultation for the Indiana bat and the northern long-eared bat (NLEB)*, dated May 2016 (revised February 2018), between FHWA, Federal Railroad Administration (FRA), and USFWS. An effect determination key was completed on July 19, 2019, and based on the responses provided, the project was found to be "not likely to adversely affect" the Indiana

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bat and/or the NLEB (Appendix C, page C-23). INDOT reviewed and verified the effect finding on July 21, 2019, and requested USFWS's review of the finding (Appendix C, page C-23). No response was received from USFWS within the 14-day review period; therefore, it was concluded they concur with the finding. Avoidance and Mitigation Measures (AMMs) are included as firm commitments in the Environmental Commitments section of this document. No critical habitats were identified within the project area and no other species were included in the IPaC Species List.

This precludes the need for further consultation on this project as required under Section 7 of the Endangered Species Act, as amended. If new information on endangered species at the site becomes available, or if project plans are changed, USFWS will be contacted for consultation.

SECTION B – OTHER RESOURCES

Drinking Water Resources

Wellhead Protection Area
Public Water System(s)
Residential Well(s)
Source Water Protection Area(s)
Sole Source Aquifer (SSA)

Presence

X

Impacts

Yes	No
X	

If a SSA is present, answer the following:

Is the Project in the St. Joseph Aquifer System?
Is the FHWA/EPA SSA MOU Applicable?
Initial Groundwater Assessment Required?
Detailed Groundwater Assessment Required?

Yes	No

Remarks:

Outside of Sole Source Aquifer (SSA)

The project is located in Jay County, which is not located within the area of the St. Joseph Sole Source Aquifer, the only legally designated sole source aquifer in the state of Indiana. Therefore, the FHWA/EPA Sole Source Aquifer Memorandum of Understanding (MOU) is not applicable to this project. Therefore, a detailed groundwater assessment is not needed, and no impacts are expected.

Not located in a Wellhead Protection Area

The Indiana Department of Environmental Management's Wellhead Proximity Determinator website (<http://www.in.gov/idem/cleanwater/pages/wellhead/>) was accessed on March 12, 2019 by BLN personnel. This project is not located within a Wellhead Protection Area. In an early coordination letter dated June 12, 2018, IDEM stated the project is not located within a wellhead area, however, the project is located within a tenth of a mile of a Wellhead Protection Area (Appendix C, page C-16). No impacts are expected.

No Wells Present, No Impacts

The Indiana Department of Natural Resources Water Well Record Database website (<https://www.in.gov/dnr/water/3595.htm>) was accessed on March 12, 2019 by BLN personnel. No wells are located near this project. Therefore, no impacts are expected.

In an Urban Area Boundary Location

Based on a desktop review of the INDOT MS4 website (<https://entaps.indot.in.gov/MS4/>) by BLN personnel on January 15, 2019 and the RFI report this project is located in an Urban Area Boundary (UAB) location. This project lies within the Portland UAB; however, a Rule 13 Permit from IDEM has not been issued. No further coordination is necessary at this time. An early coordination letter was sent on

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June 11, 2018 to the City of Portland and Jay County Community Development; a follow-up email was sent to the Portland Water Utility on March 12, 2019. The MS4 coordinator did not respond within the 30-day time frame.

In a Public Water System

Based on a desktop review, a site visit on March 12, 2019 by BLN personnel, and the aerial map of the project area (Appendix B, page B-3) this project is located where there is a public water system. The public water system will be temporarily affected as water hydrant relocation is anticipated. Coordination with the City of Portland Water Department was conducted by BLN as part of the utility coordination for the project. Early coordination letters were sent on June 11, 2018 to Jay County officials and the Mayor of Portland. No responses were received. Coordination with City of Portland Water is occurring as part of the BLN Design Utility coordination. Plans were provided to the City of Portland Water on May 13, 2019 (Appendix I, page I-5) to verify utility location and to identify potential conflict between the existing facility and the proposed project. BLN met with the City of Portland Water Department in November 2019 to discuss the project. Temporary disruption of service is possible as a result of hydrant relocation. BLN is involved with on-going utility coordination City of Portland Water as part of the project development.

Flood Plains

	<u>Presence</u>	<u>Impacts</u>	
		<u>Yes</u>	<u>No</u>
Longitudinal Encroachment	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
Transverse Encroachment	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Project located within a regulated floodplain	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Homes located in floodplain within 1000' up/downstream from project	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>

Discuss impacts according to classification system described in the "Procedural Manual for Preparing Environmental Studies".

Remarks:

In floodplain

Based on a desktop review of The Indiana Department of Natural Resources Indiana Floodway Information Portal website (<http://dnrmmaps.dnr.in.gov/appsphp/fdms/>) was accessed on March 12, 2019 by BLN, and the RFI report; this project is located in a regulatory floodplain as determined from approved IDNR floodplain maps (Appendix B, page B-5). An early coordination letter was sent on July 18, 2019 to the local Floodplain Administrator. The floodplain Administrator did not respond within the 30-day time frame. This project qualifies as a Category 3 per the INDOT CE Manual, which states the modifications to drainage structures included in this project will result in an insubstantial change in their capacity to carry flood water. This change could cause a minimal increase in flood heights and flood limits. These minimal increases will not result in any substantial adverse impacts on the natural and beneficial floodplain values; they will not result in substantial change in flood risks or damage; and they do not have substantial potential for interruption or termination of emergency service or emergency routes; therefore, it has been determined that this encroachment is not substantial. An Early Coordination letter was sent to the IDNR Division of Fish and Wildlife on June 11, 2018. The IDNR Division of Fish and Wildlife responded with a letter dated July 6, 2018 (Appendix C, page C-10) indicating formal approval by IDNR under the regulatory programs administered by the Division of Water is not required for this project. An Early Coordination letter was sent to the local Floodplain Administrator on June 11, 2018. No response was provided.

Farmland

	<u>Presence</u>	<u>Impacts</u>	
		<u>Yes</u>	<u>No</u>
Agricultural Lands	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Prime Farmland (per NRCS)	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

Total Points (from Section VII of CPA-106/AD-1006* N/A

*If 160 or greater, see CE Manual for guidance.

See CE Manual for guidance to determine which NRCS form is appropriate for your project.

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Remarks: **No presence, no impact**
Based on a desktop review, a site visit May 1, 2018 by BLN staff, the aerial map of the project area (Appendix B, page B-3), there is land that meets the definition of farmland under the Farmland Protection Policy Act (FPPA) adjacent to the southern terminus of the project area. No permanent or temporary right-of-way will be required from this area. An early coordination letter was sent on June 11, 2018 to Natural Resources Conservation Services (NRCS). In a response dated May 26, 2020, see Appendix C, page C-37, the NRCS indicated that the project will not cause a conversion of prime farmland. The requirements of the FPPA do not apply to this project; therefore, no impacts are expected.

SECTION C – CULTURAL RESOURCES

	Category	Type	INDOT Approval Dates	N/A
Minor Projects PA Clearance				X

Eligible and/or Listed Resource Present

Results of Research

Archaeology	
NRHP Buildings/Site(s)	
NRHP District(s)	
NRHP Bridge(s)	

Project Effect

No Historic Properties Affected ☒ No Adverse Effect ☐ Adverse Effect ☐

Documentation Prepared

Documentation (mark all that apply)

		ES/FHWA Approval Date(s)	SHPO Approval Date(s)
Historic Properties Short Report			
Historic Property Report	X	January 31, 2020	November 6, 2019
Archaeological Records Check/ Review			
Archaeological Phase Ia Survey Report	X	January 31, 2020	March 4, 2020
Archaeological Phase Ic Survey Report			
Archaeological Phase II Investigation Report			
Archaeological Phase III Data Recovery			
APE, Eligibility and Effect Determination	X	January 31, 2020	March 4, 2020
800.11 Documentation	X	January 31, 2020	March 4, 2020

Memorandum of Agreement (MOA) ☐

MOA Signature Dates (List all signatories)

Describe all efforts to document cultural resources, including a detailed summary of the Section 106 process, using the categories outlined in the remarks box. The completion of the Section 106 process requires that a Legal Notice be published in local newspapers. Please indicate the publication date, name of paper(s) and the comment period deadline. Likewise include any further Section 106 work which must be completed at a later date, such as mitigation or deep trenching.

Remarks: **Area of Potential Effect (APE):**
An Area of Potential Effect (APE) was established for the proposed project as part of the Historic Properties Report. The APE is the area in which an undertaking may cause direct or indirect changes in character or use of a historic property. The boundary of the APE is determined through the consideration of the effect of the

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undertaking in respect to visual and audible intrusions, changes in traffic patterns, and alterations in land use or public access.

The APE of the project includes all properties adjacent to the project and those with a proximate viewshed of the project. The APE extends approximately 0.12 mile east of the project at its widest point, 0.10 mile west of the project at its widest point, 0.05 mile north, and 0.05 south of the project. A detailed map of the APE can be found in Appendix D, page D-9.

Coordination with Consulting Parties:

The following parties/agencies were invited to become consulting parties (CP) to this project and were sent an early coordination letter and archaeology short report (Tribes only) on January 23, 2019. The State Historic Preservation Officer (SHPO) received a paper copy of the early coordination letter and archaeology short report. All potential CPs were invited to view the information on IN SCOPE <http://erms.indot.in.gov/Section106Documents/>. The SHPO is an automatic consulting party; that office and others accepting consulting party status are shown in boldface type.

- **Indiana State Historic Preservation Officer**
- Indiana Landmarks, Eastern Regional Field Office
- Jay County Commissioners
- Mayor of Portland
- Jay County Commissioners
- Jay County Highway Department
- Jay County Historian
- Jay County Historical Society
- Portland Historic Preservation Commission
- **Eastern Shawnee Tribe of Oklahoma**
- **Miami Tribe of Oklahoma**
- Peoria Tribe of Indians of Oklahoma
- Pokagon Band of Potawatomi Indians
- Delaware Tribe of Indians, Oklahoma
- Forest County Potawatomi Community

The Federal Highway Administration (FHWA) is the federal agency associated with this undertaking. The INDOT, Cultural Resources Office (INDOT, CRO) is acting on behalf of the FHWA. The Indiana Department of Natural Resources, Division of Historic Preservation and Archaeology (IDNR, State Historic Preservation Officer (IDNR, SHPO) is automatically considered a consulting party for federally funded transportation projects.

Archaeology:

In 2019, Qualified Professional Archaeologist who meets the Secretary of the Interior's Professional Qualification Standards employed by Cultural Resource Analysts, Inc. (CRA). completed the original archaeological Phase 1a Survey Report on January 2, 2019 (Appendix D, page D-10). Based on changes in the project scope an Addendum to the Phase 1a Survey Report was also completed on December 20, 2019 (Appendix D, page D-12). These reports were approved by INDOT-CRO and in a letter dated March 6, 2019 (Appendix D, page D-24) the SHPO stated that there were no identified archaeological sites within the project area and it was recommended that no additional archaeological investigations appear necessary and the project be allowed to proceed as planned.

Historic Properties:

The National Register of Historic Places (NRHP), Indiana Register of Historic Sites and Structures (State Register), the State Historic Architectural and Archaeological Research Database (SHAARD and the Indiana Historic Building, Bridges and Cemeteries map), and the Jay County Interim Report (1985) were consulted. Approximately 80 above-ground resources were identified within the APE. There are two previously surveyed

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above-ground resources within the project APE: The Green Park Cemetery, IHSSI No. 075-521-33093; CR-38-64, and the Roscoe Wheat House, IHSSI No. 075-521-33092, rated Outstanding.

A Historic Properties Report (HPR) was prepared by Green 3, LLC on January 6, 2019, and contained the results of this survey and literature review (Appendix D, page D-7). The APE contains no properties listed in the National Register and, as a result of identification and evaluation efforts eight newly inventoried properties were identified that retained sufficient integrity to have been given a rating of Contributing. The only property evaluated for listing in the National Register was the Roscoe Wheat House, IHSSI No. 075-521-33092, rated Outstanding, and concluded that it was not recommended eligible for listing in the National Register. The report was approved by INDOT-CRO and in a letter dated November 6, 2019 the SHPO stated that they agreed no properties that are listed in or eligible for inclusion in the NRHP lie with the APE. (Appendix D, page D-26).

Green Park Cemetery:

The Green Park Cemetery was established in 1929 and is approximately 40 acres in size on the southwest corner of W Seventh St. & Blaine Pike, along the west side of Blaine Pike. The Green Park Cemetery is well marked and maintained. There are some large mature trees between the roadway and the existing cemetery markers. The Blaine pike project will not impact any of the trees along the west side of the roadway within the cemetery limits. On July 20, 2020 BLN coordinated with Mr. Dave Teeter, President of the Green park Cemetery about the project. BLN discussed the project scope and indicated that the project would not impact the cemetery property and that access to the Green Park Cemetery would be maintained during construction. Mr. Teeter indicated he lived in the area, was already familiar with the project and was in support of the proposed work. He saw no potential impacts to the Green Park Cemetery and indicated that he was in support of the project and thought the work should be done. (Appendix D, page D-28).

As the project is within 100 feet of the Green Park Cemetery, BLN coordinated with INDOT-CRO to determine if a Cemetery Development Plan (CDP) would be necessary as part of the project. On June 12, 2020 INDOT-CRO indicated that CDP's are not required for projects that occur within 100' of a cemetery if there is no work or excavation on the west side of the roadway. The work is only milling and resurfacing of the roadway without impacting the cemetery. CDP's are required when there are impacts to a cemetery (usually temporary r/w for driveway reconstruction), when boundaries of a known cemetery are undefined and adjacent to a project area, or when there are unmarked cemeteries or family burial grounds within 100 feet of a project area. A CDP is not required for this project unless the project scope has changed, and the updated project scope will impact the cemetery. If the project scope is the same, it will not impact the cemetery and thus does not require a CDP (Appendix D, page D-29).

Documentation, Findings:

A "No Historic Properties Affected" finding was submitted to INDOT, CRO for approval. On January 31, 2020, INDOT CRO, on behalf of FHWA, approved the "No Historic Properties Affected" determination (Appendix D, page D-1). Following this finding, the effect documentation was provided to the IDNR, SHPO and participating consulting parties, for a 30-day review period. In a letter dated March 4, 2020, the IDNR, SHPO concurred with the "No Historic Properties Affected" determination (Appendix D, page D-32). No additional comments were received. Thus, the Section 106 process has been completed and the responsibilities of the FHWA under Section 106 have been fulfilled.

Public Involvement:

The "No Historic Properties Affected" determination was advertised in the *Portland Commercial Review* newspaper on March 20, 2020 for a 30-day comment period (Appendix D, page D-34). The comment period closed on April 18, 2020. with no comments received. This completed the public involvement requirements of cultural resources.

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SECTION D – SECTION 4(f) RESOURCES/ SECTION 6(f) RESOURCES

Section 4(f) Involvement (mark all that apply)

Parks & Other Recreational Land

Publicly owned park
Publicly owned recreation area
Other (school, state/national forest, bikeway, etc.)

Presence

X
X

Use

Yes	No
	X
	X

Evaluations

Prepared

Programmatic Section 4(f)*
“De minimis” Impact*
Individual Section 4(f)

FHWA Approval date

--

Wildlife & Waterfowl Refuges

National Wildlife Refuge
National Natural Landmark
State Wildlife Area
State Nature Preserve

Presence

Use

Yes	No

Evaluations

Prepared

Programmatic Section 4(f)*
“De minimis” Impact*
Individual Section 4(f)

FHWA Approval date

--

Historic Properties

Sites eligible and/or listed on the NRHP

Presence

--

Use

Yes	No

Evaluations

Prepared

Programmatic Section 4(f)*
“De minimis” Impact*
Individual Section 4(f)

FHWA Approval date

--

**FHWA approval of the environmental document also serves as approval of any Section 4(f) Programmatic and/or De minimis evaluation(s) discussed below.*

Discuss Programmatic Section 4(f) and “de minimis” Section 4(f) impacts in the remarks box below. Individual Section 4(f) documentation must be separate Draft and Final documents. For further discussions on Programmatic, “de minimis” and Individual Section 4(f) evaluations please refer to the “Procedural Manual for the Preparation of Environmental Studies”. Discuss proposed alternatives that satisfy the requirements of Section 4(f).

Remarks:

Presence, No Impact, No Use

Section 4(f) of the U.S. Department of Transportation Act of 1966 prohibits the use of certain public and historic lands for federally funded transportation facilities unless there is no feasible and prudent alternative. The law applies to significant publicly owned parks, recreation areas, wildlife/waterfowl refuges, and NRHP eligible or listed historic properties. Lands subject to this law are considered Section 4(f).

Based on a desktop review, a site visit on May 1, 2018 by BLN staff, the aerial map of the project area (Appendix B, page B-3), and the RFI report (Appendix E, page E-1) there are no wildlife/waterfowl refuges

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or NRHP eligible or listed historic properties located within 0.5 miles of the project. There is a 4(f)-resource located within 0.5 miles of the project. The Portland Memorial Park (alternative name: Northend Park or Sportland Park Development) does not have direct access onto Blaine Pike, but the facility is located within 100 feet of the corridor, near Western Avenue. It contains a soccer field, four baseball diamonds, open fields, and parking. The Portland Memorial Park does not have direct access onto Blaine Pike, but the facility is located within 100 feet of the corridor, near Western Avenue. The proposed Portland River Path is approximately 0.01-mile northeast of the project area and is a proposed new trail.

BLN coordinated with Tom Leonard the City of Portland Director of Parks and Recreation on June 15, 2020. BLN provided a current set of plans and scope of work for the project and indicated that the Portland Memorial Park is located along the project corridor near Western Avenue and the facility is located within 100 feet of the Blaine Pike corridor. The project will not impact this resource by taking permanent right of way or restricting access. BLN requested any comments or concerns about the project regarding impacts to Portland Memorial Park and the proposed Portland River Path. Mr. Leonard responded there were no questions or concerns with the project in relation to resources of the City of Portland Department of Parks and Recreation. (Appendix I, page I-7-8). The project will not use this resource by taking permanent right of way and will not alter the environment in such a way as to constitute constructive use of this resource. Therefore, no impacts are expected.

Section 6(f) Involvement

Presence

Use

Section 6(f) Property

☒ X

Yes

☐

No

☒ X

Discuss proposed alternatives that satisfy the requirements of Section 6(f). Discuss any Section 6(f) involvement.

Remark:

Presence, No Impact

The U.S. Land and Water Conservation Fund Act of 1965 established the Land and Water Conservation Fund (LWCF) to preserve, develop, and assure accessibility to outdoor recreation resources. Section 6(f) of this Act prohibits conversion of lands purchased with LWCF monies to a non-recreation use.

A review of 6(f) properties on the Land and Water Conservation Fund (LWCF) website at <https://www.lwcfcoalition.com/tools> revealed a total of two properties in Jay County (Appendix I, page I-1). One of these projects, Portland Memorial Park (alternative name: Northend Park or Sportland Park Development), is located within 0.5 miles of the project (Appendix I, page I-1). The Portland Memorial Park does not have direct access onto Blaine Pike, but the facility is located within 100 feet of the corridor, near Western Avenue. BLN coordinated with Tom Leonard the City of Portland Director of Parks and Recreation on June 15, 2020. BLN provided a current set of plans and scope of work for the project and indicated that the Portland Memorial Park is located along the project corridor near Western Avenue and the facility is located within 100 feet of the Blaine Pike corridor. The project will not impact this resource by taking permanent right of way or restricting access. BLN requested any comments or concerns about the project regarding Portland Memorial Park, the proposed Portland River Path, or any other resource under the jurisdiction of the Parks Department / Parks Board. Mr. Leonard responded there were no questions or concerns with the project in relation to resources of the City of Portland Department of Parks and Recreation. The project will not take any temporary or permanent right of way and will not cause a conversion of land that LWCF funds were used, therefore, no impacts are expected.

SECTION E – Air Quality

Air Quality

Conformity Status of the Project

Is the project in an air quality non-attainment or maintenance area?

Yes

☐

No

☒ X

This is page 18 of 25 Project name: Blaine Pike Pedestrian System Improvements Date: September 24, 2020

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If YES, then:

Is the project in the most current MPO TIP?

Is the project exempt from conformity?

If the project is NOT exempt from conformity, then:

Is the project in the Transportation Plan (TP)?

Is a hot spot analysis required (CO/PM)?

Level of MSAT Analysis required?

Level 1a ☒ Level 1b ☐ Level 2 ☐ Level 3 ☐ Level 4 ☐ Level 5 ☐

Remarks:

STIP/TIP

This project is included in the Fiscal Year (FY) 2020-2024 Statewide Transportation Improvement Program (STIP) (Appendix H, page H-1).

Attainment Area

This project is located in Jay County, which is currently in attainment for all criteria pollutants according to the Indiana Department of Environmental Management (https://www.in.gov/idem/airquality/files/nonattainment_areas_map.pdf). Therefore, the conformity procedures of 40 CFR Part 93 do not apply.

MSAT Level 1a Analysis

This project is of a type qualifying as a categorical exclusion (Group 1) under 23 CFR 772.227(c), or exempt under the Clean Air Act conformity rule under 40 CFR 93.126, and as such, a Mobile Source Air Toxics analysis is not required.

SECTION F - NOISE

Noise

Yes

No

Is a noise analysis required in accordance with FHWA regulations and INDOT's traffic noise policy?

☐
☒

No

Yes/ Date

ES Review of Noise Analysis

N/A

Remarks:

Type III Project

This project is a Type III project. In accordance with 23 CFR 772 and the current *Indiana Department of Transportation Traffic Noise Analysis Procedure*, this action does not require a formal noise analysis.

SECTION G – COMMUNITY IMPACTS

Regional, Community & Neighborhood Factors

Will the proposed action comply with the local/regional development patterns for the area?

Yes

No

☒

☐

Will the proposed action result in substantial impacts to community cohesion?

☐

☒

Will the proposed action result in substantial impacts to local tax base or property values?

☐

☒

Will construction activities impact community events (festivals, fairs, etc.)?

☐

☒

Does the community have an approved transition plan?

☒

☐

If No, are steps being made to advance the community's transition plan?

☐

☐

Does the project comply with the transition plan? (explain in the remarks box)

☒

☐

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Remarks: There may be temporary inconveniences associate with construction such as increased travel times, possible construction noise, and fugitive dust. There will be no substantial impacts on community cohesion or property values as a result of the project. Acquisition of the additional permanent right-of-way would not appreciably affect the property tax base of Jay County.

According to a review of the City of Portland's website and www.fairsandfestivals.net, an online resource for local fairs and festivals, there are no scheduled festivals or other public events at this time that will be impacted as a result of the project. As required by the Americans with Disabilities Act (ADA) each of the communities within Jay County, as well as the county itself, has developed an ADA Transition Plan. The project complies with the Jay County ADA Transition Plan as the proposed improvements will provide ADA compliant ramps and path where they currently do not exist and will improve existing curb ramps that are not currently ADA compliant. The project also complies with local & regional development patterns for the area.

Indirect and Cumulative Impacts

Will the proposed action result in substantial indirect or cumulative impacts?

Yes

☐

No

☒

Remarks: Indirect impacts are effects which are caused by the action and are later in time or farther removed in distance but are still reasonably foreseeable. Indirect effects may include growth inducing effects and other effects related to induced changes in the pattern of land use, population density, or growth rate. Cumulative impacts affect the environment with result from the incremental impact of the action when added to other past, present, and reasonably foreseeable future actions regardless of what agency or person undertakes such actions.

This project will address the deteriorated condition of the roadway and improve pedestrian and non-motorized access. The non-motorized path that is being installed exists in some areas of the project. This project will expand the length of this path and bring the path up to code in the places were it already exists. The construction / improvements to the non-motorized path could lead to the eventual planning and development of additional portions of the trail /path. Due to the nature of this project, no substantial indirect or cumulative impacts are expected.

Public Facilities & Services

Will the proposed action result in substantial impacts on health and educational facilities, public and private utilities, emergency services, religious institutions, airports, public transportation or pedestrian and bicycle facilities? *Discuss how the maintenance of traffic will affect public facilities and services.*

Yes

☐

No

☒

Remarks: **Presence, No Impact**

Based on a desktop review, a site visit on May 1, 2018 by BLN staff, the aerial map of the project area (Appendix B, page B-3) and the RFI report (Appendix E, page E-1), there are 30 public facilities located within the 0.5 mile of the project. None of these facilities are within the project area.

The Portland Municipal Airport is within a 3.8-mile radius of the project. Early coordination with INDOT Aviation was initiated on June 25, 2018. IN a letter dated June 25, 2020 (Appendix C, page C-11) INDOT Aviation indicated that the Portland Municipal Airport is located approximately 1.2 nautical miles north of the northernmost proposed project site. An Indiana Tall Structure permit would not be required unless the project involves the construction of a temporary (e.g., crane) or permanent structure that penetrates a 100:1 slope from the nearest point of the Portland Municipal Airport runway. The project scope will not involve constriction activities that will require construction of a temporary (e.g., crane) or permanent structure that penetrates a 100:1 slope from the nearest point of the Portland Municipal Airport runway. No impact is expected.

The desktop review identified a 6-inch natural gas pipeline with the Ohio Valley Gas Corporation approximately 0.20 mile southwest of the project area. Utility coordination for the project was conducted with the Ohio Valley Gas Corporation by the BLN project utility coordinator as part of the project development process. The project will not impact the existing natural gas line, no impact is expected.

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County Jay Route Blaine Pike Des. No. 1600965

It is the responsibility of the project sponsor to notify school corporations and emergency services at least two weeks prior to any construction that would block or limit access. This recommendation will become a firm commitment and included in the Section J – Environmental Commitments section of this document.

Environmental Justice (EJ) (Presidential EO 12898)

During the development of the project were EJ issues identified?

Does the project require an EJ analysis?

If YES, then:

Are any EJ populations located within the project area?

Will the project result in adversely high or disproportionate impacts to EJ populations?

Yes	No
<input type="checkbox"/>	<input checked="" type="checkbox"/>
<input checked="" type="checkbox"/>	<input type="checkbox"/>
<input checked="" type="checkbox"/>	<input type="checkbox"/>
<input type="checkbox"/>	<input checked="" type="checkbox"/>

Remarks:

EJ Analysis, EJ Populations

Under FHWA Order 6640.23A, FHWA and the City of Portland, as a recipient of funding from FHWA, are responsible to ensure that their programs, policies, and activities do not have a disproportionately high and adverse effect on minority or low-income populations. Per the current INDOT Categorical Exclusion Manual, an Environmental Justice (EJ) Analysis is required for any project that has two or more relocations or 0.5 acres of additional permanent right-of-way. The project will require approximately 2.79 acres of permanent right-of-way. Therefore, an EJ Analysis is required.

Potential EJ impacts are detected by locating minority and low-income populations relative to a reference population to determine if populations of EJ concern exists and whether there could be disproportionately high and adverse impacts to them. The reference population may be a county, city, or town and is called the community of comparison (COC). In this project, the COC is Jay County. The community that overlaps the project area is called the affected community (AC). In this project, the AC is Census Tract 9631. An AC has a population of concern for EJ if the population is more than 50% minority or low-income or if the low-income or minority population is 125% of the COC. Data from the 2013-2017 American Community Survey 5 Year Estimates was obtained from the US Census Bureau Website <https://factfinder.census.gov/> on July 22, 2019 by BLN staff. The data collected for minority and low-income populations within the AC are summarized in the below table.

Table: Minority and Low-Income Data (ACS, 2013-2017)		
	COC Jay County	AC Census Tract 9631
Percent Minority	4.90	11.87
125% of COC	6.13	
EJ Population of Concern		Yes
Percent Low-Income	17.90	22.25
125% of COC	22.38	
EJ Population of Concern		No

AC Census Tract 9631 has a percent minority of 11.87% which is below 50% and is above the 125% COC threshold. Therefore, AC Census Tract 9631 is a minority population of EJ concern.

AC Census Tract 9631 has a percent low-income of 22.25% which is below 50% and is below the 125% COC threshold. Therefore, AC Census Tract 9631 does not contain a low-income population of EJ concern.

Conclusion

INDOT Environmental Services was contacted on July 24, 2019 by BLN staff about the minority population of EJ concern. On July 25, 2019 they wrote “INDOT-Environmental Services Division (ESD) has reviewed the project information along with the Environmental Justice (EJ) Analysis for the reference project. The project would require strip right-of-way, require no relocations, and would not disrupt community cohesion

Indiana Department of Transportation

County Jay Route Blaine Pike Des. No. 1600965

or create a physical barrier. The project would include improvements to the roadway. With the information provided, INDOT-ESD would not consider the impacts associated with this project as causing a disproportionately high and adverse effect on minority and/or low income populations of EJ concern relative to non EJ populations in accordance with the provision of Executive Order 12898 and FHWA Order 6640.23a. No further EJ Analysis is required. The census data sheets, maps, and calculations can be found in Appendix I, page I-2.

Relocation of People, Businesses or Farms

Will the proposed action result in the relocation of people, businesses, or farms?

Is a Business Information Survey (BIS) required?

Is a Conceptual Stage Relocation Study (CSRS) required?

Has utility relocation coordination been initiated for this project?

Yes

No

X

X
X
X

Number of relocations: Residences: _____ Businesses: _____ Farms: _____ Other: _____

If a BIS or CSRS is required, discuss the results in the remarks box.

Remarks:

No Relocations

No relocations of people, businesses, or farms will take place as a result of this project. A list of utility companies that have been coordinated with can be accessed on the design plans index sheet, see Appendix B-14.

SECTION H – HAZARDOUS MATERIALS & REGULATED SUBSTANCES

Hazardous Materials & Regulated Substances (Mark all that apply)

Red Flag Investigation

Phase I Environmental Site Assessment (Phase I ESA)

Phase II Environmental Site Assessment (Phase II ESA)

Design/Specifications for Remediation required?

Documentation

X

No Yes/ Date

ES Review of Investigations

X – 01/15/2019

Include a summary of findings for each investigation.

Remarks:

Presence, With Impact or Potential Impact

A Red Flag Investigation was conducted on October 16, 2018 by BLN staff to determine the presence of facilities or infrastructures that may require special consideration in design of the project. (Appendix E, page E-1) Three (3) RCRA Generators/TSDs are within the 0.5-mile search radius. The nearest RCRA Generator/TSD, Patriot Paint Manufacturing (304 Blaine Pike, Portland Indiana 47371, AI #24908), is mapped as a point 0.11 miles west of the project area; however, it appears that the property extends to immediately west of the project area. If excavation occurs in this location, proper removal and disposal of contaminated soil and/or groundwater may be necessary. One (1) state cleanup site is within the 0.5-mile search radius. Patriot Paint Manufacturing (304 Blaine Pike, AI #24908) is located adjacent to the project area. If excavation occurs in this location, proper removal and disposal of contaminated soil and/or groundwater may be necessary. This recommendation will become a firm commitment and included in the Section J – Environmental Commitments section of this document.

SECTION I – PERMITS CHECKLIST

This is page 22 of 25 Project name: Blaine Pike Pedestrian System Improvements Date: September 24, 2020

Indiana Department of Transportation

County Jay

Route Blaine PikeDes. No. 1600965

Likely Required

Permits (mark all that apply)

Army Corps of Engineers (404/Section10 Permit)

Individual Permit (IP)	
Nationwide Permit (NWP)	
Regional General Permit (RGP)	
Pre-Construction Notification (PCN)	
Other	
Wetland Mitigation required	
Stream Mitigation required	

IDEM

Section 401 WQC	
Isolated Wetlands determination	
Rule 5	X
Other	
Wetland Mitigation required	
Stream Mitigation required	

IDNR

Construction in a Floodway	
Navigable Waterway Permit	
Lake Preservation Permit	
Other	
Mitigation Required	

US Coast Guard Section 9 Bridge Permit

Others (Please discuss in the remarks box below)

Remarks:

Permits

For projects that have one acre or more of land disturbance, an IDEM Rule 5 erosion control/stormwater permit is required. This project will require a Rule 5 permit.

The project is located within a floodplain. An Early Coordination letter was sent to the IDNR Division of Fish and Wildlife on June 11, 2018. The IDNR Division of Fish and Wildlife responded with a letter dated July 6, 2018, see Appendix C, page C-10 indicated formal approval by IDNR under the regulatory programs administered by the Division of Water is not required for this project. An Early Coordination letter was sent to the local Floodplain Administrator on June 11, 2018. No response was provided.

It is the responsibility of the City of Portland to identify and obtain all required permits.

SECTION J- ENVIRONMENTAL COMMITMENTS

The following information should be provided below: List all commitments, name of agency/organization requesting the commitment(s) and indicating which are firm and which are for further consideration. The commitments should be numbered.

Remarks:

Firm:

1. If the scope of work or permanent or temporary right-of-way amounts change, the INDOT Environmental Services Division (ESD) and the INDOT Vincennes District Environmental Section will be contacted immediately. (INDOT- ESD and INDOT Vincennes)
2. It is the responsibility of the project sponsor to notify school corporations and emergency services at least two weeks prior to any construction that would block or limit access. (INDOT-ESD)

Indiana Department of Transportation

County Jay Route Blaine Pike Des. No. 1600965

3. Patriot Paint Manufacturing (304 Blaine Pike, Portland Indiana 47371AI #24908), is mapped as a point 0.11 mile west of the project area; however, it appears that the property extends to immediately west of the project area. If excavation occurs in this location, proper removal and disposal of contaminated soil and/or groundwater may be necessary. (INDOT-ESD)
4. General AMM 1: Ensure all operators, employees, and contractors working in areas of known or presumed bat habitat are aware of all FHWA/FRA/FTA (Transportation Agencies) environmental commitments, including all applicable AMMs. (USFWS)
5. Lighting AMM 1: Direct temporary lighting away from suitable habitat during the active season. (USFWS)
6. Tree AMM 1: Modify all phases/aspects of the project (e.g., temporary work areas, alignments) to avoid tree removal. (UFWS)
7. Tree AMM 2: Apply time of year restrictions, April 1 to September 30, for tree removal when bats are not likely to be present, or limit tree removal to 10 or fewer trees per project at any time of the year within 100 feet of existing road rail surface and outside of documented roosting/foraging habitat or travel corridors; visual emergence survey must be conducted with no bats observed. (USFWS & IDNR)
8. Tree AMM 3: Ensure tree removal is limited to that specified in project plans and ensure that contractors understand clearing limits and how they are marked in the field (e.g. install bright colored flagging/fencing prior to any tree clearing to ensure contractors stay within clearing limits). (USFWS, IDNR)
9. Tree AMM 4: Do not remove documented Indiana bat or NLEB roosts that are still suitable for roosting, or trees within 0.25 miles of roosts, or documented forage habitat any time of the year. (USFWS)
- For Further Consideration:**
10. Plant five trees, at least two inches in diameter at breast height for each tree that is removed that is 10 inches or greater in diameter at breast height. (IDNR)

Indiana Department of Transportation

County Jay Route Blaine Pike Des. No. 1600965

SECTION K- EARLY COORDINATION

Please list the date coordination was sent and all agencies that were contacted as a part of the development of this Environmental Study. Also, include the date of their response or indicate that no response was received. INDOT and FHWA are automatically considered early coordination participants and should only be listed if a response is received.

Remarks: Early coordination was initiated on June 11, 2018 with applicable federal, state, and local agencies. A copy of the outgoing early coordination letter is included in Appendix C-1 to C-3. The agencies that were contacted and the date on which they replied are identified below.

Early Coordination Recipients	Response Received	Appendix
Indiana Department of Transportation - Environmental Services	June 13, 2018	C-6
Indiana Geological Survey	September 13, 2018	C-7
Indiana Department of Natural Resources – Fish & Wildlife	July 6, 2018	C-10
Indiana Department of Transportation - Aviation	June 25, 2018	C-11
Indiana Department of Environmental Management	June 12, 2018	C-12
Indiana Department of Environmental Management - Groundwater	June 12, 2018	C-16
USFWS IPaC Species List	July 19, 2019	C-17
USFWS IPaC Concurrence Verification	July 21, 2019	C-23
Natural Resources Conservation Service	May 26, 2020	C-38
Federal Highway Administration	No Response	
National Parks Service	No Response	
U.S. Army Corps of Engineers - Louisville District	No Response	
Indiana Department of Transportation - Public Hearings	No Response	
Norfolk Southern Corporation	No Response	
Jay County Community Development	No Response	
Jay County Highway Engineer	No Response	
Mayor of Portland	No Response	

Appendix A:

**INDOT Supporting
Documents**

Categorical Exclusion Level Thresholds

	PCE	Level 1	Level 2	Level 3	Level 4 ¹
Section 106	Falls within guidelines of Minor Projects PA	“No Historic Properties Affected”	“No Adverse Effect”	-	“Adverse Effect” Or Historic Bridge involvement ²
Stream Impacts	No construction in waterways or water bodies	< 300 linear feet of stream impacts	≥ 300 linear feet of stream impacts	-	Individual 404 Permit
Wetland Impacts	No adverse impacts to wetlands	< 0.1 acre	-	< 1 acre	≥ 1 acre
Right-of-way³	Property acquisition for preservation only or none	< 0.5 acre	≥ 0.5 acre	-	-
Relocations	None	-	-	< 5	≥ 5
Threatened/Endangered Species (Species Specific Programmatic for Indiana bat & northern long eared bat)	“No Effect”, “Not likely to Adversely Affect” (Without AMMs ⁴ or with AMMs required for all projects ⁵)	“Not likely to Adversely Affect” (With any other AMMs)	-	“Likely to Adversely Affect”	Project does not fall under Species Specific Programmatic
Threatened/Endangered Species (Any other species)	Falls within guidelines of USFWS 2013 Interim Policy	“No Effect”, “Not likely to Adversely Affect”	-	-	“Likely to Adversely Affect”
Environmental Justice	No disproportionately high and adverse impacts	-	-	-	Potential ⁶
Sole Source Aquifer	Detailed Assessment Not Required	-	-	-	Detailed Assessment
Floodplain	No Substantial Impacts	-	-	-	Substantial Impacts
Coastal Zone Consistency	Consistent	-	-	-	Not Consistent
National Wild and Scenic River	Not Present	-	-	-	Present
New Alignment	None	-	-	-	Any
Section 4(f) Impacts	None	-	-	-	Any
Section 6(f) Impacts	None	-	-	-	Any
Added Through Lane	None	-	-	-	Any
Permanent Traffic Alteration	None	-	-	-	Any
Coast Guard Permit	None	-	-	-	Any
Noise Analysis Required	No	-	-	-	Yes
Air Quality Analysis Required	No	-	-	-	Yes ⁷
Approval Level <ul style="list-style-type: none"> District Env. Supervisor Env. Services Division FHWA 	Concurrence by INDOT District Environmental or Environmental Services	Yes	Yes	Yes Yes	Yes Yes Yes

¹Coordinate with INDOT Environmental Services. INDOT will then coordinate with the appropriate FHWA Environmental Specialist.

²Any involvement with a bridge processed under the Historic Bridge Programmatic Agreement.

³Permanent and/or temporary right-of-way.

⁴AMMs = Avoidance and Mitigation Measures.

⁵AMMs determined by the IPAC decision key to be needed that are listed in the USFWS *User’s Guide for the Range-wide Programmatic Consultation for Indiana bat and Northern long-eared bat* as “required for all projects”.

⁶Potential for causing a disproportionately high and adverse impact.

⁷Hot Spot Analysis and/or MSAT Quantitative Emission Analysis.

*Substantial public or agency controversy may require a higher-level NEPA document.

Appendix B:

Graphics



Source: Indiana MAP

1:2,250,000

1 in = 187,500 ft

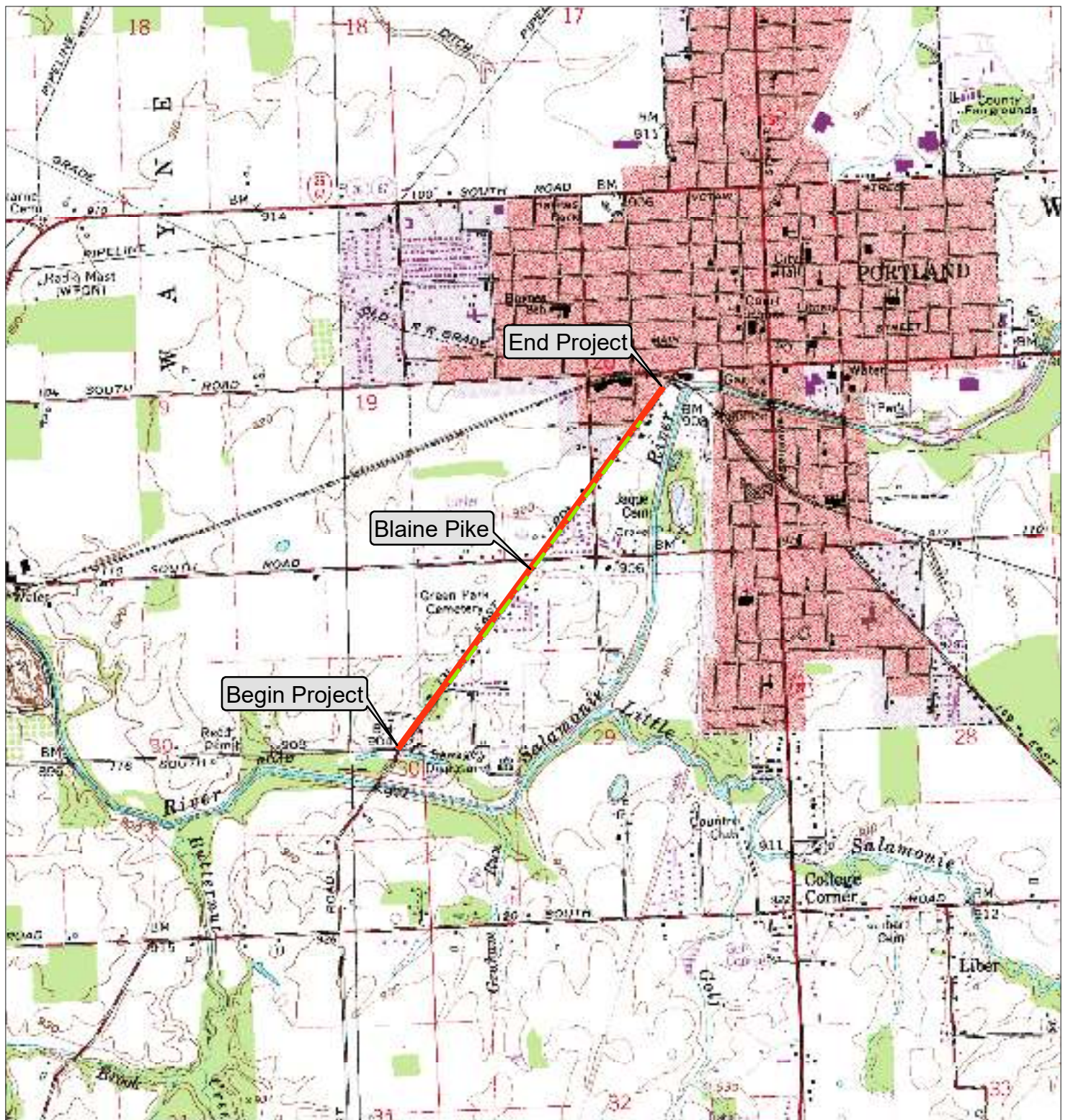
BLN
BEAM LONGEST REEF

Legend

- Project County
- County Boundary

Project Location Map
Road Resurfacing Project
CR 150S to South of Norfolk Southern Railroad Jay County,
Indiana
Des. No. 1600965

May 03, 2018





Source: U.S. Geological Survey

1:24,000

1 in = 2,000 ft



Legend

-  Shared-Use Path
-  Project Alignment

USGS Topographic Map

Road Resurfacing Project

Blaine Pike from Biteersweet Lane to Norfolk Southern Railroad

Jay County, Indiana

Des. No. 1600965

September 24, 2020

P:\170084 Portland Blaine Pike\07Environmental\GIS\Project Graphics\Template - USGS Topo - 11x8.5.mxd | Thursday, September 24, 2020 | 5:16:33 PM



Categorical Exclusion B-2



Source: Indiana MAP
 1:12,000
 1 in = 1,000 ft

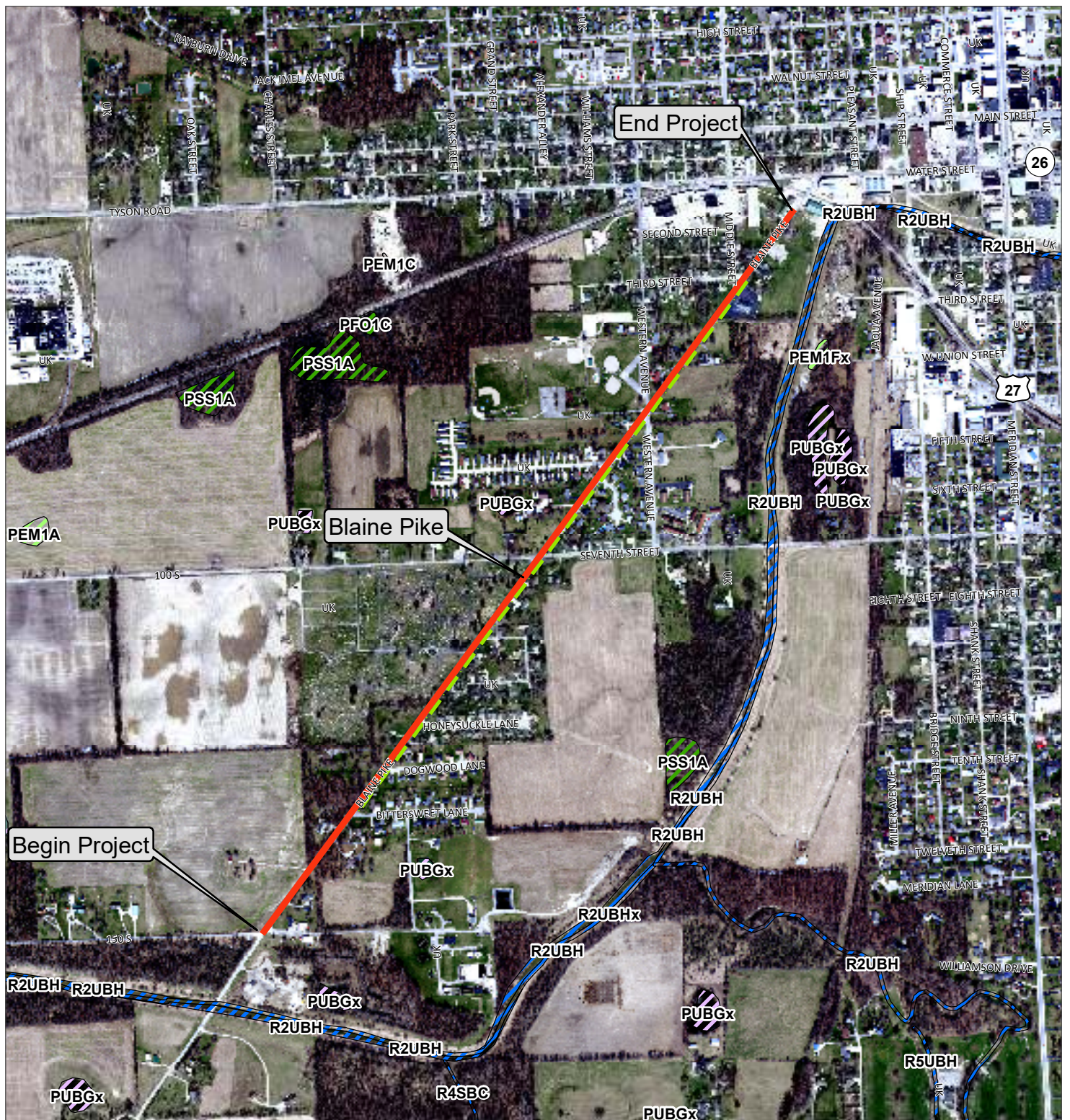


Legend

-  Shared-Use Path
-  Project Alignment

Aerial Map
 Road Resurfacing Project
 Blaine Pike from Bittersweet Lane to Norfolk Southern Railroad
 Jay County, Indiana
 Des. No. 1600965

September 24, 2020



Source: U.S. Fish & Wildlife Service

1:12,000

1 in = 1,000 ft

BLN
BLOOMINGTON LAND NATURE

Legend

- | | | | |
|--|-----------------------------------|---|-------------------|
|  | Freshwater Pond |  | Shared-Use Path |
|  | Freshwater Forested/Shrub Wetland |  | Project Alignment |
|  | Freshwater Emergent Wetland | | |
|  | Riverine | | |

National Wetlands Inventory Map
 Road Resurfacing Project
 Blaine Pike from Bittersweet Lane to Norfolk Southern Railroad
 Jay County, Indiana
 Des. No. 1600965

September 24, 2020

P:\170084 Portland Blaine Pike\07Environmental\GIS\Project Graphics\Template - National Wetlands Inventory Map - 11x8.5.mxd | Thursday, September 24, 2020 | 5:13:40 PM

Categorical Exclusion B-4








Source: Indiana Department of Natural Resources

1:12,000

1 in = 1,000 ft

BLN
TOWN OF BLAINE

Legend

-  Floodway
-  1 % Annual Chance Flood Hazard
-  0.2 % Annual Chance Flood Hazard
-  Shared-Use Path
-  Project Alignment

Floodplain Map
Road Resurfacing Project
Blaine Pike from Bittersweet Lane to Norfolk Southern Railroad
Jay County, Indiana
Des. No. 1600965

September 24, 2020

P:\170084 Portland Blaine Pike\07Environmental\GIS\Project Graphics\Template - Floodplain Map - 11x8.5.mxd | Thursday, September 24, 2020 | 5:08:38 PM

Categorical Exclusion B-5



1. View Looking Southwest along Blaine Pike at CR 150 South, the South Terminus



2. View Looking West along CR 150 South at Blaine Pike



3. View Looking Southwest along Blaine Pike at Bittersweet Lane



View 4. Looking Downstream at Salamonie River, the drainage outlet for the Project



5. View Looking Northeast along Blaine Pike at South Terminus, Bittersweet Lane



6. View Looking Northeast along Blaine Pike at Green Park Cemetery South Entrance



7. View Looking Southwest along Blaine Pike, North of Seventh Street (note curbed parking lane on east side of road)



8. View Looking Northeast along Blaine Pike, North of Seventh Street



9. View Looking Northeast along Blaine Pike at Sesame Street Intersection (note curbed parking lane on left)



10. View Looking North on Western Avenue across Blaine Pike



11. View Looking Northeast along Blaine Pike at Drainage Culvert Located Southwest of Third Street



12. View Looking Northeast on Blaine Pike at North Terminus / Norfolk Southern Railroad



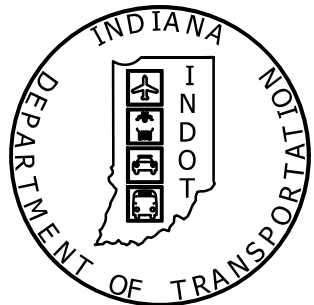
13. View Looking Southwest along Blaine Pike North of the Norfolk Southern Rail Crossing



14. View Looking East along the Norfolk Southern Rail Crossing of Blaine Pike

PROJECT	DESIGNATION
1600965	1600965
CONTRACT	BRIDGE FILE
R-40319	N.A.

INDIANA DEPARTMENT OF TRANSPORTATION



BLAINE PIKE PEDESTRIAN SYSTEM IMPROVEMENTS CITY OF PORTLAND, JAY COUNTY, INDIANA

PROJECT DESCRIPTION:

THE PROPOSED PROJECT INCLUDES THE CONSTRUCTION OF A SHARED-USE PATH, CROSSWALKS, CURBS, AND RAMPS, AND HMA OVERLAY AND PREVENTIVE MAINTENANCE ALONG BLAINE PIKE FROM C.R. 150 S. TO 131 FEET SOUTH OF NORFOLK & SOUTHERN RR ALL IN SECTIONS 20, 29 & 30, T-23-N, R-14-E, WAYNE TOWNSHIP, JAY COUNTY, INDIANA

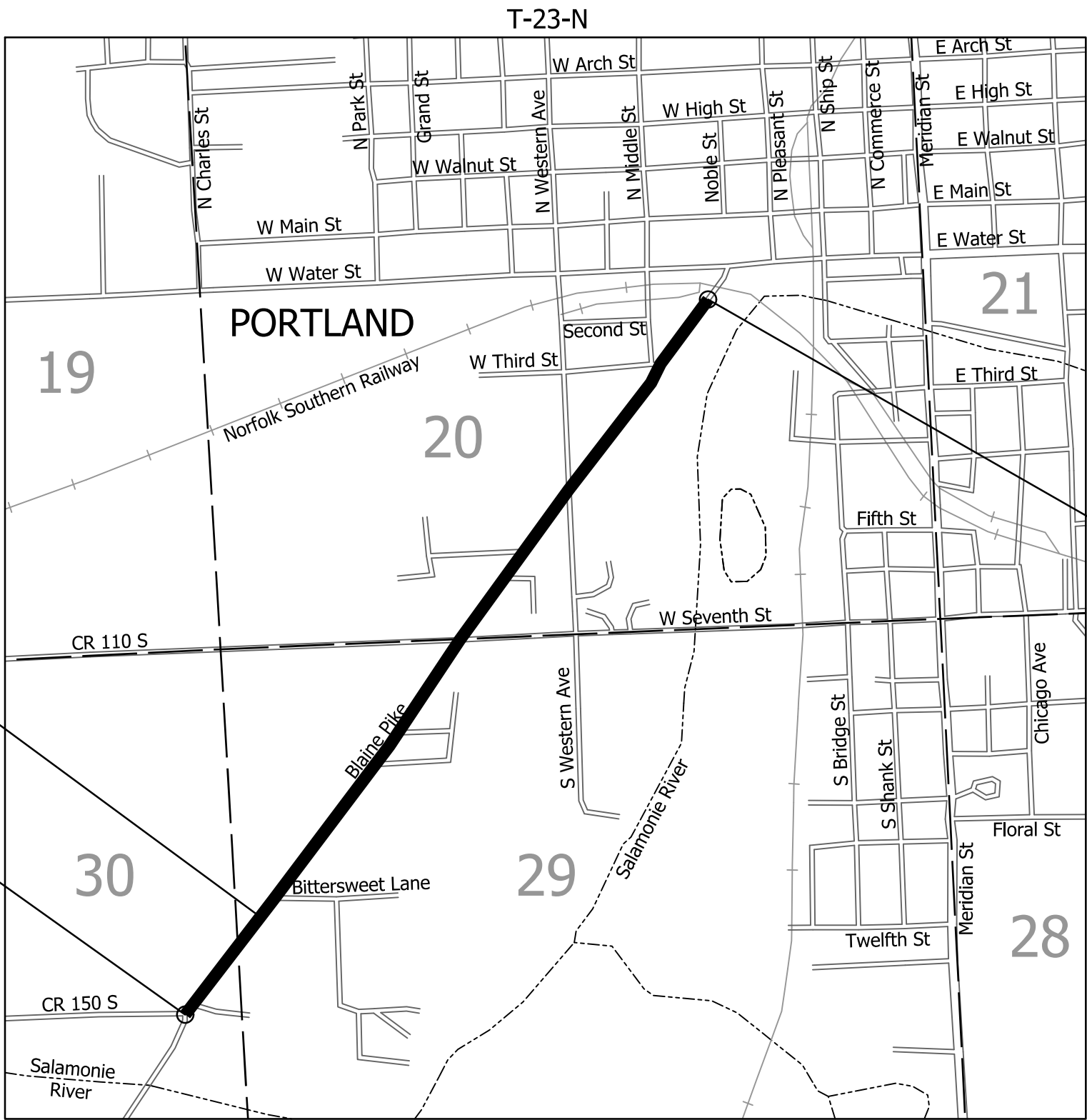
CITY OF PORTLAND

MAYOR RANDY GEESAMAN

Project Length: 1.211 MI.
Max. Path Grade: 4.13%

P.O.T. 10+00.00 "PR-X" (Back) =
P.O.T. 10+00.00 "X" (Ahead)

BEGIN PROJECT 1600965
STA. 2+50 "PR-X"

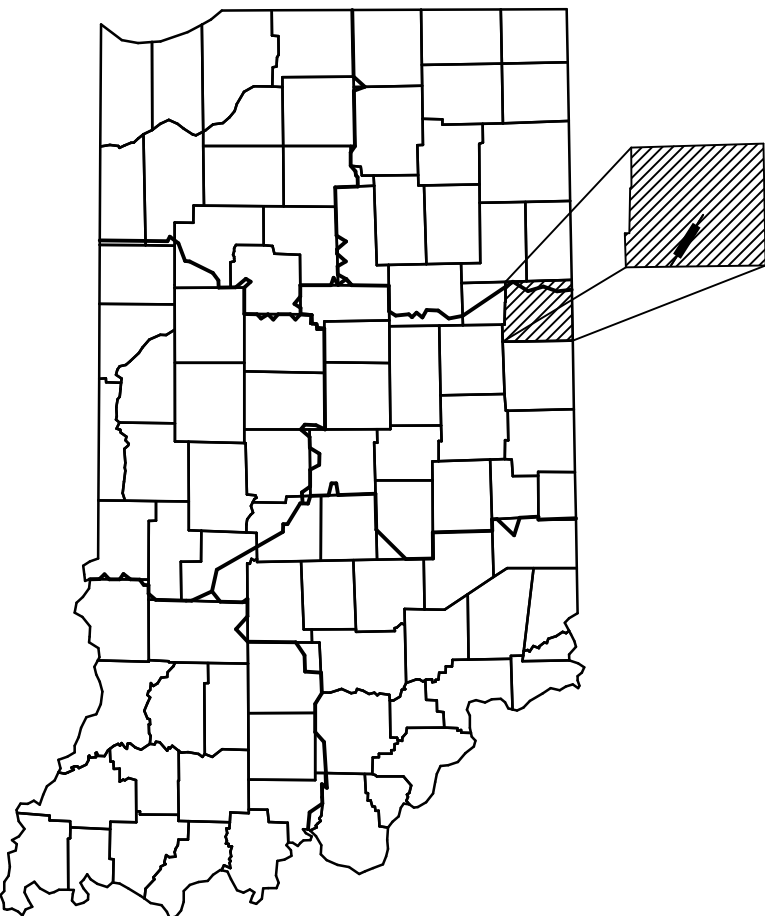


N

SCALE: 1" = 1000'

END PROJECT 1600965
STA. 66+44 "X"

TRAFFIC DATA	BLAINE PIKE
A.A.D.T. (2022)	1,825 V.P.D.
A.A.D.T. (2042)	2,196 V.P.D.
D.H.V (2042)	220 V.P.H.
DIRECTIONAL DISTRIBUTION	51/49 %
TRUCKS	4.5 % A.A.D.T. 7 % D.H.V.
DESIGN DATA	BLAINE PIKE
DESIGN SPEED	30 M.P.H.
PROJECT DESIGN CRITERIA	PARTIAL 3R (NON-FREEWAY)
FUNCTIONAL CLASSIFICATION	MINOR ARTERIAL
RURAL/URBAN	URBAN (SUBURBAN)
TERRAIN	LEVEL
ACCESS CONTROL	NONE



PROJECT LOCATION SHOWN BY

LATITUDE: N 40° 25' 29" LONGITUDE: W 84° 59' 24"

H.U.C.: 05120102010030 & 05120102010050

PRELIMINARY PLANS
SEPTEMBER 9, 2020

INDIANA DEPARTMENT OF TRANSPORTATION
STANDARD SPECIFICATIONS DATED 2020
TO BE USED WITH THESE PLANS

BLN:
BEAM·LONGEST·NEFF

PLANS
PREPARED BY: Beam, Longest & Neff, L.L.C. (317) 849-5832
PHONE NUMBER

CERTIFIED BY: _____ DATE _____

APPROVED
FOR LETTING: _____ DATE _____

INDIANA DEPARTMENT OF TRANSPORTATION

BRIDGE FILE	
N.A.	
DESIGNATION	
1600965	
SURVEY BOOK	SHEETS
ELECTRONIC	1 of 128
CONTRACT	PROJECT
R-40319	1600965

9/9/2020 1:9/9/2020 1:802:45 AM I P:\170084 Portland Blaine Pike\09Road_Indiana\04 Design files\DCGN\Plans\170084_02_Sht_Index.dgn

UTILITIES

INDOT PM:
Susan Mustard
Email: smustard@indot.in.gov

Jay County Contact:
Ami Huffman
Email: ahuffman@jaycodev.org

ELECTRIC:
American Electric Power
Indiana Michigan Power
5000 Wheeling Avenue
Muncie, IN 47304
Attn: Robert Astrop
Ph: 765-287-3382
Email: rdastrop@aep.com

TELEPHONE:
CenturyLink/Mountain
50 N. Jackson Street
Franklin, IN 46131
Attn: Eric Flory
Ph: 419-497-2045
Email: eric.flory@centurylink.com


CABLE:
Comcast
720 Taylor Street
Fort Wayne, IN 46802
Attn: William (Doug) Fishburn
Ph: 260-410-3504
Email: rhonda_dalton@comcast.com

GAS:
Ohio Valley Gas Corp.
111 Energy Park Drive
P.O. Box 469
Winchester, IN 47394
Attn: Damon Breen
Ph: 765-584-6842 ext. 110
Email: dbreen@ovgc.com

WATER:
Portland Water
205 South Wayne Street
Portland, IN 47371
Attn: Doug Jackson
Ph: 260-726-4525
Fax: 260-726-2763
Email: djackson@thecityofportland.net

SANITARY:
Portland Wastewater
1315 Shadeland Lane
Portland, IN 47371
Attn: Brad Clayton
Ph: 260-703-1073
Email: bclayton@thecityofportland.net

INDIANA UNDERGROUND PLANT PROTECTION SERVICE, INC.



Know what's below.
Call before you dig.

Per Indiana State Law IC-8-1-26-16, It is against the law to excavate without notifying the underground location service two (2) full working days before commencing work.

INDIANA UNDERGROUND
1-800-382-5544 OR CALL 811
24 HOURS A DAY 7 DAYS A WEEK

Note: Utility Locations are shown based upon information (maps and paint marks) supplied by others, and there is no guarantee of the accuracy or completeness of said locations.

REVISIONS		
SHEET NO.	DATE	REVISED

GENERAL NOTES	
	All earth shoulders, median areas, and cut and fill slopes shall be plain or mulch seeded except where sodding is specified.
	The final cross sections of the grading contract will be the original cross sections of the paving contract. However, partial or complete cross sections shall be taken if necessary to determine the actual excavation quantities.
	The paper relocation will be cross sectioned by the Engineer before construction.
	Existing asphalt pavement located outside the construction limits, between Sta. ----- and Sta -----, shall be removed as directed.
	The quantity of peat excavation shown on the plans has been estimated on the basis of theoretical cross sections by using treatment of existing fills, treatment by removal, or treatment by displacement, where each treatment applies.
**	<p>Soils under the proposed shared-use path may become unstable during construction when exposed to precipitation and construction traffic. In such an event, the foundation material shall be improved by one of two alternatives.</p> <p>Alternate 1: Excavate foundation soil 12" below proposed subgrade treatment and replace with No. 53 aggregate over Geotextile, Type 2B.</p> <p>Alternate 2: Stabilize foundation soil below proposed subgrade treatment to a depth of 8" using chemical modification treatment.</p> <p>An estimated quantity of 7739 Sys of Foundation Soil Improvement is planned on the entire shared-use path subgrade treatment area. However, Foundation Soil Improvement will be at the direction of the Engineer at the time of construction.</p>

** REPRESENTS GENERAL NOTES REQUIRED

INDEX	
SHEET NO.	DRAWINGS INDEX
1	TITLE SHEET
2	INDEX SHEET
3	TYPICAL CROSS SECTIONS
4 - 6	PLAT NO. 1
7 - 8	MAINTENANCE OF TRAFFIC
9 - 21	PLAN SHEETS
22 - 31	CONSTRUCTION DETAILS
32 - 35	CURB RAMP DETAILS
36 - 37	MISCELLANEOUS DETAILS
38	APPROACH TABLE
39	STRUCTURE DATA TABLE
40 - 128	CROSS SECTIONS

LEGEND

- (C1)

PCCP for Approaches, 6" on Subgrade Treatment, Type II
- (CR)

Curb Ramp, Concrete
- (D1)

HMA for Approaches, Type B
165 #/Sys HMA Surface, Type B on
275 #/Sys HMA Intermediate, Type B on
6" Compacted Aggregate No. 53 on
Subgrade Treatment, Type II
- (F1)

HMA for Sidewalk consisting of:
140 #/Sys HMA Surface, Type B on
220 #/Sys HMA Intermediate, Type B on
6" Compacted Aggregate, No. 53 on
Subgrade Treatment, Type III
- (F2)

Sidewalk, Concrete, 4"
- (N)

Compacted Aggregate for Surface, No. 73
- (O)

Compacted Aggregate, No. 53
- (R)

1.5" Asphalt Milling, before
HMA Overlay consisting of:
165 #/Syd QC/QA HMA Surface, X, X, 9.5 mm
- (14)

Integral Concrete Curb
- (15)

Concrete Curb and Gutter
- (16)

Rolled Curb
- (26)

Nursery Sodding
- (29)

Mulched Seeding, U
- (41)

"PED XING" Pavement Message Marking, Thermoplastic
- (42)

Transverse Markings, Thermoplastic, Stop Line, White, 24 in.
- (43)

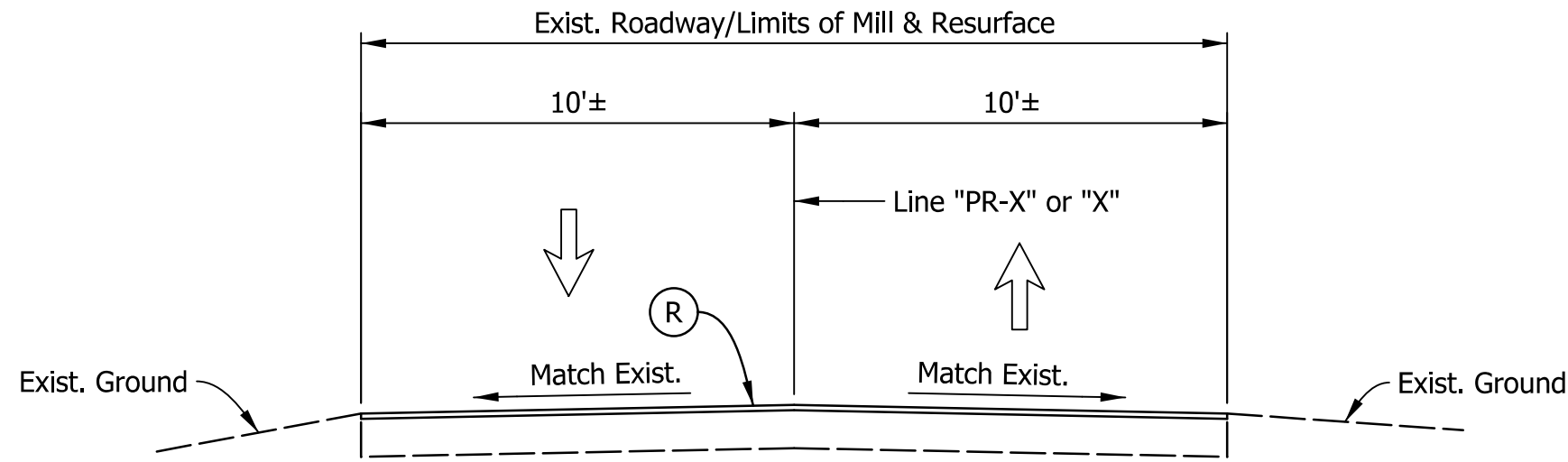
Transverse Marking, Thermoplastic, Crosswalk Line, White, 6 in.
- (44)

Transverse Marking, Thermoplastic, Crosswalk Line, White, 24 in.
- (45)

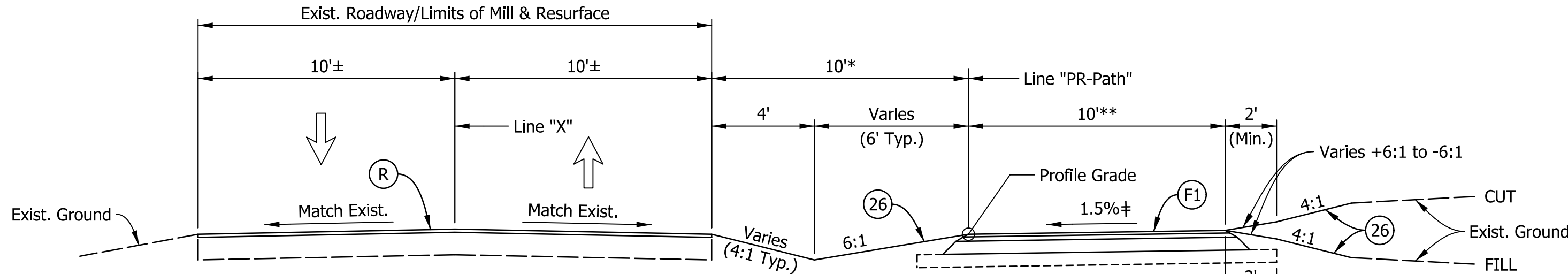
Line, Thermoplastic, Solid, White, 4 in.
- (46)

Line, Thermoplastic, Solid, Yellow, 4 in.

9/9/2020 1:9/9/2020 1:8:02:55 AM I:\P\170084 Portland Blaine Pike\09Road_Indiana\04 Design files\DCN\Plans\170084_03_Sht_Typical_01.dgn



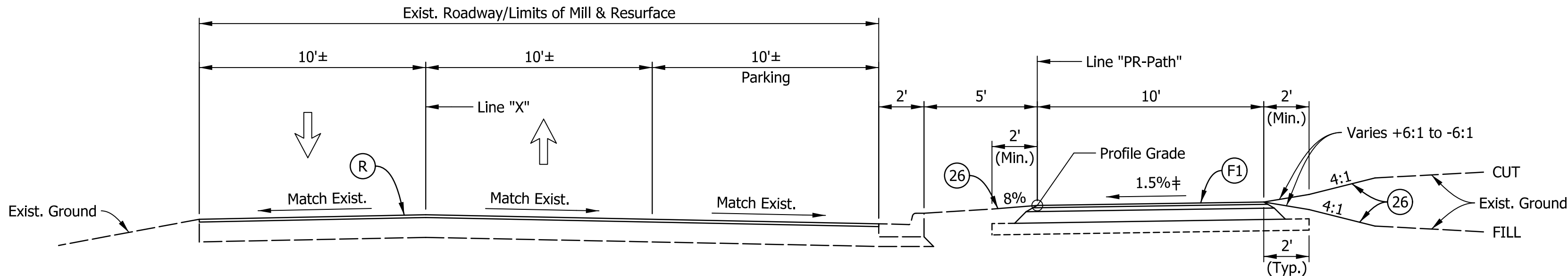
TYPICAL SECTION
Sta. 2+50 "PR-X" to Sta. 10+00 "PR-X"
Sta. 10+00 "X" to Sta. 12+80 "X"



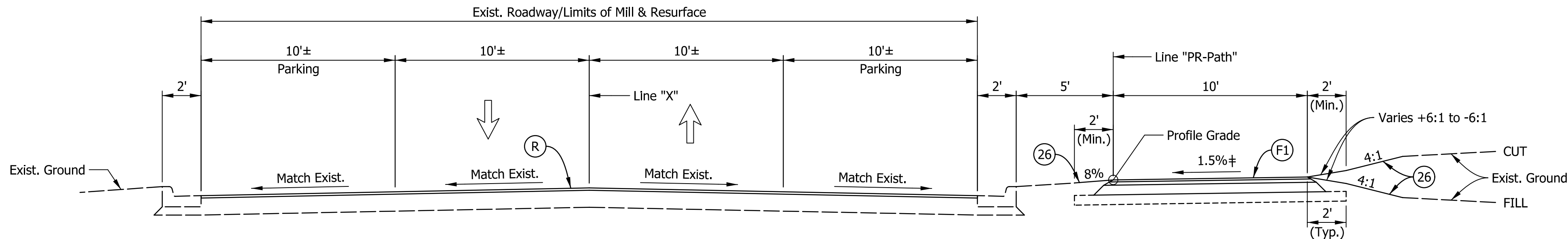
TYPICAL SECTION
Sta. 12+80 "X" to Sta. 36+76 "X"
Sta. 47+46 "X" to Sta. 66+44 "X"

* Varies from 10' at Sta. 14+04.80 to 4.8' at Sta. 14+72.50. See Plans.
Varies from 4.8' at Sta. 14+72.50 to 10' at Sta. 15+22.37. See Plans.

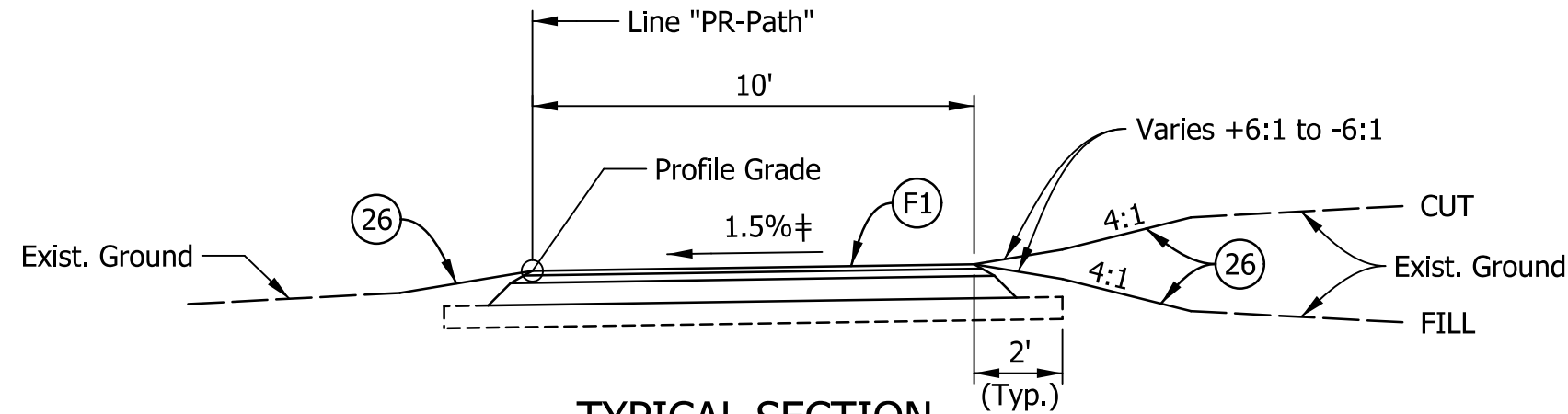
** Varies from 10' at Sta. 14+21.62 to 8' at Sta. 14+52.00. See Plans.
Varies from 8' at Sta. 14+92.94 to 10' at Sta. 15+08.91. See Plans.



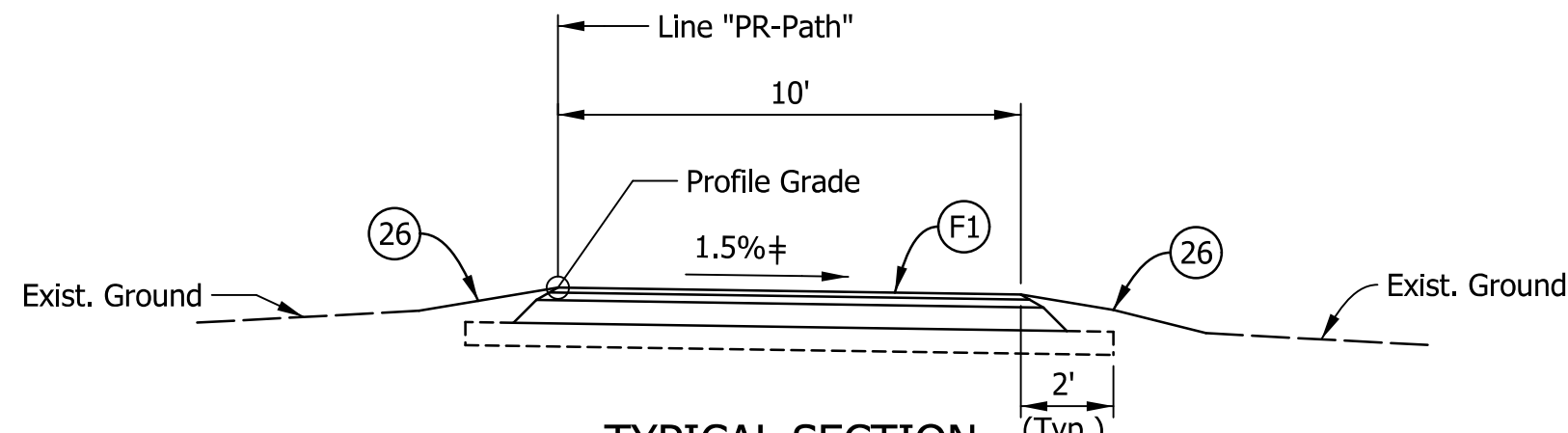
TYPICAL SECTION
Sta. 36+76 "X" to Sta. 41+70 "X"
Sta. 44+10 "X" to Sta. 47+46 "X"



TYPICAL SECTION
Sta. 41+70 "X" to Sta. 44+10 "X"



TYPICAL SECTION
(Path Sloping Toward Road)
Sta. 112+85 "PR-Path" to Sta. 129+70 "PR-Path"
Sta. 135+25 "PR-Path" to Sta. 154+75 "PR-Path"



TYPICAL SECTION
(Path Sloping Away from Road)
Sta. 129+70 "PR-Path" to Sta. 135+25 "PR-Path"
Sta. 154+75 "PR-Path" to Sta. 161+88 "PR-Path"

± Cross Slope may vary from -1.5% to +1.5%. See Cross Sections.
(30' Min. Cross Slope Transition Length)

Note to Reviewer:
Purpose of typical path sections above is
to show information about path cross slope.

Foundation Soil Improvement for Path:

Alternate 1: Excavate foundation soil 12" below proposed subgrade treatment and replace with No. 53 aggregate over Geotextile, Type 2B.

Alternate 2: Stabilize foundation soil below proposed subgrade treatment to a depth of 8" using chemical modification treatment.

LEGEND

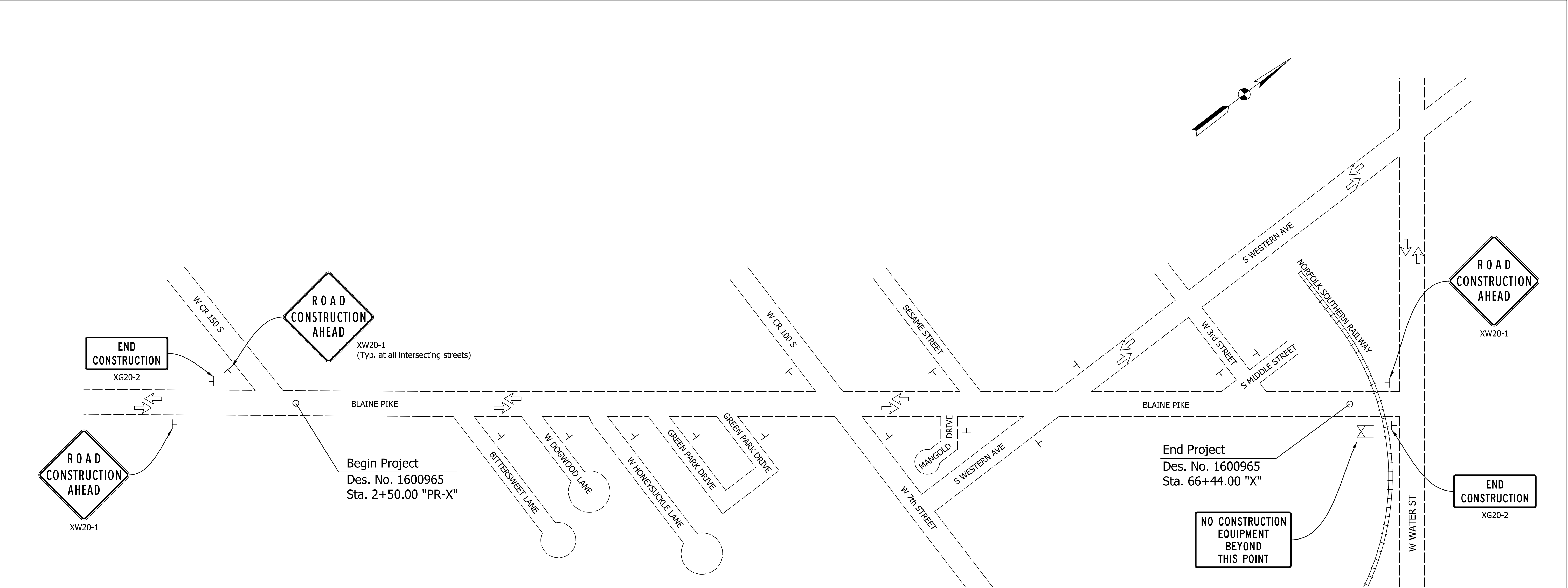
- (F1) HMA for Sidewalk consisting of:
140 #/Sys HMA Surface, Type B on
220 #/Sys HMA Intermediate, Type B on
6" Compacted Aggregate, No. 53 on
Subgrade Treatment, Type III
- (R) 1.5" Asphalt Milling, before
HMA Overlay consisting of:
165 #/Syd QC/QA HMA Surface, X, X, 9.5 mm
- (26) Nursery Sodding

NOTES:

See Plan Sheets for Shared Use Path Location.

Pavement Wedge limits at driveways shall be in accordance with
Standard Drawing E610-DRIV-21.

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SEQUENCE OF OPERATIONS AND GENERAL NOTES

1. Construct Shared Path in accordance with INDOT Std. Dwg. E801-TCTC-05 (Lane Closure) & E801-TCLC-07 (Shoulder Closure).
2. Resurfacing operations under traffic, one half at a time, in accordance with INDOT Std. Dwg. E801-TCFO-01.
3. Flagging operations, including lane closures, to occur during daytime hours only.
4. Access to all businesses and and residences within Project Limits shall remain open at all times.
5. Signs and devices for shoulder and lane closures not shown, and shall be in addition to signs shown on this sheet.

NOTE:
NO CONSTRUCTION EQUIPMENT OR ACTIVITY
ALLOWED BEYOND STA. 66+44 "X".

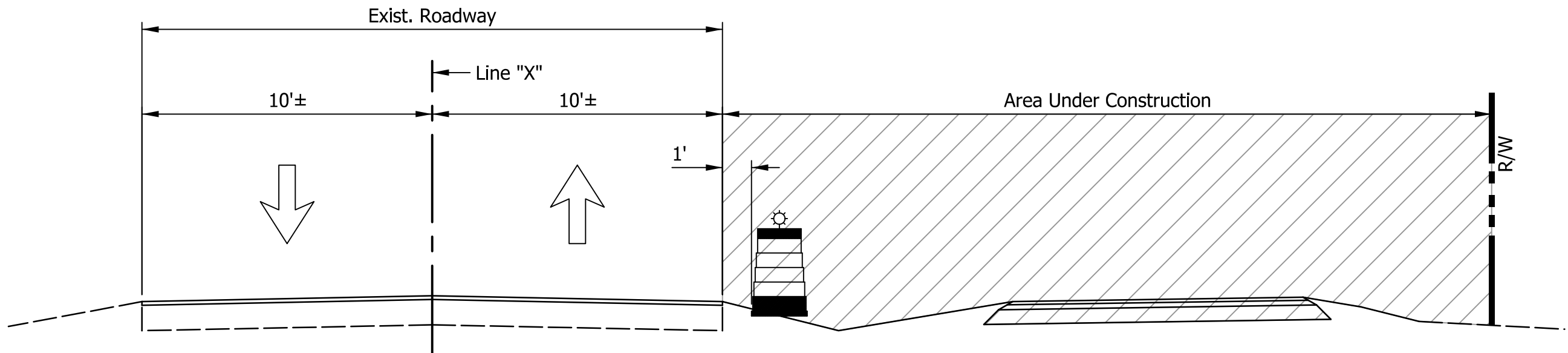
LEGEND

- Construction Sign (XW20-1 unless otherwise noted)
- Direction of Traffic Flow

RECOMMENDED FOR APPROVAL _____	
DESIGN ENGINEER _____	DATE _____
DESIGNED: BTD _____	DRAWN: MAB _____
CHECKED: DJG _____	CHECKED: DJG _____

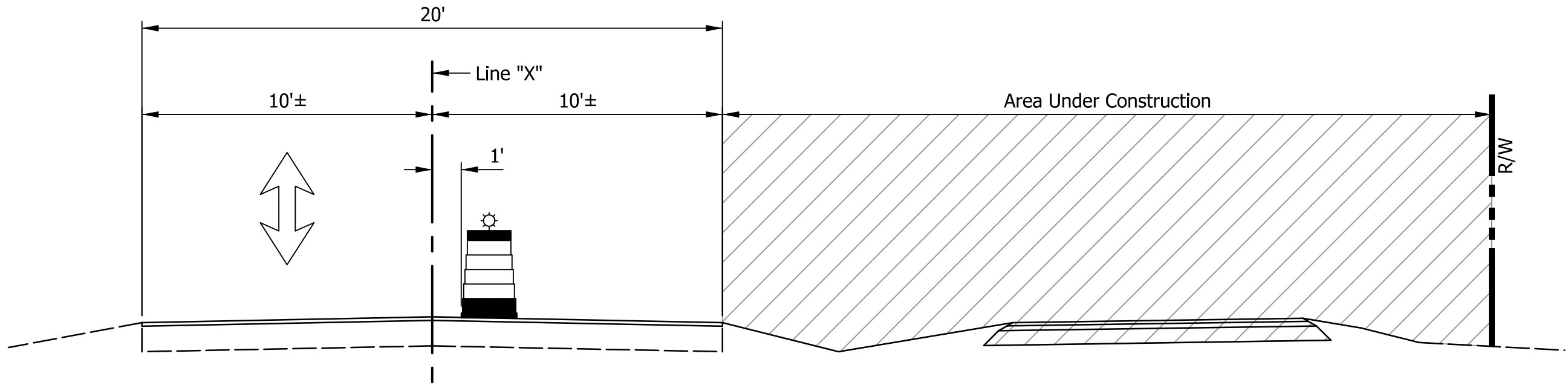
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N.A.	N.A.	
VERTICAL SCALE	DESIGNATION	
N.A.	1600965	
SURVEY BOOK	SHEETS	
ELECTRONIC	7	of 128
CONTRACT	PROJECT	
R-40319	1600965	

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TYPICAL SECTION - SHOULDER CLOSURE

Shared Path Construction
Scale : 1/4" = 1'-0"



TYPICAL SECTION - LANE CLOSURE

Shared Path Construction
Scale: 1/4" = 1'-0"
DAYTIME OPERATIONS ONLY

NOTE:
Flagging Operations Required
See Standard Drawing E801-TCFO-01 for additional details.

RECOMMENDED FOR APPROVAL _____	
DESIGNED: <u>BTB</u>	DRAWN: <u>MAB</u>
CHECKED: <u>DJG</u>	CHECKED: <u>DJG</u>

INDIANA DEPARTMENT OF TRANSPORTATION	
MAINTENANCE OF TRAFFIC DETAILS	

HORIZONTAL SCALE	BRIDGE FILE	
AS NOTED	N.A.	
VERTICAL SCALE	DESIGNATION	
AS NOTED	1600965	
SURVEY BOOK	SHEETS	
ELECTRONIC	8	of 128
CONTRACT	PROJECT	
R-40319	1600965	

9/9/2020 1:9/2020 1:803:01 AM I P:\170084 Portland Blaine Pike\09Road_Indiana\04 Design files\CGN\Plans\170084_07_Sht_PlanProfile_X_01.dgn



RECOMMENDED FOR APPROVAL _____		DESIGN ENGINEER _____		DATE _____	
DESIGNED: BTD		DRAWN: MAB			
CHECKED: DJG		CHECKED: DJG			

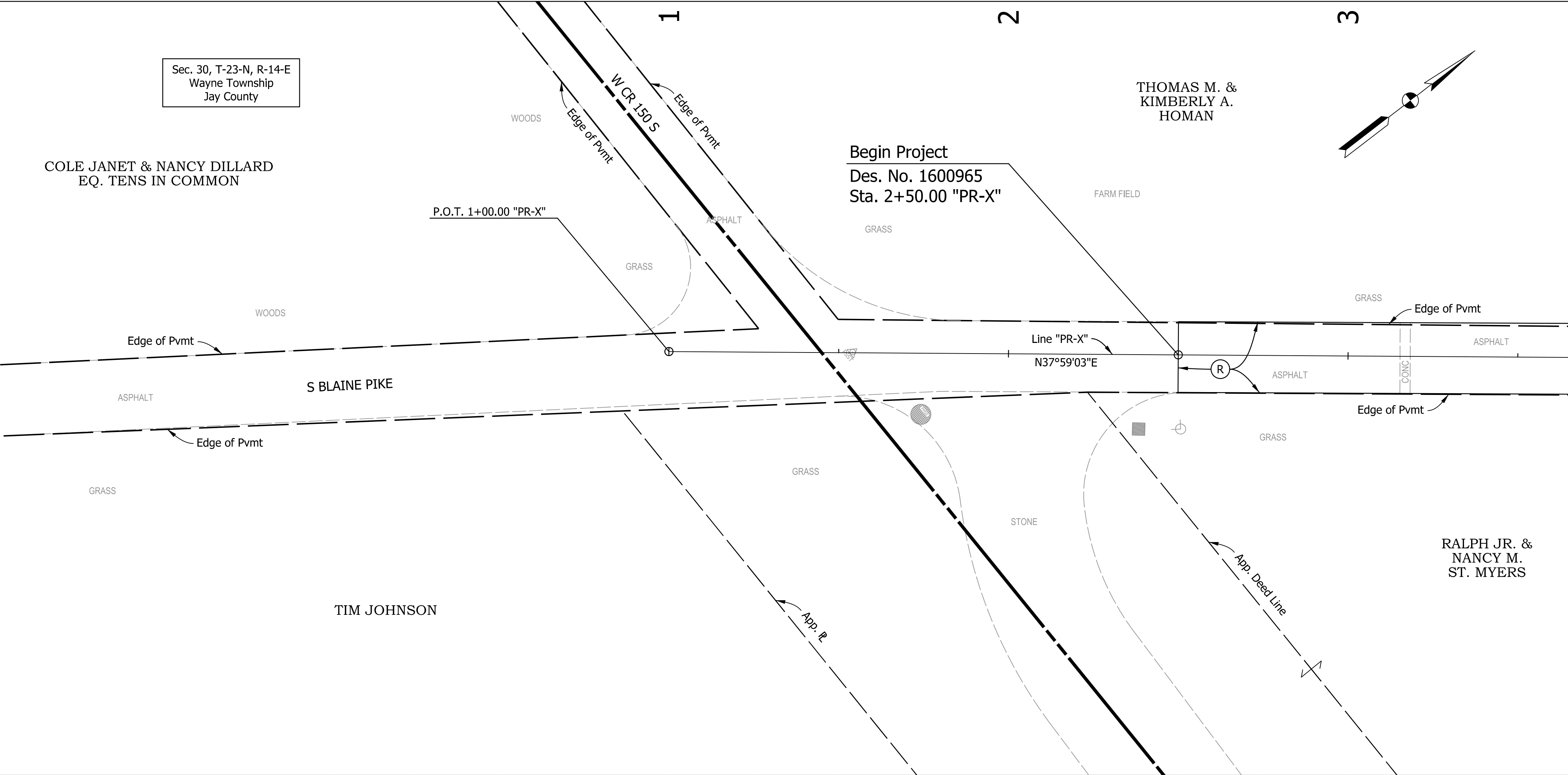
INDIANA
DEPARTMENT OF TRANSPORTATION

PLAN & PROFILE
LINE "PR-X"

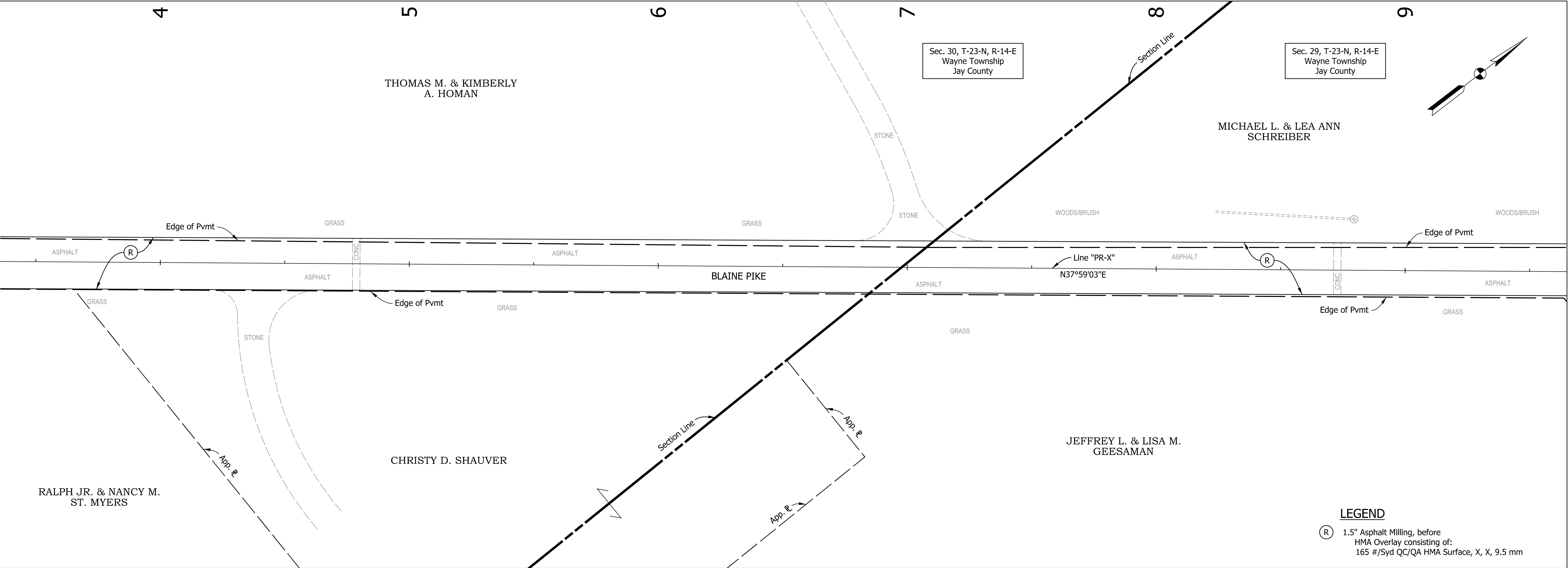
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VERTICAL SCALE 1" = 5'		DESIGNATION 1600965	
SURVEY BOOK ELECTRONIC		SHEETS 9 of 128	
CONTRACT R-40319		PROJECT 1600965	

LEGEND

- (R) 1.5" Asphalt Milling, before
HMA Overlay consisting of:
165 #/Syd QC/QA HMA Surface, X, X, 9.5 mm

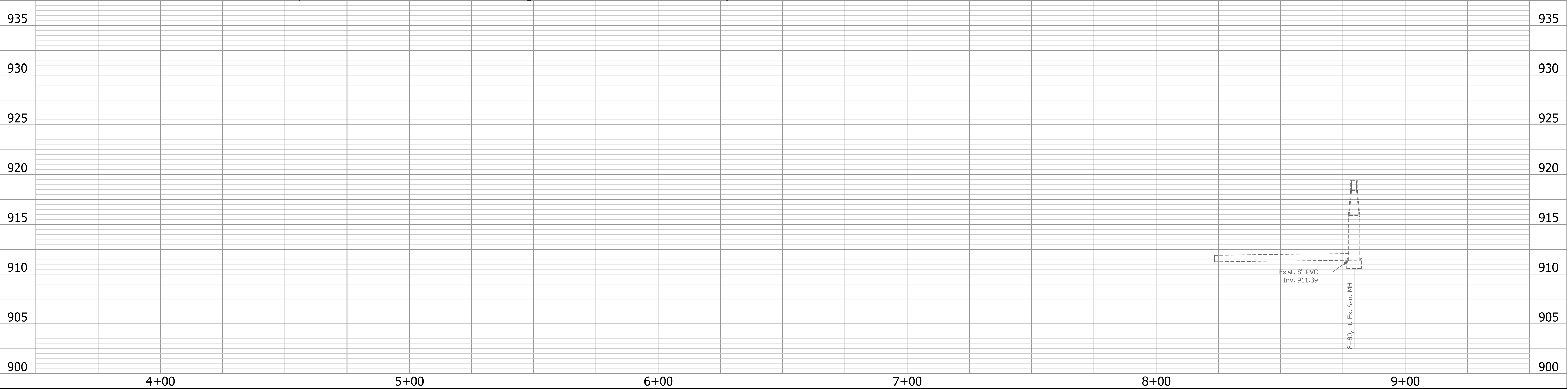


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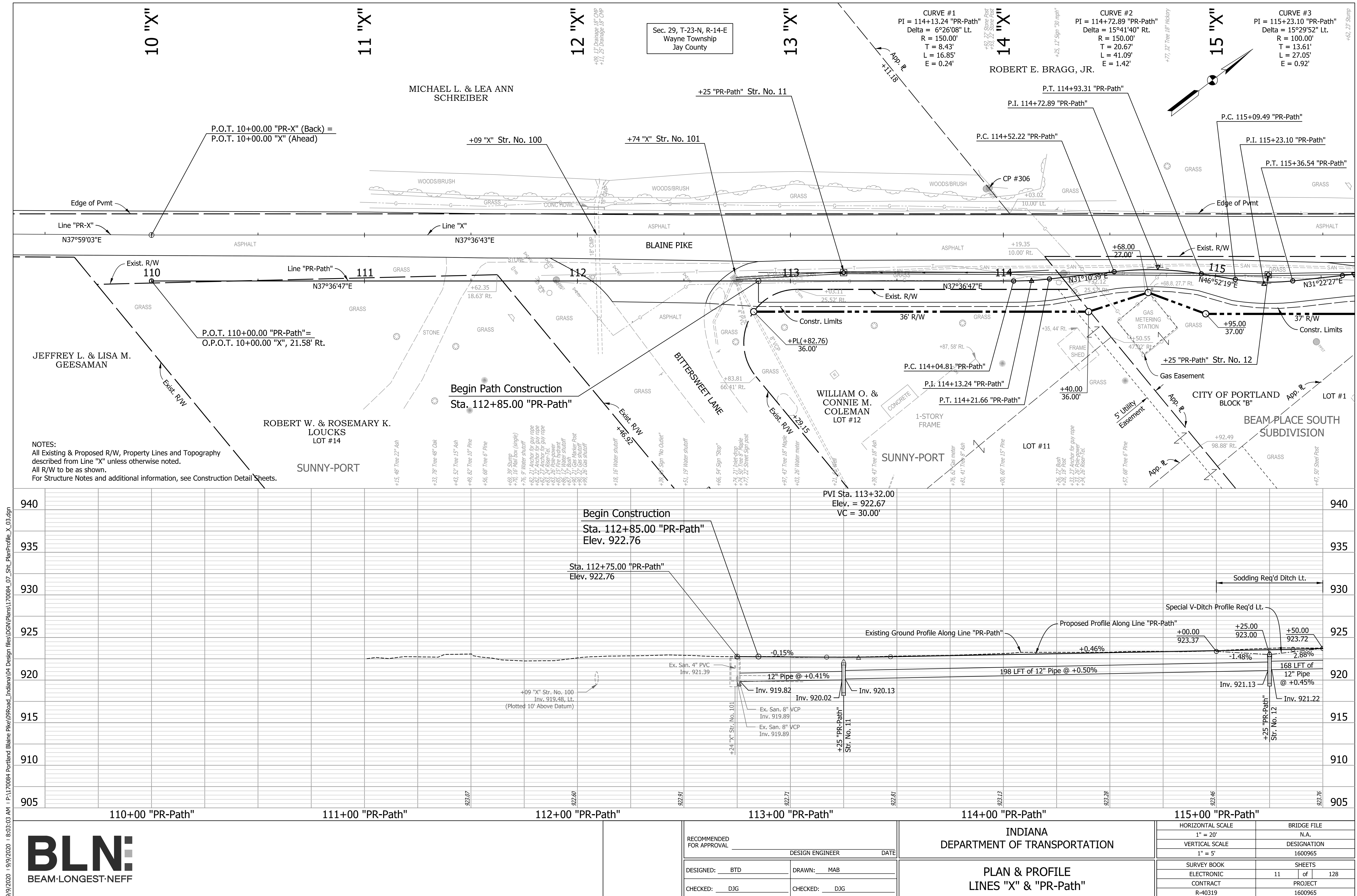


LEGEND

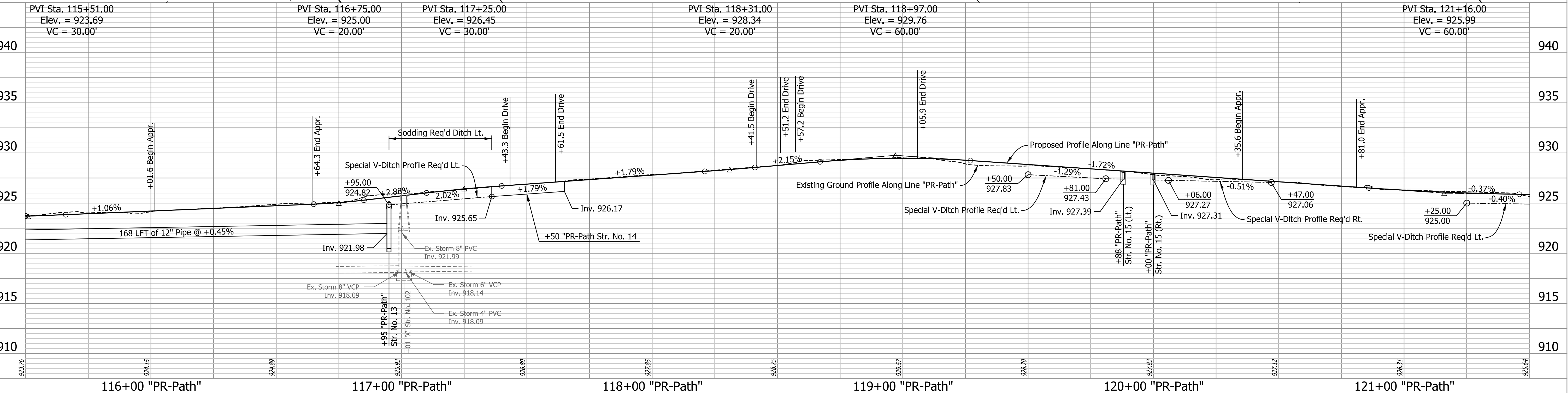
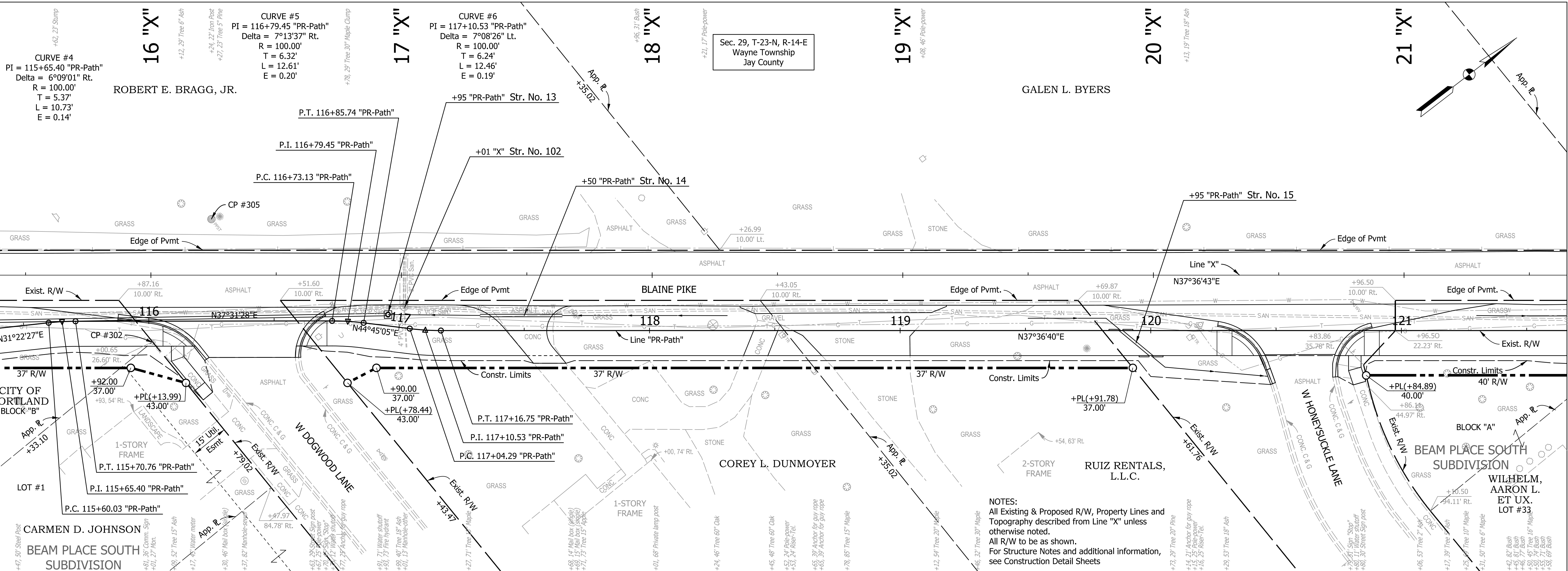
(R) 1.5" Asphalt Milling, before
HMA Overlay consisting of:
165 #/Syd QC/QA HMA Surface, X, X, 9.5 mm



<div>BLN: BEAM·LONGEST·NEFF</div>	RECOMMENDED FOR APPROVAL _____		DESIGN ENGINEER _____		DATE _____		INDIANA DEPARTMENT OF TRANSPORTATION		HORIZONTAL SCALE 1" = 20'		BRIDGE FILE N.A.	
									VERTICAL SCALE 1" = 5'		DESIGNATION 1600965	
	DESIGNED: <u> BTD </u>		DRAWN: <u> MAB </u>				PLAN & PROFILE LINE "PR-X"		SURVEY BOOK		SHEETS	
					ELECTRONIC				10 of 128			
	CHECKED: <u> DJG </u>		CHECKED: <u> DJG </u>		CONTRACT				PROJECT			
				R-40319		1600965						



9/9/2020 1:03:05 AM I:\170084 Portland Blaine Pike\09Road_Plan\170084_07_Sht_PlanProfile_X_04.dgn



BLN BEAM·LONGEST-NEFF	RECOMMENDED FOR APPROVAL _____ DESIGN ENGINEER _____ DATE _____ DESIGNED: BTD DRAWN: MAB CHECKED: DJG CHECKED: DJG		INDIANA DEPARTMENT OF TRANSPORTATION PLAN & PROFILE LINES "X" & "PR-Path"		HORIZONTAL SCALE 1" = 20'		BRIDGE FILE N.A.	
					VERTICAL SCALE 1" = 5'		DESIGNATION 1600965	
					SURVEY BOOK ELECTRONIC		SHEETS 12 of 128	
					CONTRACT R-40319		PROJECT 1600965	

9/9/2020 1:03:06 AM I:\170084 Portland Blaine Pike\09Road_Plan\04 Design files\CGWPlans\170084_07_Sit_Plan\Profile_X_05.dgn



NOTES:
All Existing & Proposed R/W, Property Lines
and Topography described from Line "X"
unless otherwise noted.
All R/W to be as shown.
For Structure Notes and additional information,
see Construction Detail Sheets.

22 "X"

23 "X"

24 "X"

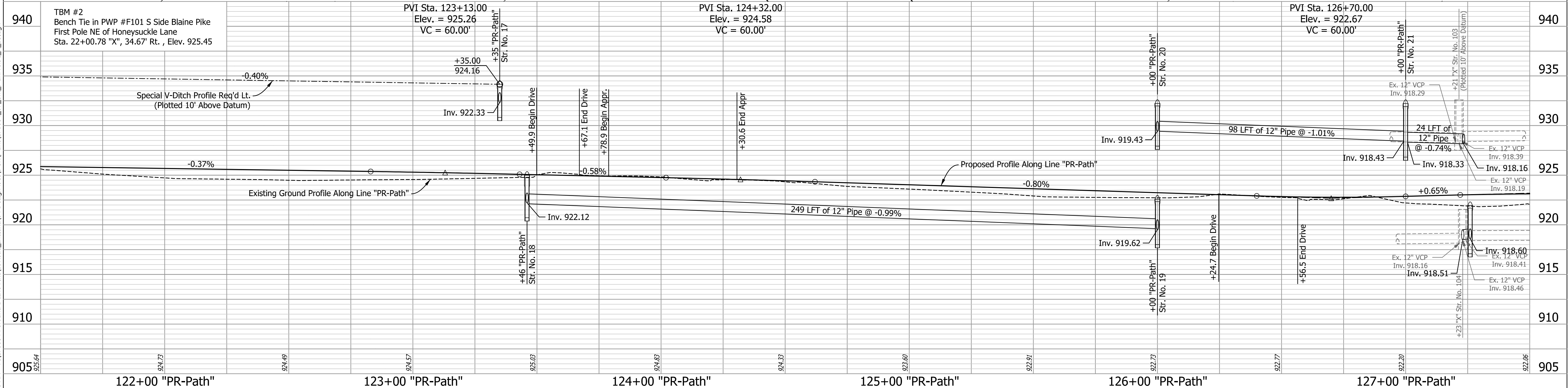
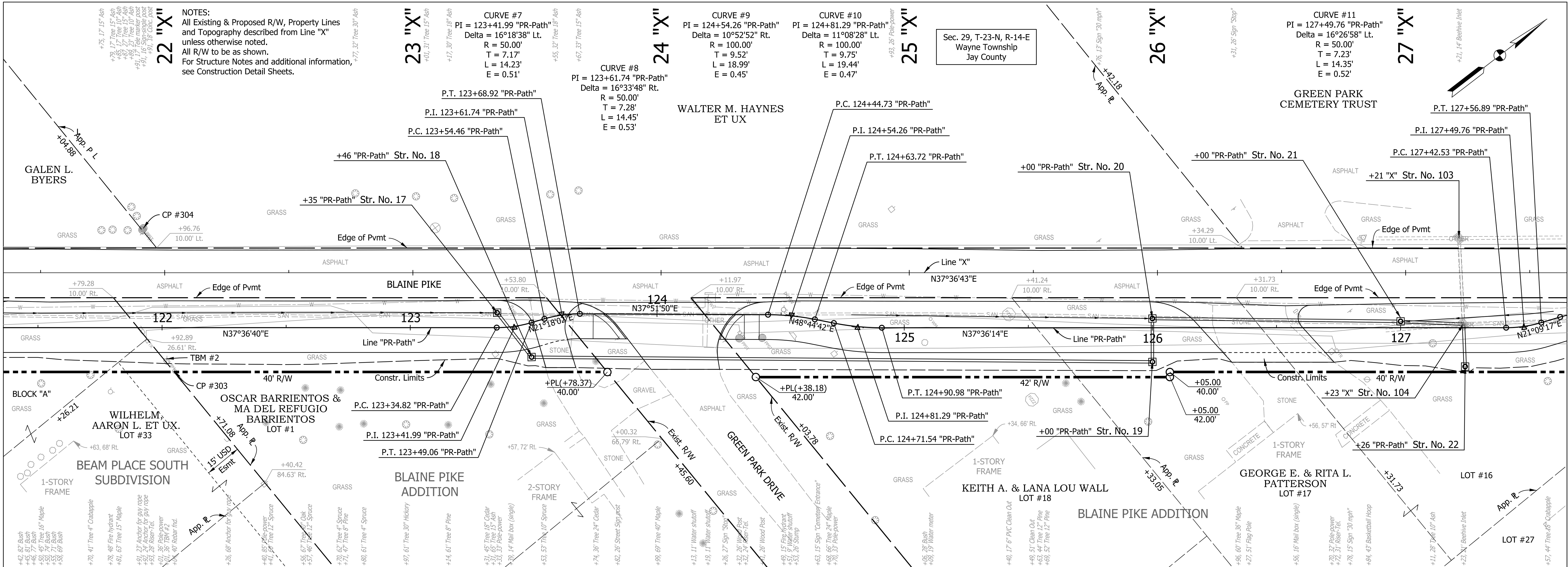
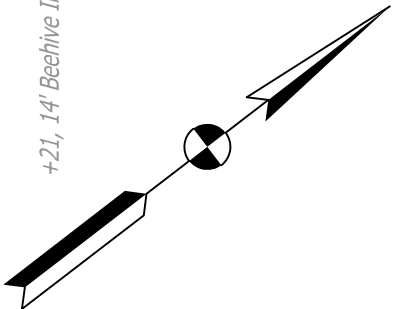
25 "X"

26 "X"

27 "X"

Sec. 29, T-23-N, R-14-E
Wayne Township
Jay County

GREEN PARK
CEMETERY TRUST



RECOMMENDED FOR APPROVAL		DESIGN ENGINEER		DATE	
DESIGNED: BTD		DRAWN: MAB			
CHECKED: DJG		CHECKED: DJG			

INDIANA
DEPARTMENT OF TRANSPORTATION

PLAN & PROFILE
LINES "X" & "PR-Path"

HORIZONTAL SCALE		BRIDGE FILE	
1" = 20'		N.A.	
VERTICAL SCALE		DESIGNATION	
1" = 5'		1600965	
SURVEY BOOK		SHEETS	
ELECTRONIC		13 of 128	
CONTRACT		PROJECT	
R-40319		1600965	

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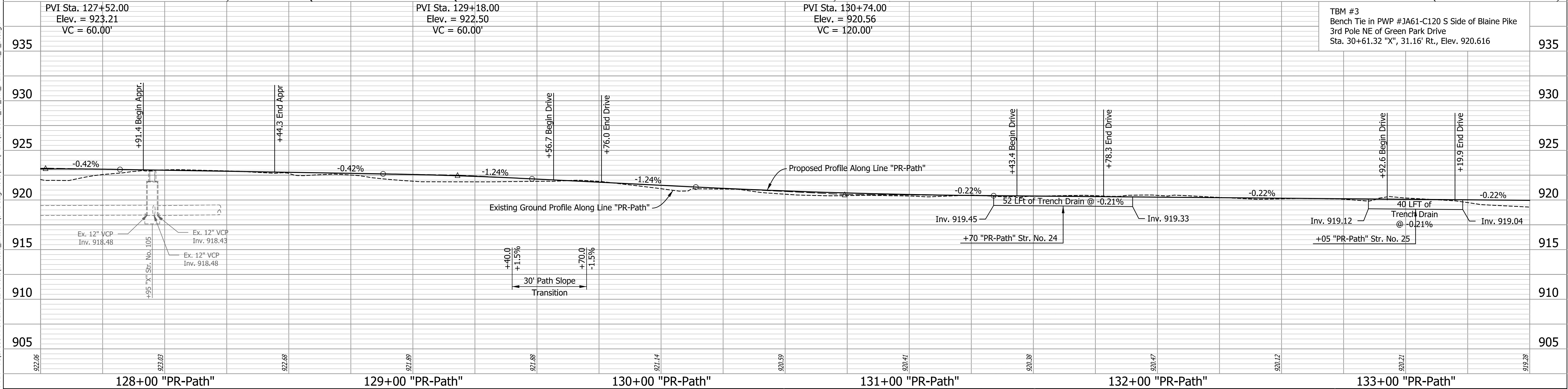
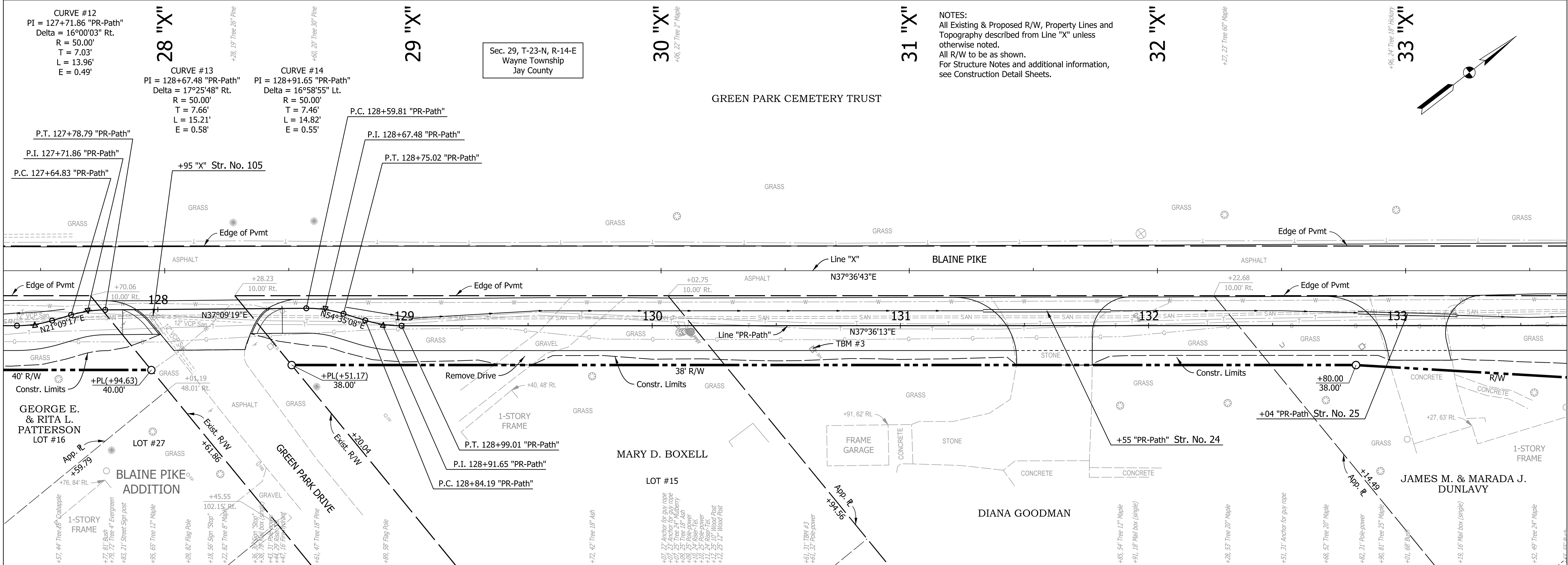
CURVE #12
PI = 127+71.86 "PR-Path"
Delta = 16°00'03" Rt.
R = 50.00'
T = 7.03'
L = 13.96'
E = 0.49'

CURVE #13
PI = 128+67.48 "PR-Path"
Delta = 17°25'48" Rt.
R = 50.00'
T = 7.66'
L = 15.21'
E = 0.58'

CURVE #14
PI = 128+91.65 "PR-Path"
Delta = 16°58'55" Lt.
R = 50.00'
T = 7.46'
L = 14.82'
E = 0.55'

Sec. 29, T-23-N, R-14-E
Wayne Township
Jay County

NOTES:
All Existing & Proposed R/W, Property Lines and
Topography described from Line "X" unless
otherwise noted.
All R/W to be as shown.
For Structure Notes and additional information,
see Construction Detail Sheets.



BLN:
BEAM·LONGEST·NEFF

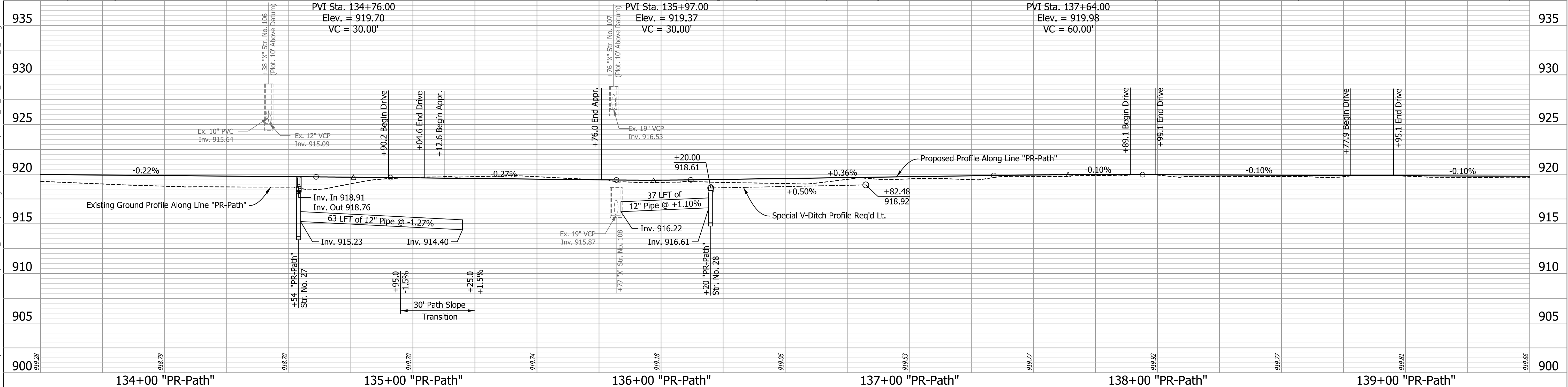
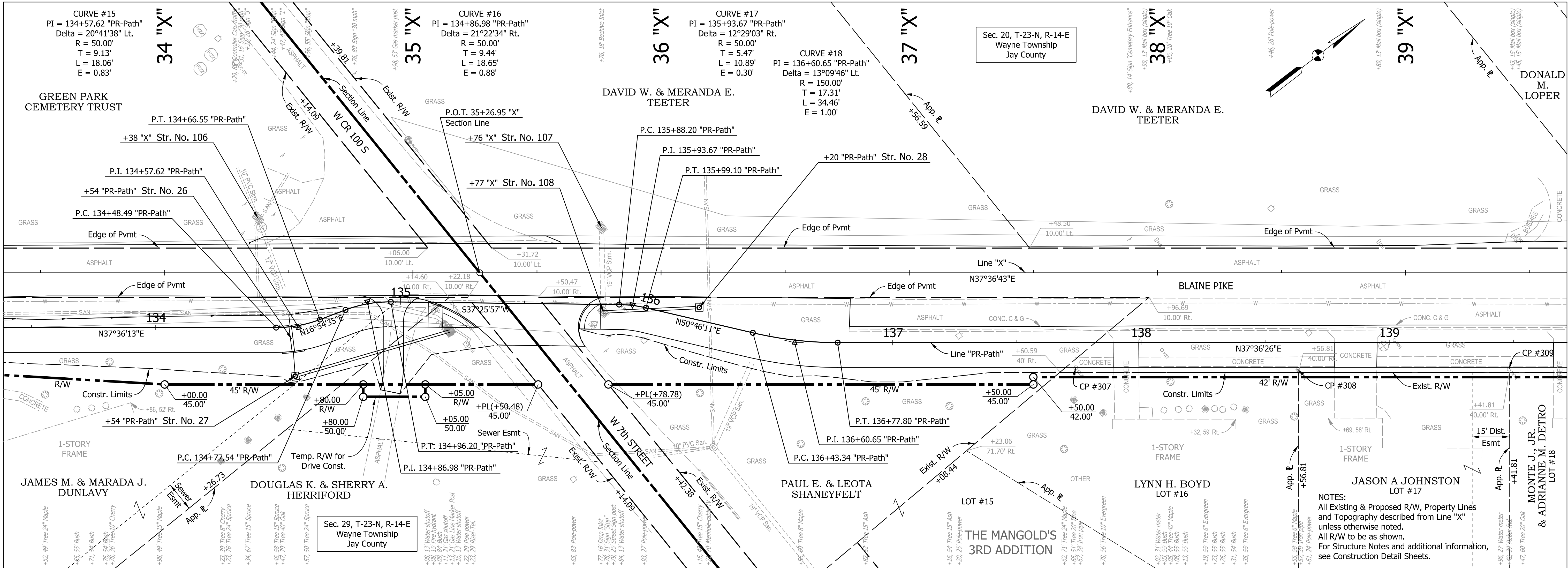
RECOMMENDED FOR APPROVAL	
DESIGNED: BTD	DRAWN: MAB
CHECKED: DJG	CHECKED: DJG

INDIANA
DEPARTMENT OF TRANSPORTATION

PLAN & PROFILE
LINES "X" & "PR-Path"

HORIZONTAL SCALE 1" = 20'	BRIDGE FILE N.A.
VERTICAL SCALE 1" = 5'	DESIGNATION 1600965
SURVEY BOOK ELECTRONIC	SHEETS 14 of 128
CONTRACT R-40319	PROJECT 1600965

9/9/2020 1:03:08 AM I:\170084 Portland Blaine Pike\09Road_Plan\170084_07_Sht_PlanProfile_X_07.dgn



BLN
BEAM·LONGEST-NEFF

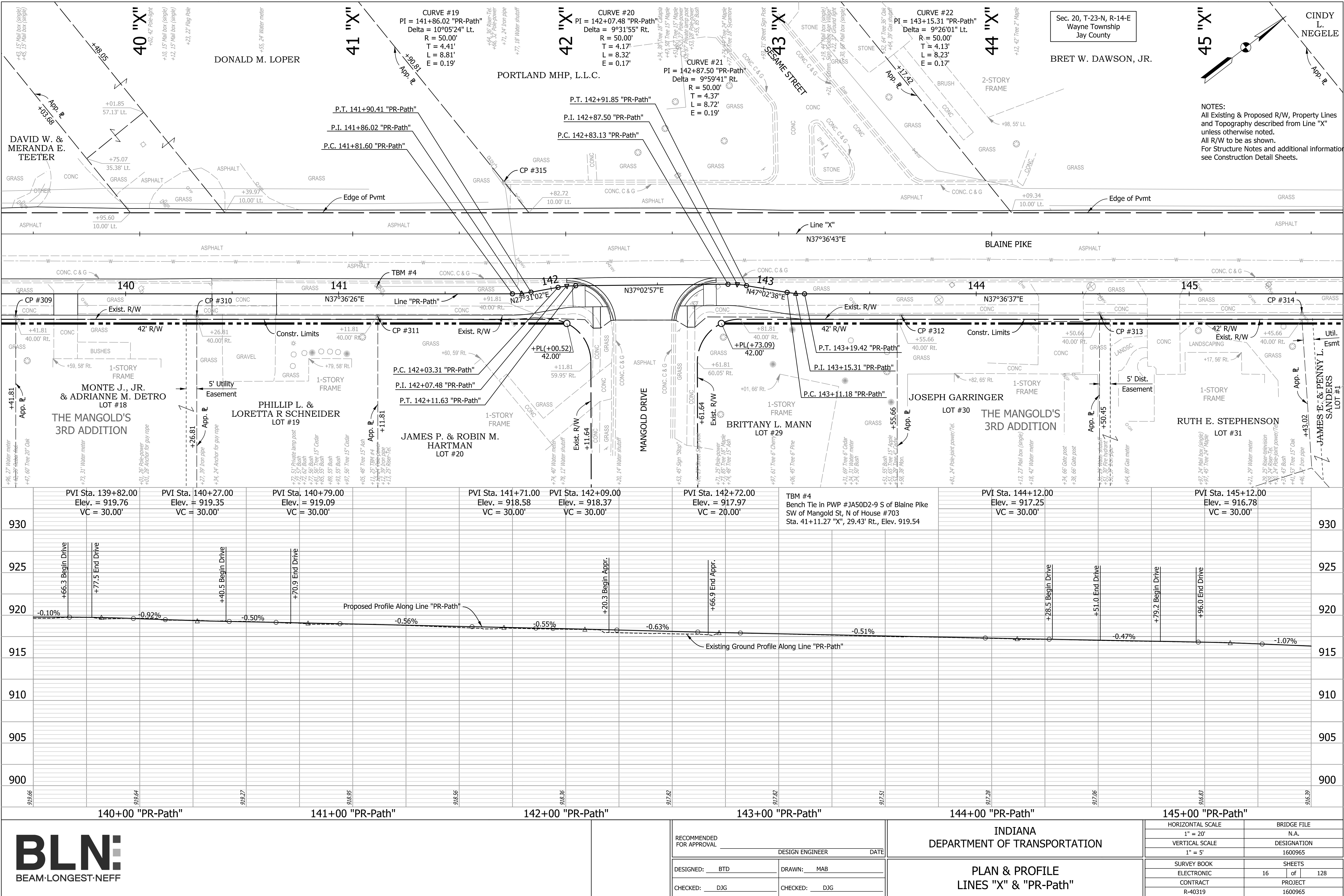
RECOMMENDED FOR APPROVAL	DESIGN ENGINEER		DATE
	DESIGNED: BTD	DRAWN: MAB	
CHECKED: DJG	CHECKED: DJG		

INDIANA
DEPARTMENT OF TRANSPORTATION

PLAN & PROFILE
LINES "X" & "PR-Path"

HORIZONTAL SCALE 1" = 20'	BRIDGE FILE	
	N.A.	
VERTICAL SCALE 1" = 5'	DESIGNATION	
	1600965	
SURVEY BOOK		SHEETS
ELECTRONIC		15 of 128
CONTRACT		PROJECT
R-40319		1600965

9/9/2020 1:03:09 AM I:\170084 Portland Blaine Pike\09Road, Indiana\04 Design files\CGNPlans\170084_07_Sht_PlanProfile_X_08.dgn



BLN:
BEAM·LONGEST-NEFF

RECOMMENDED
FOR APPROVAL

DESIGN ENGINEER

DATE

DESIGNED:

BTD

DRAWN:

MAB

CHECKED:

DJG

CHECKED:

DJG

INDIANA
DEPARTMENT OF TRANSPORTATION

PLAN & PROFILE
LINES "X" & "PR-Path"

HORIZONTAL SCALE

1" = 20'

VERTICAL SCALE

1" = 5'

SURVEY BOOK

ELECTRONIC

CONTRACT

R-40319

BRIDGE FILE

N.A.

DESIGNATION

1600965

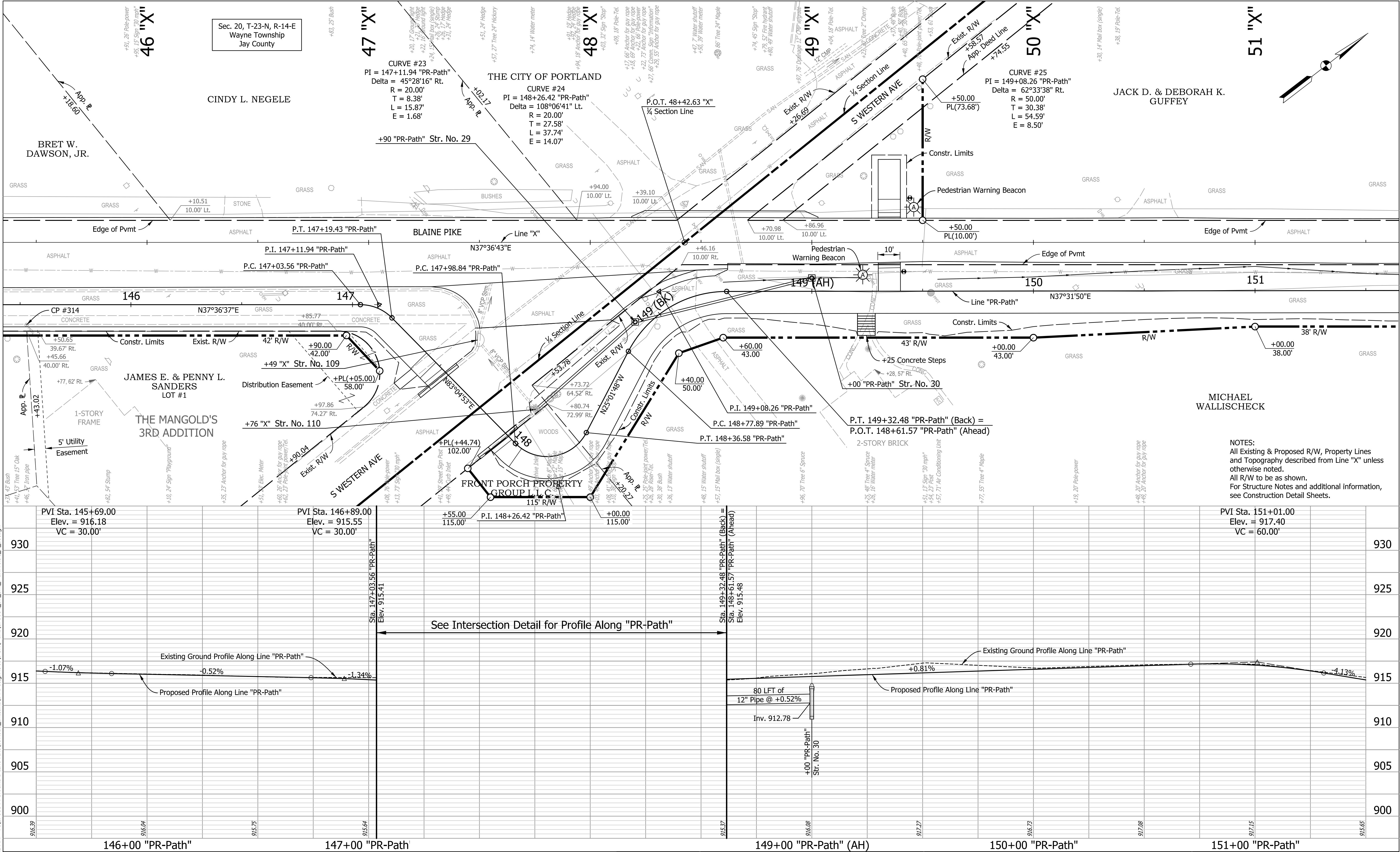
SHEETS

16 of 128

PROJECT

1600965

9/9/2020 1:09:20 PM I:\170084 Portland Blaine Pike\09Road_Plan\170084_07_Sht_PlanProfile_X_09.dgn



BLN:
BEAM·LONGEST-NEFF

RECOMMENDED FOR APPROVAL	
DESIGN ENGINEER	DATE
DESIGNED: BTD	DRAWN: MAB
CHECKED: DJG	CHECKED: DJG

INDIANA
DEPARTMENT OF TRANSPORTATION

PLAN & PROFILE
LINES "X" & "PR-Path"

HORIZONTAL SCALE	BRIDGE FILE
1" = 20'	N.A.
VERTICAL SCALE	DESIGNATION
1" = 5'	1600965
SURVEY BOOK	SHEETS
ELECTRONIC	17 of 128
CONTRACT	PROJECT
R-40319	1600965

9/9/2020 1:09:12 AM I:\170084 Portland Blaine Pike\09Road_Indiana\04 Design files\DCN\Plans\170084_08_Sht_PlanProfile_PR-Path_01.dgn

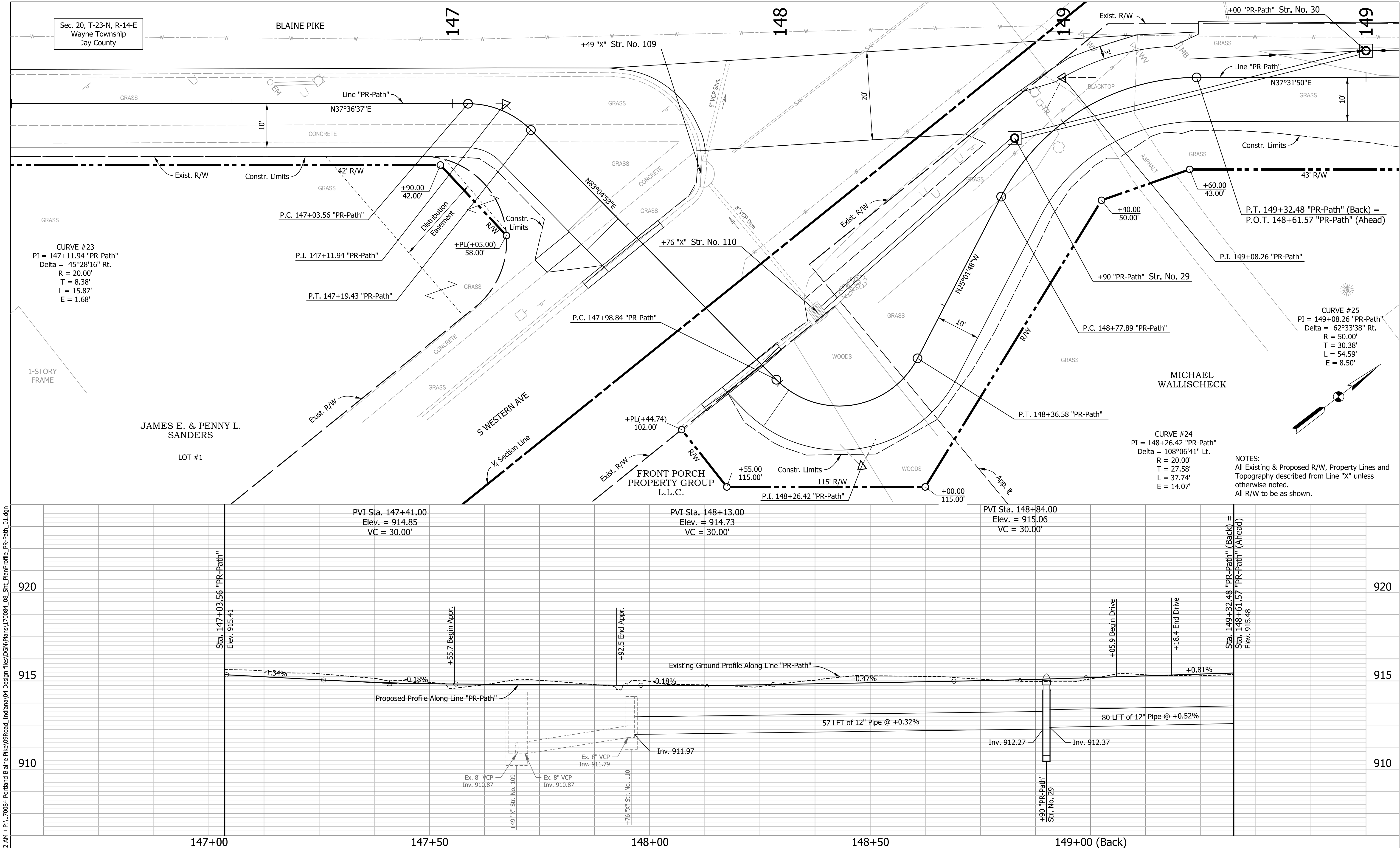


RECOMMENDED FOR APPROVAL _____		
DESIGN ENGINEER _____		DATE _____
DESIGNED: BTD	DRAWN: MAB	
CHECKED: DJG	CHECKED: DJG	

INDIANA
DEPARTMENT OF TRANSPORTATION

PLAN & PROFILE
LINE "PR-Path"

HORIZONTAL SCALE 1" = 10'		BRIDGE FILE N.A.	
VERTICAL SCALE 2" = 5'		DESIGNATION 1600965	
SURVEY BOOK ELECTRONIC		SHEETS 18 of 128	
CONTRACT R-40319		PROJECT 1600965	



NOTES:
All Existing & Proposed R/W, Property Lines and
Topography described from Line "X" unless
otherwise noted.
All R/W to be as shown.

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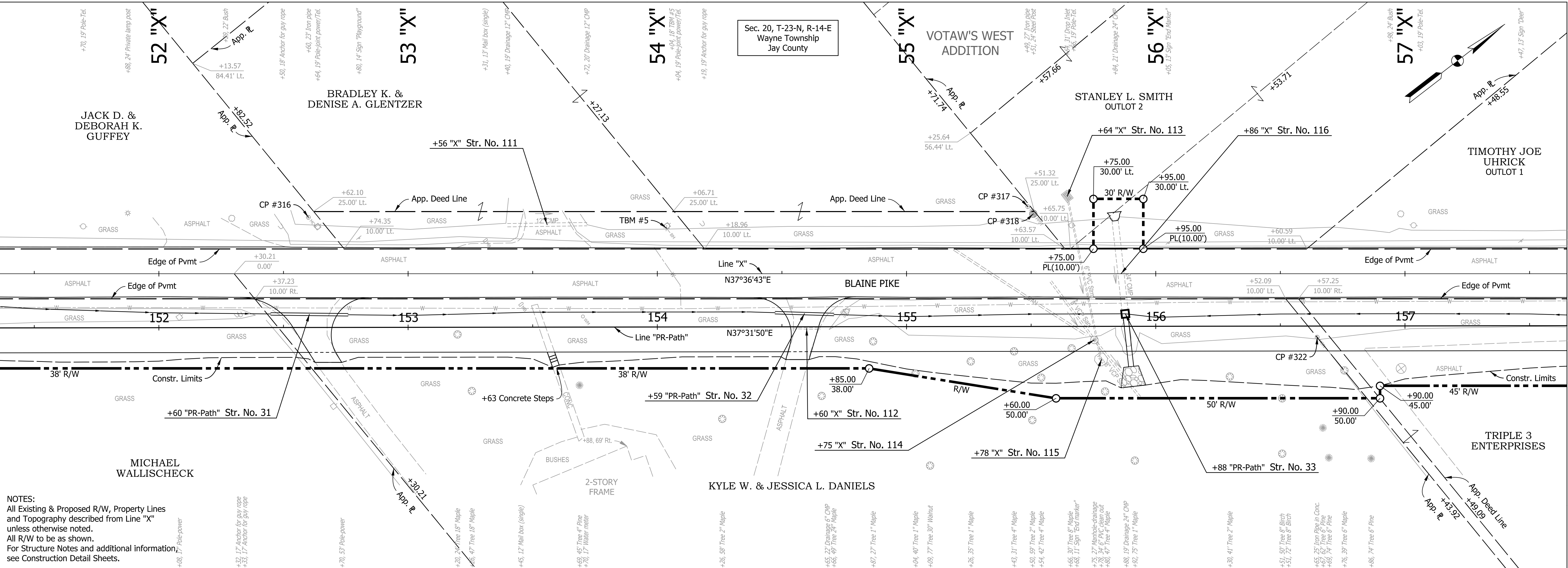


RECOMMENDED FOR APPROVAL		DESIGN ENGINEER		DATE	
DESIGNED: BTD		DRAWN: MAB			
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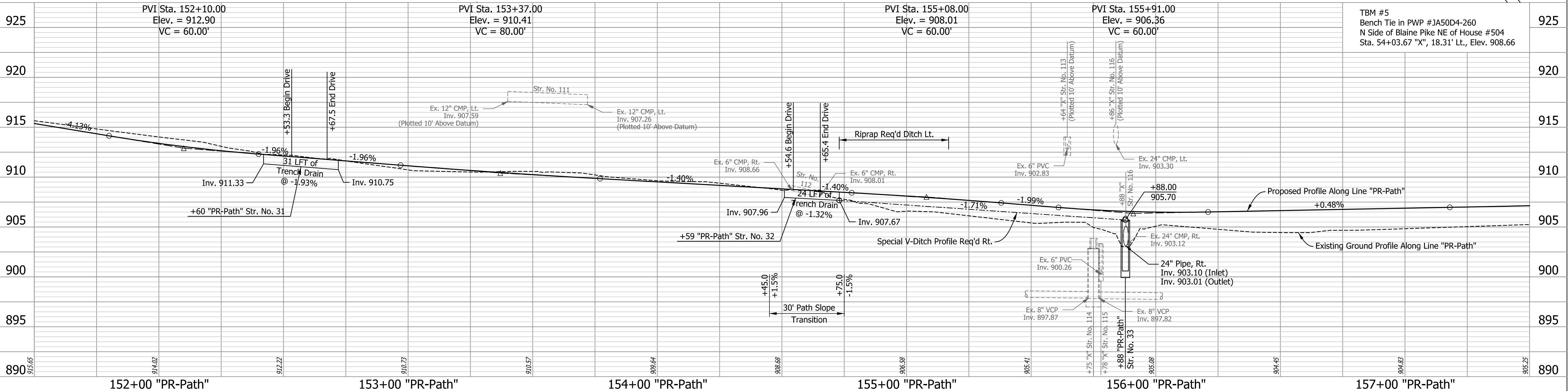
INDIANA
DEPARTMENT OF TRANSPORTATION

PLAN & PROFILE
LINES "X" & "PR-Path"

HORIZONTAL SCALE		BRIDGE FILE	
1" = 20'		N.A.	
VERTICAL SCALE		DESIGNATION	
1" = 5'		1600965	
SURVEY BOOK		SHEETS	
ELECTRONIC		19 of 128	
CONTRACT		PROJECT	
R-40319		1600965	



NOTES:
All Existing & Proposed R/W, Property Lines
and Topography described from Line "X"
unless otherwise noted.
All R/W to be as shown.
For Structure Notes and additional information,
see Construction Detail Sheets.



9/9/2020 1:9/9/2020 1 8:20:30 AM I P:\170084 Portland Blaine Pike\09Road, Indiana\04 Design files\DCN\Plans\170084_07_Sht_PlanProfile_X_12.dgn



RECOMMENDED
FOR APPROVAL

DESIGN ENGINEER

DATE

DESIGNED:

BTD

DRAWN:

MAB

CHECKED:

DJG

CHECKED:

DJG

INDIANA
DEPARTMENT OF TRANSPORTATION

PLAN & PROFILE
LINES "X" & "PR-Path"

HORIZONTAL SCALE

1" = 20'

VERTICAL SCALE

1" = 5'

BRIDGE FILE

N.A.

DESIGNATION

1600965

SURVEY BOOK

ELECTRONIC

CONTRACT

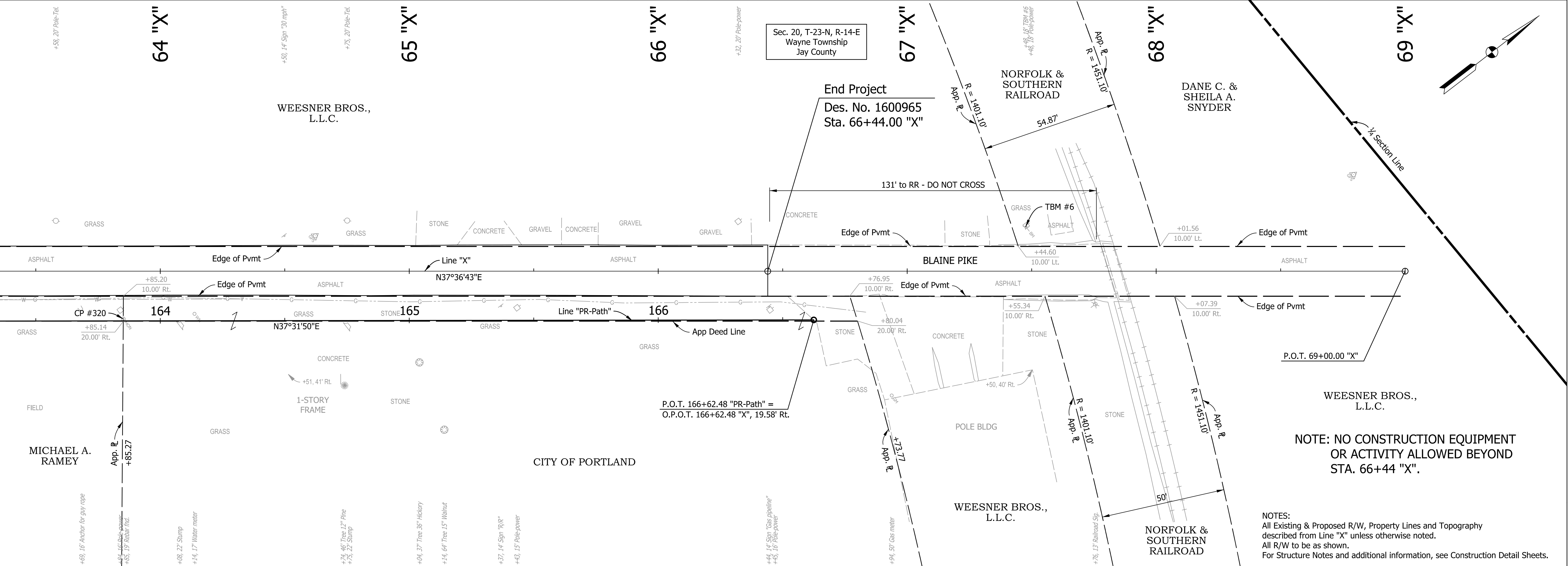
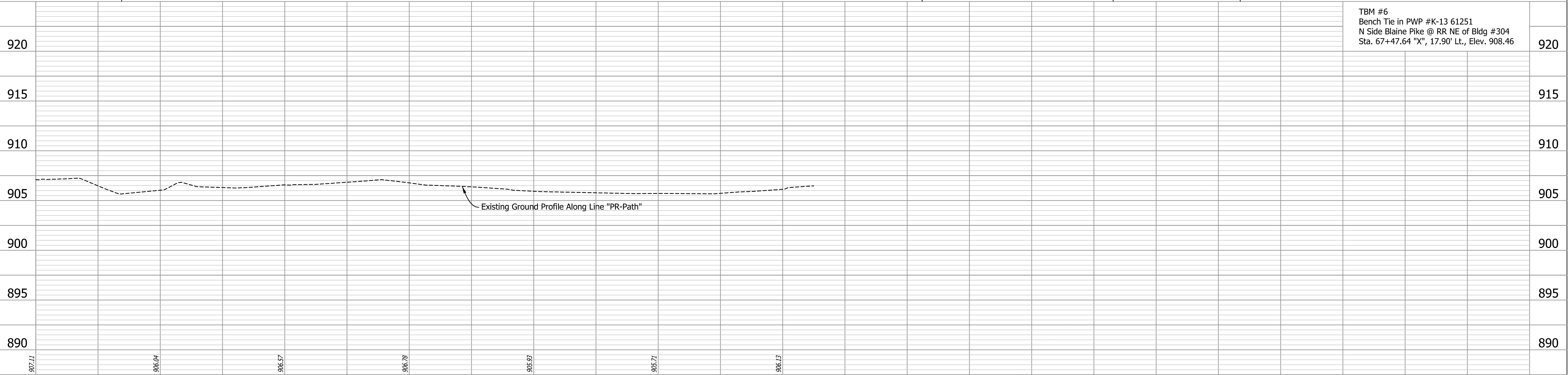
R-40319

SHEETS

21 of 128

PROJECT

1600965



NOTE: NO CONSTRUCTION EQUIPMENT
OR ACTIVITY ALLOWED BEYOND
STA. 66+44 "X".

NOTES:
All Existing & Proposed R/W, Property Lines and Topography
described from Line "X" unless otherwise noted.
All R/W to be as shown.
For Structure Notes and additional information, see Construction Detail Sheets.

TBM #6
Bench Tie in PWP #K-13 61251
N Side Blaine Pike @ RR NE of Bldg #304
Sta. 67+47.64 "X", 17.90' Lt., Elev. 908.46

920

915

910

905

900

895

890

164+00 "PR-Path"

165+00 "PR-Path"

166+00 "PR-Path"

9/9/2020 1:19:03:16 AM I:\170084 Portland Blaine Pike\09Road_Indiana\04 Design files\09Road_Indiana\170084_09_Sht_ConstDetail_X_03.dgn



LEGEND

- (F1) HMA for Sidewalk consisting of:
140 #/Sys HMA Surface, Type B on
220 #/Sys HMA Intermediate, Type B on
6" Compacted Aggregate, No. 53 on
Subgrade Treatment, Type III
- (R) 1.5" Asphalt Milling, before
HMA Overlay consisting of:
165 #/Syd QC/QA HMA Surface, X, X, 9.5 mm
- (26) Nursery Sodding

NOTES:
All Existing & Proposed R/W, Property Lines and Topography
described from Line "X" unless otherwise noted.
All R/W to be as shown.
All Proposed Construction described from Line "PR-Path"
unless otherwise noted.

RECOMMENDED FOR APPROVAL _____	
DESIGNED: BTD	DRAWN: MAB
CHECKED: DJG	CHECKED: DJG

INDIANA
DEPARTMENT OF TRANSPORTATION

CONSTRUCTION DETAILS
LINE "PR-Path"

HORIZONTAL SCALE		BRIDGE FILE	
1" = 20'		N.A.	
VERTICAL SCALE		DESIGNATION	
1" = 5'		1600965	
SURVEY BOOK		SHEETS	
ELECTRONIC		22	of 128
CONTRACT		PROJECT	
R-40319		1600965	

9/9/2020 1:9/2020 1:803:17 AM I P:\170084 Portland Blaine Pike\09Road_Indiana\04 Design files\DCN\Plans\170084_09_Sht_ConstDetail_X_04.dgn



BEAM PLACE SOUTH
SUBDIVISION

ROBERT E. BRAGG, JR.

+95 Str. No. 13
18" Pipe Catch Basin &
168 LFT of 12" Pipe Req'd
Connect to Str. No. 101

+01 "X" Str. No. 102
Exist. Manhole
Adjust Casting to Grade

+50 Str. No. 14
33 LFT of 12" CIP Trench Drain

COREY L. DUNMOYER

119

GALEN L. BYERS

BLAINE PIKE

RUIZ RENTALS,
L.L.C.

120

121

WALTER M.
HAYNES
ET UX

WILHELM,
AARON L.
ET UX.
LOT #33

BEAM PLACE SOUTH
SUBDIVISION

LEGEND

- (C1) PCCP for Approaches, 6" on Subgrade Treatment, Type II
- (CR) Curb Ramp, Concrete
- (D1) HMA for Approaches, Type B
165 #/Sys HMA Surface, Type B on
275 #/Sys HMA Intermediate, Type B on
6" Compacted Aggregate No. 53 on
Subgrade Treatment, Type II
- (F1) HMA for Sidewalk consisting of:
140 #/Sys HMA Surface, Type B on
220 #/Sys HMA Intermediate, Type B on
6" Compacted Aggregate, No. 53 on
Subgrade Treatment, Type III
- (F2) Sidewalk, Concrete, 4"
- (R) 1.5" Asphalt Milling, before
HMA Overlay consisting of:
165 #/Syd QC/QA HMA Surface, X, X, 9.5 mm
- (26) Nursery Sodding
- (43) Transverse Marking, Thermoplastic, Crosswalk Line, White, 6 in.

NOTES:
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Topography described from Line "X" unless
otherwise noted.
All R/W to be as shown.
All Proposed Construction described from Line "PR-Path"
unless otherwise noted.

RECOMMENDED FOR APPROVAL		DESIGN ENGINEER		DATE	
DESIGNED: BTD		DRAWN: MAB			
CHECKED: DJG		CHECKED: DJG			

INDIANA
DEPARTMENT OF TRANSPORTATION

CONSTRUCTION DETAILS
LINE "PR-Path"

HORIZONTAL SCALE		BRIDGE FILE	
1" = 20'		N.A.	
VERTICAL SCALE		DESIGNATION	
1" = 5'		1600965	
SURVEY BOOK		SHEETS	
ELECTRONIC		23	of 128
CONTRACT		PROJECT	
R-40319		1600965	



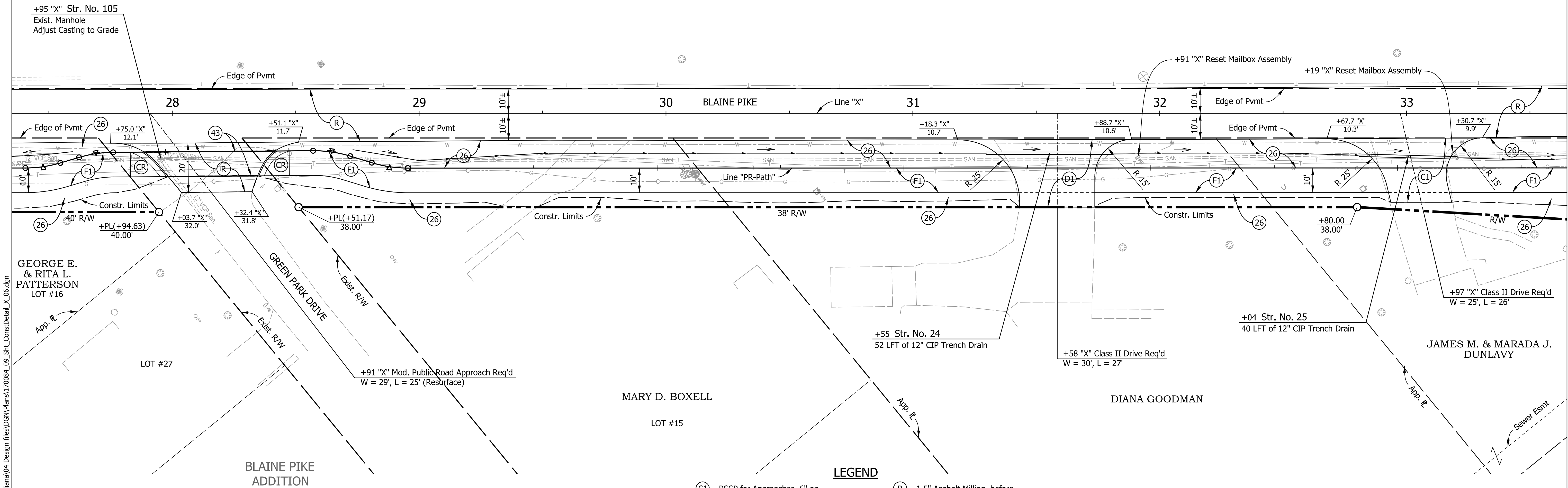
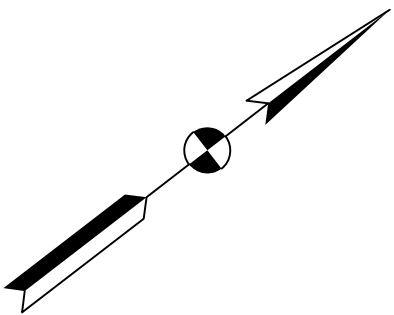
<p>(CR) Curb Ramp, Concrete</p> <p>(D1) HMA for Approaches, Type B 165 #/Sys HMA Surface, Type B on 275 #/Sys HMA Intermediate, Type B on 6" Compacted Aggregate No. 53 on Subgrade Treatment, Type II</p> <p>(F1) HMA for Sidewalk consisting of: 140 #/Sys HMA Surface, Type B on 220 #/Sys HMA Intermediate, Type B on 6" Compacted Aggregate, No. 53 on Subgrade Treatment, Type III</p>	<p>(R) 1.5" Asphalt Milling, before HMA Overlay consisting of: 165 #/Sys QC/QA HMA Surface, X, X, 9.5 mm</p> <p>(26) Nursery Sodding</p> <p>(43) Transverse Marking, Thermoplastic, Crosswalk Line, White, 6 in.</p>
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BLN
BEAM · LONGEST · NEFF

RECOMMENDED FOR APPROVAL _____ DESIGN ENGINEER _____ DATE _____	INDIANA DEPARTMENT OF TRANSPORTATION			HORIZONTAL SCALE		BRIDGE FILE				
				1" = 20'		N.A.				
				VERTICAL SCALE		DESIGNATION				
				1" = 5'		1600965				
DESIGNED: _____ BTD	DRAWN: _____ MAB			CONSTRUCTION DETAILS LINE "PR-Path"			SURVEY BOOK		SHEETS	
			ELECTRONIC				24	of	128	
CHECKED: _____ DJG	CHECKED: _____ DJG						CONTRACT		PROJECT	
			R-40319				1600965			

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GREEN PARK CEMETERY TRUST



- LEGEND**
- (C1) PCCP for Approaches, 6" on Subgrade Treatment, Type II
 - (CR) Curb Ramp, Concrete
 - (D1) HMA for Approaches, Type B
165 #/Sys HMA Surface, Type B on
275 #/Sys HMA Intermediate, Type B on
6" Compacted Aggregate No. 53 on
Subgrade Treatment, Type II
 - (F1) HMA for Sidewalk consisting of:
140 #/Sys HMA Surface, Type B on
220 #/Sys HMA Intermediate, Type B on
6" Compacted Aggregate, No. 53 on
Subgrade Treatment, Type III
 - (R) 1.5" Asphalt Milling, before
HMA Overlay consisting of:
165 #/Sys QC/QA HMA Surface, X, X, 9.5 mm
 - (26) Nursery Sodding
 - (43) Transverse Marking, Thermoplastic, Crosswalk Line, White, 6 in.

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Topography described from Line "X" unless
otherwise noted.
All R/W to be as shown.
All Proposed Construction described from Line "PR-Path"
unless otherwise noted.

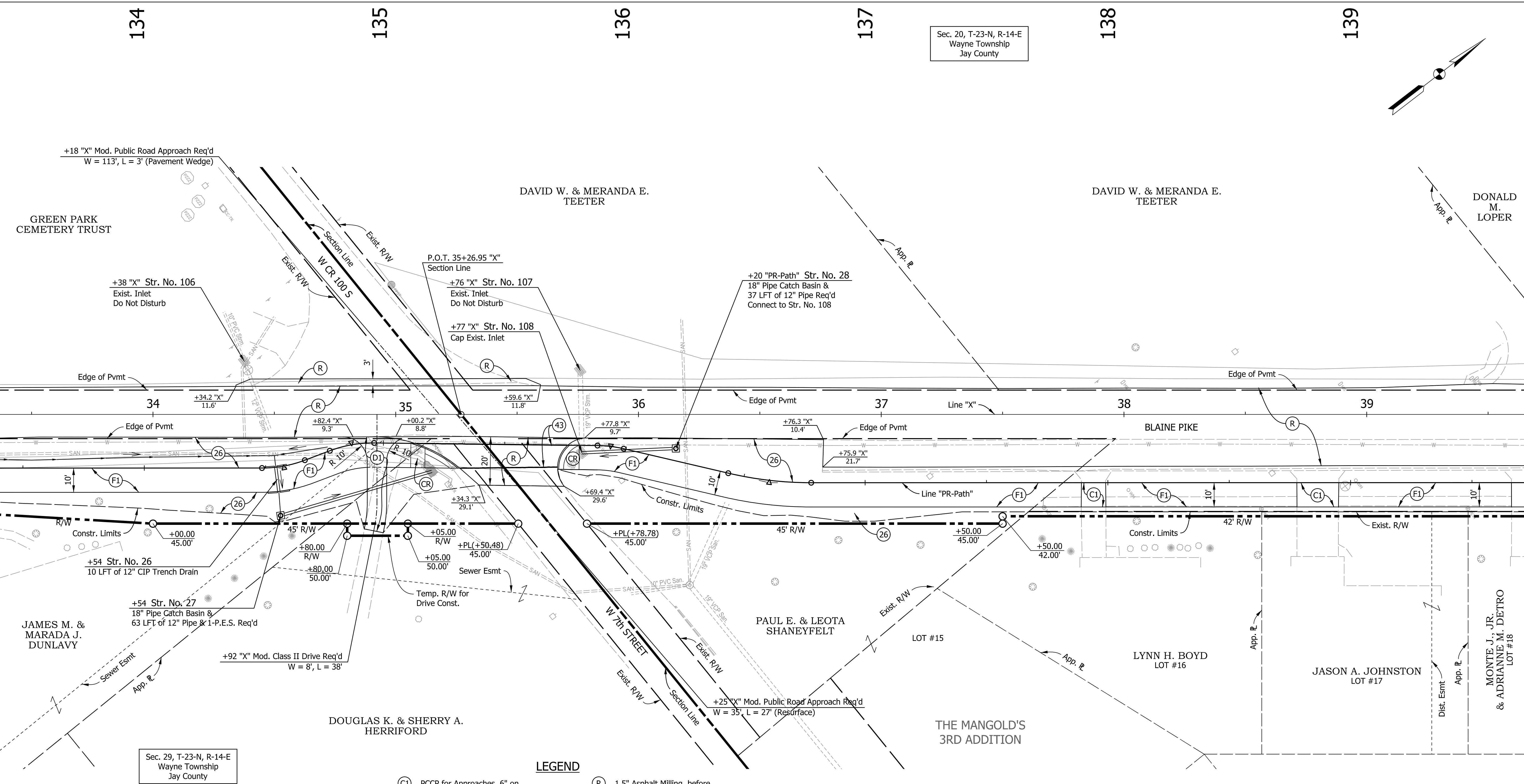
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DESIGN ENGINEER _____	DATE _____
DESIGNED: BTD	DRAWN: MAB
CHECKED: DJG	CHECKED: DJG

INDIANA
DEPARTMENT OF TRANSPORTATION

CONSTRUCTION DETAILS
LINE "PR-Path"

HORIZONTAL SCALE 1" = 20'		BRIDGE FILE N.A.	
VERTICAL SCALE 1" = 5'		DESIGNATION 1600965	
SURVEY BOOK		SHEETS	
ELECTRONIC		25	of 128
CONTRACT R-40319		PROJECT 1600965	

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Sec. 29, T-23-N, R-14-E
Wayne Township
Jay County

Sec. 20, T-23-N, R-14-E
Wayne Township
Jay County

LEGEND

- (C1) PCCP for Approaches, 6" on Subgrade Treatment, Type II
- (CR) Curb Ramp, Concrete
- (D1) HMA for Approaches, Type B
165 #/Sys HMA Surface, Type B on
275 #/Sys HMA Intermediate, Type B on
6" Compacted Aggregate No. 53 on
Subgrade Treatment, Type II
- (F1) HMA for Sidewalk consisting of:
140 #/Sys HMA Surface, Type B on
220 #/Sys HMA Intermediate, Type B on
6" Compacted Aggregate, No. 53 on
Subgrade Treatment, Type III
- (R) 1.5" Asphalt Milling, before
HMA Overlay consisting of:
165 #/Sysd QC/QA HMA Surface, X, X, 9.5 mm
- (26) Nursery Sodding
- (43) Transverse Marking, Thermoplastic, Crosswalk Line, White, 6 in.

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All Existing & Proposed R/W, Property Lines and Topography
described from Line "X" unless otherwise noted.
All R/W to be as shown.
All Proposed Construction described from Line "PR-Path"
unless otherwise noted.

BLN:
BEAM·LONGEST-NEFF

RECOMMENDED FOR APPROVAL		
DESIGN ENGINEER		DATE
DESIGNED: BTD	DRAWN: MAB	
CHECKED: DJG	CHECKED: DJG	

INDIANA
DEPARTMENT OF TRANSPORTATION

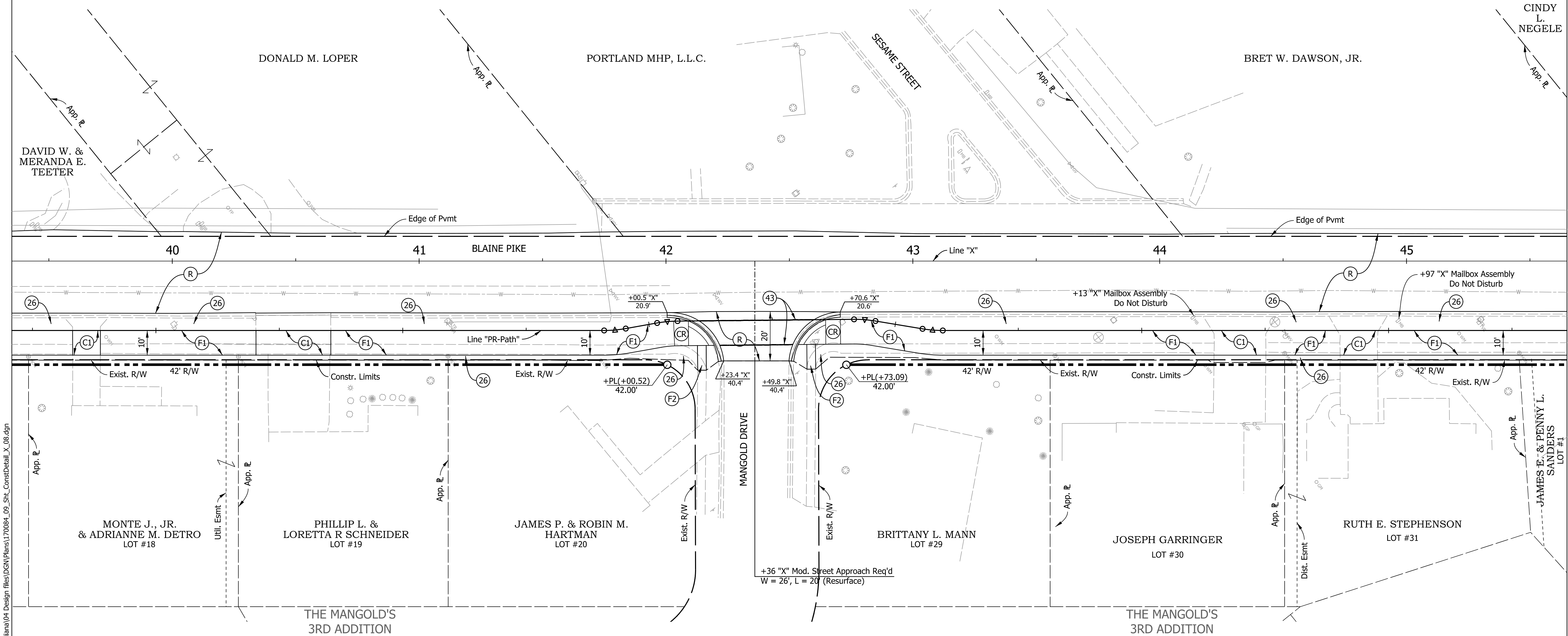
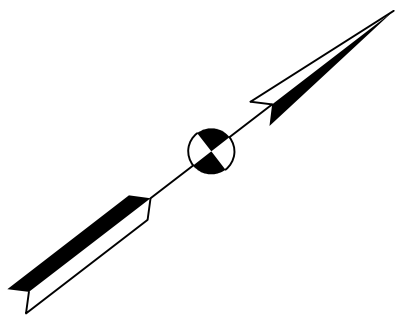
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LINE "PR-Path"

HORIZONTAL SCALE		BRIDGE FILE	
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VERTICAL SCALE		DESIGNATION	
1" = 5'		1600965	
SURVEY BOOK		SHEETS	
ELECTRONIC		26	of 128
CONTRACT		PROJECT	
R-40319		1600965	

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Sec. 20, T-23-N, R-14-E
Wayne Township
Jay County



LEGEND

- (C1) PCCP for Approaches, 6" on Subgrade Treatment, Type II
- (CR) Curb Ramp, Concrete
- (F1) HMA for Sidewalk consisting of: 140 #/Sys HMA Surface, Type B on 220 #/Sys HMA Intermediate, Type B on 6" Compacted Aggregate, No. 53 on Subgrade Treatment, Type III
- (F2) Sidewalk, Concrete, 4"
- (R) 1.5" Asphalt Milling, before HMA Overlay consisting of: 165 #/Sys QC/QA HMA Surface, X, X, 9.5 mm
- (26) Nursery Sodding
- (43) Transverse Marking, Thermoplastic, Crosswalk Line, White, 6 in.

NOTES:
All Existing & Proposed R/W, Property Lines and Topography described from Line "X" unless otherwise noted.
All R/W to be as shown.
All Proposed Construction described from Line "PR-Path" unless otherwise noted.

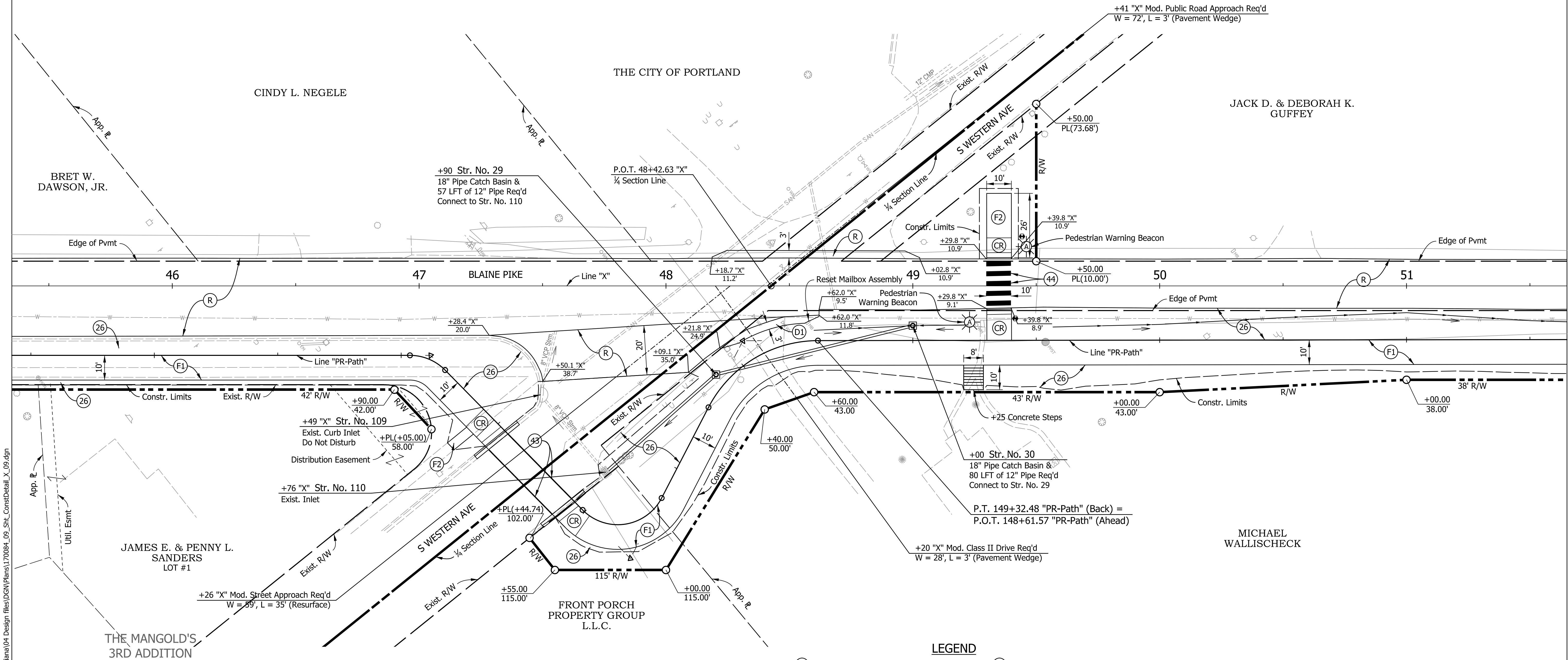
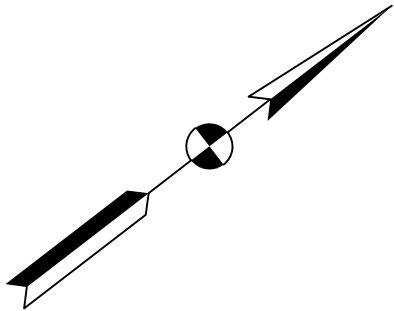
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DESIGNED: BTD	DRAWN: MAB
CHECKED: DJG	CHECKED: DJG

INDIANA
DEPARTMENT OF TRANSPORTATION

CONSTRUCTION DETAILS
LINE "PR-Path"

HORIZONTAL SCALE		BRIDGE FILE	
1" = 20'		N.A.	
VERTICAL SCALE		DESIGNATION	
1" = 5'		1600965	
SURVEY BOOK		SHEETS	
ELECTRONIC		27	of 128
CONTRACT		PROJECT	
R-40319		1600965	

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LEGEND

- (CR) Curb Ramp, Concrete

(D1) HMA for Approaches, Type B
165 #/Sys HMA Surface, Type B on
275 #/Sys HMA Intermediate, Type B on
6\" Compacted Aggregate No. 53 on
Subgrade Treatment, Type II

(F1) HMA for Sidewalk consisting of:
140 #/Sys HMA Surface, Type B on
220 #/Sys HMA Intermediate, Type B on
6\" Compacted Aggregate, No. 53 on
Subgrade Treatment, Type III

(F2) Sidewalk, Concrete, 4\"
- (R) 1.5\" Asphalt Milling, before
HMA Overlay consisting of:
165 #/Syd QC/QA HMA Surface, X, X, 9.5 mm

(26) Nursery Sodding

(43) Transverse Marking, Thermoplastic, Crosswalk Line, White, 6 in.

(44) Transverse Marking, Thermoplastic, Crosswalk Line, White, 24 in.

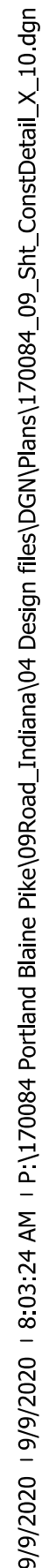
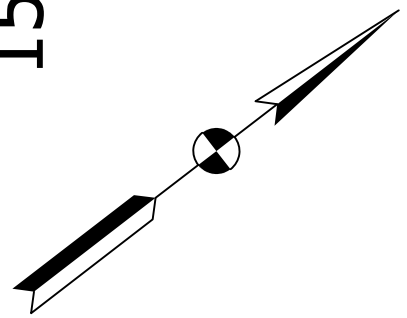
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All R/W to be as shown.
All Proposed Construction described from Line "PR-Path" unless otherwise noted.

RECOMMENDED FOR APPROVAL _____		
DESIGN ENGINEER _____		DATE _____
DESIGNED: BTD	DRAWN: MAB	
CHECKED: DJG	CHECKED: DJG	

INDIANA
DEPARTMENT OF TRANSPORTATION

CONSTRUCTION DETAILS
LINE "PR-Path"

HORIZONTAL SCALE		BRIDGE FILE	
1" = 20'		N.A.	
VERTICAL SCALE		DESIGNATION	
1" = 5'		1600965	
SURVEY BOOK		SHEETS	
ELECTRONIC		28	of 128
CONTRACT		PROJECT	
R-40319		1600965	



NOTES:
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HORIZONTAL SCALE		BRIDGE FILE	
1" = 20'		N.A.	
VERTICAL SCALE		DESIGNATION	
1" = 5'		1600965	
SURVEY BOOK		SHEETS	
ELECTRONIC	29	of	128
CONTRACT	PROJECT		
R-40319	1600965		

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VOTAW'S WEST
ADDITION

TIMOTHY JOE UHRICK
OUTLOT 1

TRIPLE 3
ENTERPRISES

+33 "X" Mod. Public Road Approach Req'd
W = 49', L = 3' (Pavement Wedge)

+02 "X" Mod. Public Road Approach Req'd
W = 62', L = 3' (Pavement Wedge)

+06 "X" Mod. Class IV Drive Req'd
W = 162', L = 3' (Pavement Wedge)

+16 "X" Class II Drive Req'd.
W = 15', L = 24'

+57 "X" Mod. Class II Drive Req'd.
W = 14', L = 24'

ROBERT E. &
ARLEAN A.
BLUMENHORST

JAMES L. HAMBROCK
ON CONTRACT
TO CLINT JORDAN

WEESNER BROS.,
L.L.C.

STEVEN R. SHAWVER

MICHAEL A. RAMEY

MICHAEL AARON RAMEY

LEGEND

- (D1) HMA for Approaches, Type B
165 #/Sys HMA Surface, Type B on
275 #/Sys HMA Intermediate, Type B on
6" Compacted Aggregate No. 53 on
Subgrade Treatment, Type II
- (F1) HMA for Sidewalk consisting of:
140 #/Sys HMA Surface, Type B on
220 #/Sys HMA Intermediate, Type B on
6" Compacted Aggregate, No. 53 on
Subgrade Treatment, Type III
- (R) 1.5" Asphalt Milling, before
HMA Overlay consisting of:
165 #/Syd QC/QA HMA Surface, X, X, 9.5 mm
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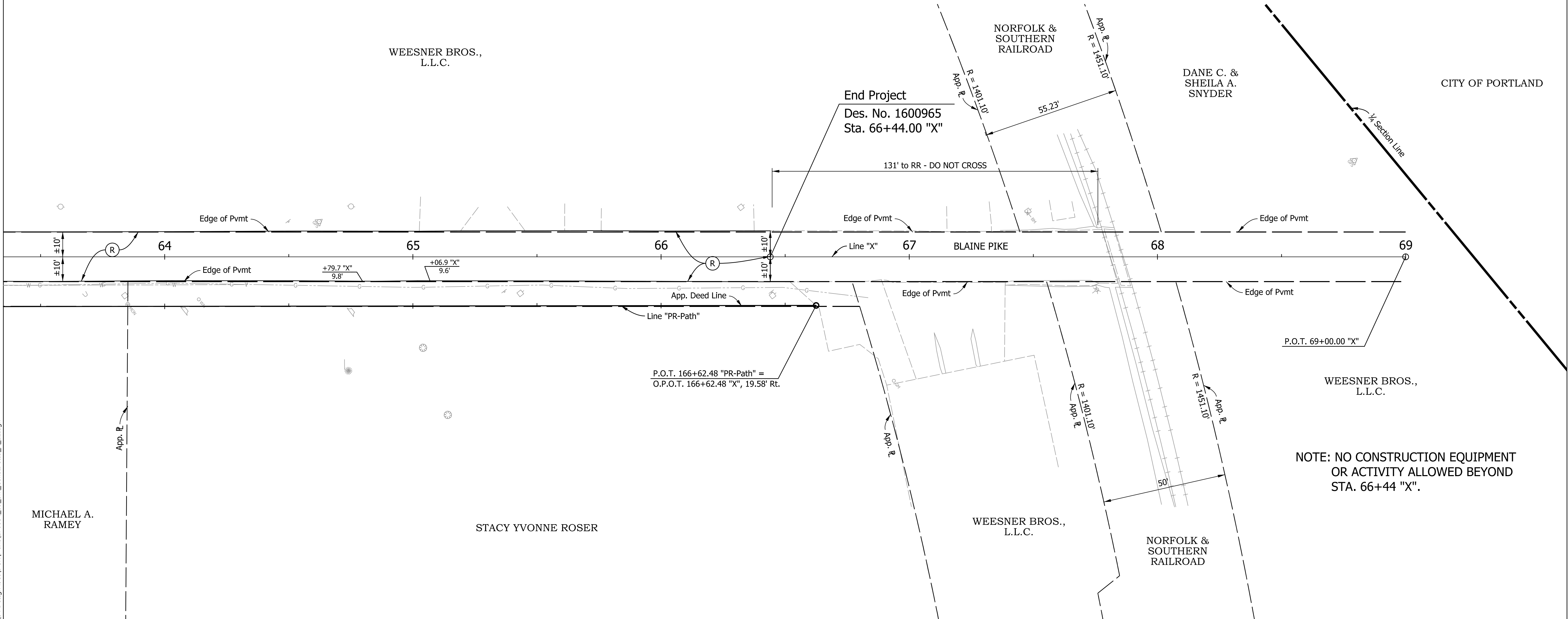
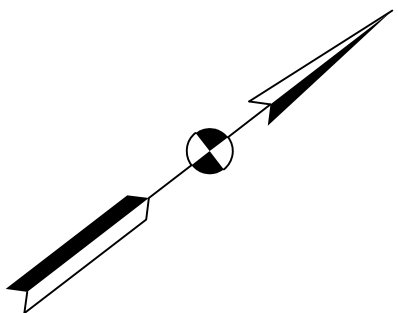
RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: BTD	DRAWN: MAB	
CHECKED: DJG	CHECKED: DJG	

INDIANA
DEPARTMENT OF TRANSPORTATION

CONSTRUCTION DETAILS
LINE "PR-Path"

HORIZONTAL SCALE 1" = 20'	BRIDGE FILE N.A.
VERTICAL SCALE 1" = 5'	DESIGNATION 1600965
SURVEY BOOK ELECTRONIC	SHEETS 30 of 128
CONTRACT R-40319	PROJECT 1600965

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LEGEND

- (D1) HMA for Approaches, Type B
165 #/Sys HMA Surface, Type B on
275 #/Sys HMA Intermediate, Type B on
6" Compacted Aggregate No. 53 on
Subgrade Treatment, Type II

(F1) HMA for Sidewalk consisting of:
140 #/Sys HMA Surface, Type B on
220 #/Sys HMA Intermediate, Type B on
6" Compacted Aggregate, No. 53 on
Subgrade Treatment, Type III
- (R) 1.5" Asphalt Milling, before
HMA Overlay consisting of:
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(26) Nursery Sodding

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RECOMMENDED FOR APPROVAL _____	
DESIGNED: BTD	DRAWN: MAB
CHECKED: DJG	CHECKED: DJG

INDIANA DEPARTMENT OF TRANSPORTATION	
CONSTRUCTION DETAILS LINE "PR-Path"	

HORIZONTAL SCALE 1" = 20'		BRIDGE FILE N.A.	
VERTICAL SCALE 1" = 5'		DESIGNATION 1600965	
SURVEY BOOK ELECTRONIC		SHEETS 31 of 128	
CONTRACT R-40319		PROJECT 1600965	

Appendix C:

Early Coordination

June 11, 2018

Early Coordination Agency

**Sample Early Coordination
Letter and Agency Listing**

Re: Des. No. 1600965
Blaine Pike Pavement and
Non – Motorized Facility Project
CR 150 South to South of Norfolk Southern Railroad
Portland, Jay County, Indiana

Beam, Longest, and Neff, LLC (BLN) has been selected by the City of Portland, Indiana to prepare the environmental documentation and design plans for the resurfacing of Blaine Pike and the construction of a multi-use path adjacent to the corridor using federal aid funds. This letter is written to describe the referenced project and to solicit your comments on those resources within your jurisdiction as part of the early coordination process.

Project Location

The project is on the southwest side of Portland in Wayne Township, Jay County Indiana. Blaine Pike is on a tangent horizontal alignment and traverses southwest to northeast. Termini for the refereed project on Blaine Pike are County Road (CR) 150 South on the south to approximately 131 feet south of the Norfolk Southern Railroad crossing of Blaine Pike on the north; the total project length is approximately 1.2 miles (6,500 feet). Specifically, the project crosses Sections 20, 29 and 30, Township 23 North, Range 14 East, as shown on the Portland, Indiana 7.5-minute USGS quadrangle map. Project location maps, an aerial photograph, and ground level photographs are attached to this correspondence.

Existing Roadway

Blaine Pike is a two-lane asphalt roadway that is approximately 20 - 21 feet wide. The functional classification of Blaine Pike is a Minor Arterial. The roadway serves as one of the primary routes for access into the City of Portland from rural Jay County. Blaine Pike provides two travel lanes, one in each direction; lane widths are 10 - 10.5 feet. There are no useable shoulders and no pavement markings. A curbed parking lane affords on-street parking from 7th Street to Western Avenue on the east side of Blaine Pike; a second curbed parking lane exists on the west side of the Blaine Pike at Sesame Street. Although on a diagonal, the horizontal alignment of the roadway is tangent, and the vertical alignment is at grade. The posted speed limit is 30 mph. Overhead power lines run parallel along the east side of Blaine Pike, with a secondary set of lines running along the west side of Western Avenue to the railroad tracks. The roadway also provides access to various special land uses including the Green Park Cemetery, Portland Memorial Park, Golden Age Adult Community and the Jay County Humane Society.

Drainage along Blaine Pike is directed through a series of open ditches and outfalls into well-defined drainage ditches that travel east and west under the roadway. A new corrugated metal pipe was recently installed, north of 3rd Street to assist in directing surface run-off away from the roadway. In several areas along the roadway, drainage appears to be captured and transported by yard inlets. It appears all surface run-off is outlet into the Salamonie River, which is located approximately one-half mile to the east and parallel to Blaine Pike.



The Norfolk Southern Railroad line crosses Blaine Pike, north of the northern project terminus. The rail crossing is in fair to good condition and has advanced signing and cross bucks with mounted flashing lights. However, there are no stop bars or pavement markings on Blaine Pike. No work will be undertaken on the rail crossing as the project will terminate approximately 131 feet south of the rail crossing.

Existing Land Use

The predominant land use along the corridor is single family residential subdivisions. Agricultural land is present beyond the project corridor to the south, east and west. The City of Portland is located to the north. Commercial and light industrial uses are clustered near the Norfolk Southern Railroad tracks. A non-profit organization (Disabled American Veterans) is on Blaine Pike near 3rd Street.

There are special land uses present within and adjacent to the project corridor. The Green Park Cemetery, which was established in 1886 is the largest cemetery in Jay County and is located along the west side of Blaine Pike between 7th Street and Honeysuckle Lane. Two access drives for the cemetery intersect with Blaine Pike. The Portland Memorial City Park does not have direct access onto Blaine Pike, but the facility it is located within 100 feet of the corridor, near Western Avenue. The Golden Age Adult Community is a senior citizen retirement community and has one access drive on the west side of Blaine Pike at Sesame Street.

The National Wetlands Inventory (NWI) map was reviewed, and there are no mapped wetlands along Blaine Pike. A Waters of the US Report (WOUSR) will be prepared as part of the project and will identify any potential jurisdictional water resources present in the project area.

The Federal Emergency Management Agency (FEMA) flood map was also reviewed to determine the presence of designated floodways or floodplains. The 100-year floodplain of the Salamonie River is present to the northeast of Blaine Pike and extends between 3rd Street to the Norfolk Southern Rail crossing. The floodplain does not extend west of Blaine Pike. Floodplain impacts will be analyzed as part of the environmental document prepared for the project.

Purpose and Need

City of Portland wishes to improve the safety of the Blaine Pike corridor for both non-motorized and pedestrian access and increased vehicular traffic. The existing sidewalks and access ramps need to be improved to be in compliance with current Public Right-of-Way Accessibility Guidelines (PROWAG) and the Americans with Disabilities Act (ADA) guidelines. The existing pavement surface exhibits major cracking which needs to be addressed to extend the life of the pavement structure. This is especially critical near the Portland Memorial Park, the Golden Age Adult Community and the Green Park Cemetery, all of which contribute to increased pedestrian traffic. In addition, there are numerous points of ingress/egress to the residential subdivisions along the project length. The proposed new multi-use use path in addition to an improved roadway surface, will provide increased pedestrian, wheelchair and bicycle safety and provide enhanced vehicular access along the corridor. In addition, the City of Portland supports these improvements to enhance connectivity and the quality of life for its residents and other travelers within the community.

Proposed Improvements

The design for this project will evaluate the existing pavement structure to assess patching needs. This will determine the depth of asphalt milling and provide a preventive maintenance overlay of the existing facility. The intent of this analysis is to extend the life of the existing pavement in the most economical way. Surface runoff will continue to be captured via the open ditches and the enclosed storm system. The existing corrugated pipe north of 3rd Street will be extended to accommodate the inclusion of the new multi-use path. A pedestrian hand rail will also be installed at this location in compliance with the PROWAG/ADA guidelines.



A 10-foot wide multi-use (shared path) will be constructed along the entire length of the corridor. Evaluation of utility and right-of-way impacts along with the existing terrain and connectivity with existing facilities, will determine the best location for the new path. A signalized, mid-block crossing of Blaine Pike at Western Avenue will be evaluated to connect the subdivisions east of Blaine Pike to Portland Memorial Park. A signalized crossing will allow protected pedestrian crossings in marked crosswalks, stopping traffic only as needed. Due to the proximity of the cemetery to Blaine Pike, the installation of the multi-use path is proposed for the east side of the road; however, there may be opportunities for the path to be constructed on the west, depending upon connectivity and safety objectives. New reflective advance warning signs and pavement markings will also be installed where required.

Blaine Pike will remain open for the duration of the construction period. The pavement milling and overlay will be performed with the use of flaggers and a moving construction site. At least one lane of traffic will be maintained at all times for access through the construction zone. Closure of the road will not be necessary.

Additional permanent right-of-way will be required for the construction of the project, as well as the easement across the railroad right-of-way. It is estimated that approximately 1.8 acres of mostly residential right-of-way will be necessary. Excavation up to a depth of two feet will be necessary for construction of the multi-use path along the corridor. Excavation will occur within 50 feet of the centerline of the roadway.

Early Coordination

As part of our early coordination effort for the referenced project, you are asked to study this enclosed information and provide a written evaluation of the potential impacts upon those resources under your jurisdiction. Please return a reply within thirty (30) days of receipt of this letter. If no reply has been received by this date, it will be indicated in the environmental documentation that your agency had no comment. Your cooperation in expediting the development of the referenced project is appreciated. If you have any questions, or if we can be of any further assistance, please contact this office at jvlach@b-l-n.com or 317-849-5832, ext. 3031. Thank you for your cooperation.

Very truly yours,
BEAM LONGEST NEFF

Jeffrey A. Vlach
Chief Environmental Analyst

cc:

Federal Highway Administration
US Army Corps of Engineers, Louisville District
Natural Resources Conservation Service
US National Park Service, Midwest Region
Indiana Geological Survey
Indiana Department of Natural Resources
Electronic Letter, Indiana Department of Environmental Management (IDEM)
Electronic Letter, IDEM Wellhead Protection
Indiana Department of Transportation (INDOT), Office of Communications
INDOT Scoping Manager, Greenfield District
INDOT, Division of Aviation
Jay County Community Development
Jay County Highway Engineer
Mayor, City of Portland

Graphics Provided in ECL are shown in Appendix B

EARLY COORDINATION MAILING LIST

Mr. Antonio Johnson
Federal Highway Administration
Federal Office Building, Room 254
575 North Pennsylvania Street, Room 254
Indianapolis, Indiana 46204

Ms. Kari Carmany - George
Environmental Scoping Manager
INDOT, Greenfield District
32 S. Broadway Street
Greenfield, Indiana 46140

Ms. Jill Reinhart
Acting State Conservationist
Natural Resources Conservation Service
6013 Lakeside Boulevard
Indianapolis, Indiana 46278

Mr. Nick Chevas
Regional Environmental Coordinator
Midwest Regional Office
National Park Service
601 Riverfront Drive
Omaha, Nebraska 68102

Ms. Nancy Hasenmueller, Section Head
Environmental Geology Section
Indiana Geological Survey
611 North Walnut Grove
Bloomington, Indiana 47405

Ms. Christie Stanifer, Environmental Coordinator
Indiana Department of Natural Resources
Division of Fish and Wildlife
402 West Washington St, Room W273
Indianapolis, IN 46204-2781

Mr. James Kinder
Indiana Department of Transportation, Aviation Section
Indiana Government Center North, Room N901
100 North Senate Avenue
Indianapolis, Indiana 46204

Mr. Rickie Clark, Public Hearings Manager
Indiana Department of Transportation
Office of Communications
100 North Senate Avenue, Room 642
Indianapolis, Indiana 46204

Indiana Department of Environmental Management
Electronic Early Coordination website

Mr. James Sullivan
Drinking Water Branch/Groundwater Section
Indiana Department of Environmental Management
100 North Senate Avenue
Indianapolis, IN 46204

Mr. Greg McKay
U.S. Army Corps of Engineers
Louisville District
ATTN: CERLR - RDN
P.O. Box 59
Louisville, KY 40201 – 0059

Mr. W. Chambers
Norfolk Southern Corporation
(Via E – mail)

Ms. Ami Huffman, Director
Jay County Community Development
118 S, Meridian Street
Portland, Indiana 47371

Mr. Daniel Watson, P.E.
Jay County Highway Engineer
1015 E, CR 200 North
Portland, Indiana 47371

The Honorable Randy Geesaman, Mayor
City of Portland
321 N. Meridian Street
Portland, Indiana 47371

From: Miller, Brandon
To: [Jeff Vlach](#)
Subject: Re: Des. No. 1600965 - Blaine Pike in Portland, Jay County, Indiana
Date: Friday, July 13, 2018 1:19:16 PM
Attachments: [image001.png](#)
[image002.png](#)
[image003.png](#)
[image004.png](#)

Mr. Vlach,

INDOT ESD has reviewed the ECL and has the following comments:

1. The letter does not state if federal funds are being used for the project. Please be explicit in future letters as to whether or not federal funds are used as this is an indicator to some agencies as to the depth of responses you will receive.
2. Blain Pike and Blaine Pike are both used within the letter. Are they both the same road? If they are different roads, please specify that they are different roads for making it clear to the coordinating agencies as it may influence their responses.
3. In the future, please include contact information for the project sponsor (the ERC for an LPA project or the current INDOT PM for an INDOT project).
4. In the subject line, do not provide the preferred alternative. You can discuss a likely alternative but the subject should not specify a predetermined solution. In the future you can use Pavement Project and non-motorized facility projects or similar language.

If you have any questions or concerns, please don't hesitate to contact me. Thanks.

Brandon Miller

NEPA Team Lead

INDOT Environmental Services

100 N. Senate Ave., Rm. 642

Indianapolis, IN 46204

Office: (317) 234-5108

Email: bramiller1@indot.in.gov





Organization and Project Information

Project ID: Blaine Pike Road Resurfacing/Multi Us Path
Des. ID: 1600965
Project Title: Blaine Pike Road Resurfacing/Multi Us Path
Name of Organization: Beam Longest Neff
Requested by: Jeff Vlach

Environmental Assessment Report

1. Geological Hazards:
 - Moderate liquefaction potential
 - Floodway
2. Mineral Resources:
 - Bedrock Resource: High Potential
 - Sand and Gravel Resource: Low Potential
3. Active or abandoned mineral resources extraction sites:
 - Abandoned Industrial Minerals Sand Gravel Pits

*All map layers from Indiana Map (maps.indiana.edu)

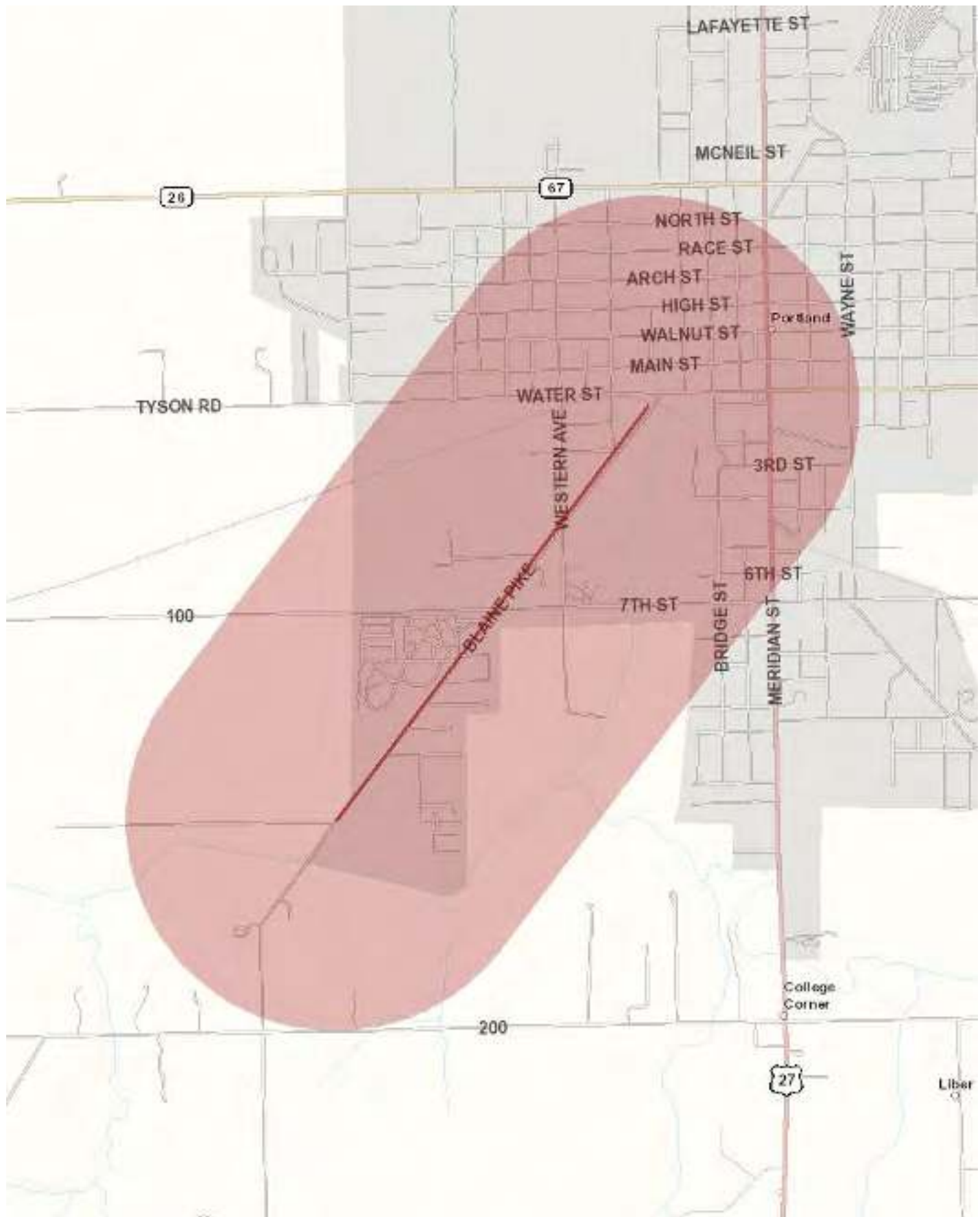
DISCLAIMER:

This document was compiled by Indiana University, Indiana Geological Survey, using data believed to be accurate; however, a degree of error is inherent in all data. This product is distributed "AS-IS" without warranties of any kind, either expressed or implied, including but not limited to warranties of suitability to a particular purpose or use. No attempt has been made in either the design or production of these data and document to define the limits or jurisdiction of any federal, state, or local government. The data used to assemble this document are intended for use only at the published scale of the source data or smaller (see the metadata links below) and are for reference purposes only. They are not to be construed as a legal document or survey instrument. A detailed on-the-ground survey and historical analysis of a single site may differ from these data and this document.

This information was furnished by Indiana Geological Survey
Address: 611 N. Walnut Grove Avenue, Bloomington, IN 47405-2208
Email: IGSEnvir@indiana.edu
Phone: 812 855-7428

Date: September 13, 2018





Metadata:

- https://maps.indiana.edu/metadata/Geology/Industrial_Minerals_Sand_Gravel_Pits_Abandoned.html
- https://maps.indiana.edu/metadata/Geology/Seismic_Earthquake_Liquefaction_Potential.html
- https://maps.indiana.edu/metadata/Geology/Industrial_Minerals_Sand_Gravel_Resources.html
- https://maps.indiana.edu/metadata/Hydrology/Floodplains_FIRM.html
- https://maps.indiana.edu/metadata/Geology/Bedrock_Geology.html

State of Indiana
DEPARTMENT OF NATURAL RESOURCES
Division of Fish and Wildlife
Early Coordination/Environmental Assessment

DNR #: ER-20629

Request Received: June 11, 2018

Requestor: Beam, Longest and Neff, LLC
Jeffery A Vlach
8125 Castleton Road
Indianapolis, IN 46250-2007

Project: Blaine Pike resurfacing with construction of a new 10' wide multi-use trail adjacent to the road from CR 150 South to the Norfolk Southern Railroad crossing, City of Portland; Des #1600955

County/Site info: Jay

The Indiana Department of Natural Resources has reviewed the above referenced project per your request. Our agency offers the following comments for your information and in accordance with the National Environmental Policy Act of 1969.

If our agency has regulatory jurisdiction over the project, the recommendations contained in this letter may become requirements of any permit issued. If we do not have permitting authority, all recommendations are voluntary.

Regulatory Assessment: Formal approval by the Department of Natural Resources under the regulatory programs administered by the Division of Water is not required for this project.

Natural Heritage Database: The Natural Heritage Program's data have been checked. To date, no plant or animal species listed as state or federally threatened, endangered, or rare have been reported to occur in the project vicinity.

Fish & Wildlife Comments: The measures below should be implemented to avoid, minimize, or compensate for impacts to fish, wildlife, and botanical resources:

1. Revegetate all bare and disturbed areas with a mixture of grasses (excluding all varieties of tall fescue) and legumes as soon as possible upon completion; low endophyte tall fescue may be used in the ditch bottom and side slopes only.
2. Minimize and contain within the project limits all tree and brush clearing.
3. Do not cut any trees suitable for Indiana bat or Northern Long-eared bat roosting (greater than 3 inches dbh, living or dead, with loose hanging bark, or with cracks, crevices, or cavities) from April 1 through September 30.
4. Appropriately designed measures for controlling erosion and sediment must be implemented to prevent sediment from entering the stream or leaving the construction site; maintain these measures until construction is complete and all disturbed areas are stabilized.
5. Plant five trees, at least 2 inches in diameter-at-breast height, for each tree which is removed that is ten inches or greater in diameter-at-breast height.

Contact Staff: Christie L. Stanifer, Environ. Coordinator, Fish & Wildlife
Our agency appreciates this opportunity to be of service. Please contact the above staff member at (317) 232-4080 if we can be of further assistance.



Christie L. Stanifer
Environ. Coordinator
Division of Fish and Wildlife

Date: July 6, 2018



INDIANA DEPARTMENT OF TRANSPORTATION

100 North Senate Avenue
Room N955
Indianapolis, Indiana 46204

PHONE: (317) 232-1477
FAX: (317) 232-1499

Eric Holcomb, Governor
Joe McGuinness, Commissioner

June 25, 2018

Mr. Jeffrey Vlach, Chief Environmental Analyst
Beam, Longest and Neff, L.L.C.
8126 Castleton Road
Indianapolis, IN 46250

Subject: Early Coordination Review (Des. No. 1600965)

Dear Mr. Vlach,

In response to your request on June 11, 2018 for early coordination review of a roadway resurfacing project and multiuse path construction along Blaine Pike in Portland, Jay County, Indiana; the Indiana Department of Transportation, Office of Aviation has reviewed the information and provides the following:

Are there any existing or proposed public-use airports within 5 nautical miles of the project limits (IC 8-21-10-6)?

The Portland Municipal Airport is located approximately 1.2 nautical miles north of the northernmost proposed project site.

Will an Indiana Tall Structure permit (IC 8-21-10-3-a) and/or Noise Sensitive (IC 8-21-10-3-b) permit be required?

Based upon the provided information, an Indiana Tall Structure permit would not be required unless the project involves the construction of a temporary (e.g., crane) or permanent structure that penetrates a 100:1 slope from the nearest point of the Portland Municipal Airport runway.

For any questions related to Indiana Tall Structure and/or Noise Sensitive permitting, please contact James Kinder at (317) 232-1485 or jkinder2@indot.in.gov.

Sincerely,

A handwritten signature in blue ink that reads "Adam French".

Adam French, MPA
Chief Airport Inspector, Office of Aviation
Indiana Department of Transportation



Indiana Department of Environmental Management

We Protect Hoosiers and Our Environment.

100 North Senate Avenue - Indianapolis, IN 46204
(800) 451-6027 - (317) 232-8603 - www.idem.IN.gov

City of Portland, Indiana
Ami Huffman
118 South Meridian Street, Suite B
Portland, IN 47371
Date

Beam, Longest, and Neff, LLC
Jeff Vlach
8126 Castleton Road
Indianapolis, IN 46250

To Engineers and Consultants Proposing Roadway Construction Projects:

RE: Resurfacing of Blaine Pike and the construction of a multi-use path adjacent to the corridor from CR 150 South to 131 feet south of the Norfolk Southern Railroad

This letter from the Indiana Department of Environmental Management (IDEM) serves as a standardized response to enquiries inviting IDEM comments on roadway construction, reconstruction, or other improvement projects within existing roadway corridors when the proposed scope of the project is beneath the threshold requiring a formal National Environmental Policy Act-mandated Environmental Assessment or Environmental Impact Statement. As the letter attempts to address all roadway-related environmental topics of potential concern, it is possible that not every topic addressed in the letter will be applicable to your particular roadway project.

For additional information on specific roadway-related topics of interest, please visit the appropriate Web pages cited below, many of which provide contact information for persons within the various program areas who can answer questions not fully addressed in this letter. Also please be mindful that some environmental requirements may be subject to change and so each person intending to include a copy of this letter in their project documentation packet is advised to download the most recently revised version of the letter; found at: <http://www.in.gov/idem/5283.htm> (<http://www.in.gov/idem/5283.htm>).

To ensure that all environmentally-related issues are adequately addressed, IDEM recommends that you read this letter in its entirety, and consider each of the following issues as you move forward with the planning of your proposed roadway construction, reconstruction, or improvement project:

WATER AND BIOTIC QUALITY

1. Section 404 of the Clean Water Act requires that you obtain a permit from the U.S. Army Corps of Engineers (USACE) before discharging dredged or fill materials into any wetlands or other waters, such as rivers, lakes, streams, and ditches. Other activities regulated include the relocation, channelization, widening, or other such alteration of a stream, and the mechanical clearing (use of heavy construction equipment) of wetlands. Thus, as a project owner or sponsor, it is your responsibility to ensure that no wetlands are disturbed without the proper permit. Although you may initially refer to the U.S. Fish and Wildlife Service National Wetland Inventory maps as a means of identifying potential areas of concern, please be mindful that those maps do not depict jurisdictional wetlands regulated by the USACE or the Department of Environmental Management. A valid jurisdictional wetlands determination can only be made by the USACE, using the 1987 Wetland Delineation Manual.

USACE recommends that you have a consultant check to determine whether your project will abut, or lie within, a wetland area. To view a list of consultants that have requested to be included on a list posted by the USACE on their Web site, see USACE Permits and Public Notices (<http://www.lrl.usace.army.mil/orf/default.asp>) (<http://www.lrl.usace.army.mil/orf/default.asp>) and then click on "Information" from the menu on the right-hand side of that page. Their "Consultant List" is the fourth entry down on the "Information" page. Please note that the USACE posts all consultants that request to appear on the list, and that inclusion of any particular consultant on the list does not represent an endorsement of that consultant by the USACE, or by IDEM.

Much of northern Indiana (Newton, Lake, Porter, LaPorte, St. Joseph, Elkhart, LaGrange, Steuben, and Dekalb counties; large portions of Jasper, Starke, Marshall, Noble, Allen, and Adams counties; and lesser portions of Benton, White, Pulaski, Kosciusko, and Wells counties) is served by the USACE District Office in Detroit (313-226-6812). The central and southern portions of the state (large portions of Benton, White, Pulaski, Kosciusko, and Wells counties; smaller portions of Jasper, Starke, Marshall, Noble, Allen, and Adams counties; and all other Indiana counties located in north-central, central, and southern Indiana) are served by the USACE Louisville District Office (502-315-6733).

Additional information on contacting these U.S. Army Corps of Engineers (USACE) District Offices, government agencies with jurisdiction over wetlands, and other water quality issues, can be found at <http://www.in.gov/idem/4396.htm> (<http://www.in.gov/idem/4396.htm>). IDEM recommends that impacts to wetlands and other water resources be avoided to the fullest extent.

2. In the event a Section 404 wetlands permit is required from the USACE, you also must obtain a Section 401 Water Quality Certification from the IDEM Office of Water Quality Wetlands Program. To learn more about the Wetlands Program, visit: <http://www.in.gov/idem/4384.htm> (<http://www.in.gov/idem/4384.htm>).
3. If the USACE determines that a wetland or other water body is isolated and not subject to Clean Water Act regulation, it is still regulated by the state of Indiana. A State Isolated Wetland permit from IDEM's Office of Water Quality (OWQ) is required for any activity that results in the discharge of dredged or fill materials into isolated wetlands. To learn more about isolated wetlands, contact the OWQ Wetlands Program at 317-233-8488.
4. If your project will involve over a 0.5 acre of wetland impact, stream relocation, or other large-scale alterations to water bodies such as the creation of a dam or a water diversion, you should seek additional input from the OWQ Wetlands Program staff. Consult the Web at: <http://www.in.gov/idem/4384.htm> (<http://www.in.gov/idem/4384.htm>) for the appropriate staff contact to further discuss your project.
5. Work within the one-hundred year floodway of a given water body is regulated by the Department of Natural Resources, Division of Water. The Division issues permits for activities regulated under the following statutes:
 - IC 14-26-2 Lakes Preservation Act 312 IAC 11
 - IC 14-26-5 Lowering of Ten Acre Lakes Act No related code
 - IC 14-28-1 Flood Control Act 310 IAC 6-1
 - IC 14-29-1 Navigable Waterways Act 312 IAC 6
 - IC 14-29-3 Sand and Gravel Permits Act 312 IAC 6

- IC 14-29-4 Construction of Channels Act No related code

For information on these Indiana (statutory) Code and Indiana Administrative Code citations, see the DNR Web site at: <http://www.in.gov/dnr/water/9451.htm> (<http://www.in.gov/dnr/water/9451.htm>) . Contact the DNR Division of Water at 317-232-4160 for further information.

The physical disturbance of the stream and riparian vegetation, especially large trees overhanging any affected water bodies should be limited to only that which is absolutely necessary to complete the project. The shade provided by the large overhanging trees helps maintain proper stream temperatures and dissolved oxygen for aquatic life.

6. For projects involving construction activity (which includes clearing, grading, excavation and other land disturbing activities) that result in the disturbance of one (1), or more, acres of total land area, contact the Office of Water Quality – Watershed Planning Branch (317/233-1864) regarding the need for of a Rule 5 Storm Water Runoff Permit. Visit the following Web page
 - <http://www.in.gov/idem/4902.htm> (<http://www.in.gov/idem/4902.htm>)

To obtain, and operate under, a Rule 5 permit you will first need to develop a Construction Plan (<http://www.in.gov/idem/4917.htm#constreq>) (<http://www.in.gov/idem/4917.htm#constreq>), and as described in 327 IAC 15-5-6.5 (<http://www.in.gov/legislative/iac/T03270/A00150.PDF>) (<http://www.in.gov/legislative/iac/T03270/A00150.PDF>), pages 16 through 19). Before you may apply for a Rule 5 Permit, or begin construction, you must submit your Construction Plan to your county Soil and Water Conservation District (SWCD) (<http://www.in.gov/isda/soil/contacts/map.html>) (<http://www.in.gov/isda/soil/contacts/map.html>).

Upon receipt of the construction plan, personnel of the SWCD or the Indiana Department of Environmental Management will review the plan to determine if it meets the requirements of 327 IAC 15-5. Plans that are deemed deficient will require re-submittal. If the plan is sufficient you will be notified and instructed to submit the verification to IDEM as part of the Rule 5 Notice of Intent (NOI) submittal. Once construction begins, staff of the SWCD or Indiana Department of Environmental Management will perform inspections of activities at the site for compliance with the regulation.

Please be mindful that approximately 149 Municipal Separate Storm Sewer System (MS4) areas are now being established by various local governmental entities throughout the state as part of the implementation of Phase II federal storm water requirements. All of these MS4 areas will eventually take responsibility for Construction Plan review, inspection, and enforcement. As these MS4 areas obtain program approval from IDEM, they will be added to a list of MS4 areas posted on the IDEM Website at: <http://www.in.gov/idem/4900.htm> (<http://www.in.gov/idem/4900.htm>).

If your project is located in an IDEM-approved MS4 area, please contact the local MS4 program about meeting their storm water requirements. Once the MS4 approves the plan, the NOI can be submitted to IDEM.

Regardless of the size of your project, or which agency you work with to meet storm water requirements, IDEM recommends that appropriate structures and techniques be utilized both during the construction phase, and after completion of the project, to minimize the impacts associated with storm water runoff. The use of appropriate planning and site development and appropriate storm water quality measures are recommended to prevent soil from leaving the construction site during active land disturbance and for post construction water quality concerns. Information and assistance regarding storm water related to construction activities are available from the Soil and Water Conservation District (SWCD) offices in each county or from IDEM.

7. For projects involving impacts to fish and botanical resources, contact the Department of Natural Resources - Division of Fish and Wildlife (317/232-4080) for addition project input.
8. For projects involving water main construction, water main extensions, and new public water supplies, contact the Office of Water Quality - Drinking Water Branch (317-308-3299) regarding the need for permits.
9. For projects involving effluent discharges to waters of the State of Indiana , contact the Office of Water Quality - Permits Branch (317-233-0468) regarding the need for a National Pollutant Discharge Elimination System (NPDES) permit.
10. For projects involving the construction of wastewater facilities and sewer lines, contact the Office of Water Quality - Permits Branch (317-232-8675) regarding the need for permits.

AIR QUALITY

The above-noted project should be designed to minimize any impact on ambient air quality in, or near, the project area. The project must comply with all federal and state air pollution regulations. Consideration should be given to the following:

1. Regarding open burning, and disposing of organic debris generated by land clearing activities; some types of open burning are allowed (<http://www.in.gov/idem/4148.htm>) (<http://www.in.gov/idem/4148.htm>) under specific conditions. You also can seek an open burning variance from IDEM.

However, IDEM generally recommends that you take vegetative wastes to a registered yard waste composting facility or that the waste be chipped or shredded with composting on site (you must register with IDEM if more than 2,000 pounds is to be composted; contact 317/232-0066). The finished compost can then be used as a mulch or soil amendment. You also may bury any vegetative wastes (such as leaves, twigs, branches, limbs, tree trunks and stumps) onsite, although burying large quantities of such material can lead to subsidence problems, later on.

Reasonable precautions must be taken to minimize fugitive dust emissions from construction and demolition activities. For example, wetting the area with water, constructing wind barriers, or treating dusty areas with chemical stabilizers (such as calcium chloride or several other commercial products). Dirt tracked onto paved roads from unpaved areas should be minimized.

Additionally, if construction or demolition is conducted in a wooded area where blackbirds have roosted or abandoned buildings or building sections in which pigeons or bats have roosted for 3-5 years precautionary measures should be taken to avoid an outbreak of histoplasmosis. This disease is caused by the fungus *Histoplasma capsulatum*, which stems from bird or bat droppings that have accumulated in one area for 3-5 years. The spores from this fungus become airborne when the area is disturbed and can cause infections over an entire community downwind of the site. The area should be wetted down prior to cleanup or demolition of the project site. For more detailed information on histoplasmosis prevention and control, please contact the Acute Disease Control Division of the Indiana State Department of Health at (317) 233-7272.

2. The U.S. EPA and the Surgeon General recommend that people not have long-term exposure to radon at levels above 4 pCi/L. (For a county-by-county map of predicted radon levels in Indiana, visit: <http://www.in.gov/idem/4145.htm> (<http://www.in.gov/idem/4145.htm>).

The U.S. EPA further recommends that all homes (and apartments within three stories of ground level) be tested for radon. If in-home radon levels are determined to be 4 pCi/L, or higher, EPA recommends a follow-up test. If the second test confirms that radon levels are 4 pCi/L, or higher, EPA recommends the installation of radon-reduction measures. (For a list of qualified radon testers and radon mitigation (or reduction) specialists visit: http://www.in.gov/isdh/regsvcs/radhealth/pdfs/radon_testers_mitigators_list.pdf (http://www.in.gov/isdh/regsvcs/radhealth/pdfs/radon_testers_mitigators_list.pdf)). It also is recommended that radon reduction measures be built into all new homes, particularly in areas like Indiana that have moderate to high predicted radon levels.

To learn more about radon, radon risks, and ways to reduce exposure visit: <http://www.in.gov/isdh/regsvcs/radhealth/radon.htm> (<http://www.in.gov/isdh/regsvcs/radhealth/radon.htm>), <http://www.in.gov/idem/4145.htm> (<http://www.in.gov/idem/4145.htm>), or <http://www.epa.gov/radon/index.html> (<http://www.epa.gov/radon/index.html>).

3. With respect to asbestos removal: all facilities slated for renovation or demolition (except residential buildings that have (4) four or fewer dwelling units and which will not be used for commercial purposes) must be inspected by an Indiana-licensed asbestos inspector prior to the commencement of any renovation or demolition activities. If regulated asbestos-containing material (RACM) that may become airborne is found, any subsequent demolition, renovation, or asbestos removal activities must be performed in accordance with the proper notification and emission control requirements.

If no asbestos is found where a renovation activity will occur, or if the renovation involves removal of less than 260 linear feet of RACM off of pipes, less than 160 square feet of RACM off of other facility components, or less than 35 cubic feet of RACM off of all facility components, the owner or operator of the project does not need to notify IDEM before beginning the renovation activity.

For questions on asbestos demolition and renovation activities, you can also call IDEM's Lead/Asbestos section at 1-888-574-8150.

However, in all cases where a demolition activity will occur (even if no asbestos is found), the owner or operator must still notify IDEM 10 working days prior to the demolition, using the form found at <http://www.in.gov/icpr/webfile/formsdiv/44593.pdf> (<http://www.in.gov/icpr/webfile/formsdiv/44593.pdf>).

Anyone submitting a renovation/demolition notification form will be billed a notification fee based upon the amount of friable asbestos containing material to be removed or demolished. Projects that involve the removal of more than 2,600 linear feet of friable asbestos containing materials on pipes, or 1,600 square feet or 400 cubic feet of friable asbestos containing material on other facility components, will be billed a fee of \$150 per project; projects below these amounts will be billed a fee of \$50 per project. All notification remitters will be billed on a quarterly basis.

For more information about IDEM policy regarding asbestos removal and disposal, visit: <http://www.in.gov/idem/4983.htm> (<http://www.in.gov/idem/4983.htm>).

4. With respect to lead-based paint removal: IDEM encourages all efforts to minimize human exposure to lead-based paint chips and dust. IDEM is particularly concerned that young children exposed to lead can suffer from learning disabilities. Although lead-based paint abatement efforts are not mandatory, any abatement that is conducted within housing built before January 1, 1978, or a child-occupied facility is required to comply with all lead-based paint work practice standards, licensing and notification requirements. For more information about lead-based paint removal visit: <http://www.in.gov/isdh/19131.htm> (<http://www.in.gov/isdh/19131.htm>).
5. Ensure that asphalt paving plants are permitted and operate properly. The use of cutback asphalt, or asphalt emulsion containing more than seven percent (7%) oil distillate, is prohibited during the months April through October. See 326 IAC 8-5-2, Asphalt Paving Rule (<http://www.ai.org/legislative/iac/T03260/A00080.PDF> (<http://www.ai.org/legislative/iac/T03260/A00080.PDF>)).
6. If your project involves the construction of a new source of air emissions or the modification of an existing source of air emissions or air pollution control equipment, it will need to be reviewed by the IDEM Office of Air Quality (OAQ). A registration or permit may be required under 326 IAC 2 (View at: www.ai.org/legislative/iac/T03260/a00020.pdf (<http://www.ai.org/legislative/iac/T03260/a00020.pdf>)). New sources that use or emit hazardous air pollutants may be subject to Section 112 of the Clean Air Act and corresponding state air regulations governing hazardous air pollutants.
7. For more information on air permits visit: <http://www.in.gov/idem/4223.htm> (<http://www.in.gov/idem/4223.htm>), or to initiate the IDEM air permitting process, please contact the Office of Air Quality Permit Reviewer of the Day at (317) 233-0178 or OAMPROD atdem.state.in.us.

LAND QUALITY

In order to maintain compliance with all applicable laws regarding contamination and/or proper waste disposal, IDEM recommends that:

1. If the site is found to contain any areas used to dispose of solid or hazardous waste, you need to contact the Office of Land Quality (OLQ) at 317-308-3103.
2. All solid wastes generated by the project, or removed from the project site, need to be taken to a properly permitted solid waste processing or disposal facility. For more information, visit <http://www.in.gov/idem/4998.htm> (<http://www.in.gov/idem/4998.htm>).
3. If any contaminated soils are discovered during this project, they may be subject to disposal as hazardous waste. Please contact the OLQ at 317-308-3103 to obtain information on proper disposal procedures.
4. If PCBs are found at this site, please contact the Industrial Waste Section of OLQ at 317-308-3103 for information regarding management of any PCB wastes from this site.
5. If there are any asbestos disposal issues related to this site, please contact the Industrial Waste Section of OLQ at 317-308-3103 for information regarding the management of asbestos wastes (Asbestos removal is addressed above, under Air Quality).
6. If the project involves the installation or removal of an underground storage tank, or involves contamination from an underground storage tank, you must contact the IDEM Underground Storage Tank program at 317/308-3039. See: <http://www.in.gov/idem/4999.htm> (<http://www.in.gov/idem/4999.htm>).

FINAL REMARKS

Should you need to obtain any environmental permits in association with this proposed project, please be mindful that IC 13-15-8 requires that you notify all adjoining property owners and/or occupants within ten days your submittal of each permit application. However, if you are seeking multiple permits, you can still meet the notification requirement with a single notice if all required permit applications are submitted with the same ten day period.

Should the scope of the proposed project be expanded to the extent that a National Environmental Policy Act Environmental Assessment (EA) or Environmental Impact Statement (EIS) is required, IDEM will actively participate in any early interagency coordination review of the project.

Meanwhile, please note that this letter does not constitute a permit, license, endorsement or any other form of approval on the part of the Indiana Department of Environmental Management regarding any project for which a copy of this letter is used. Also note that it is the responsibility of the project engineer or consultant using this letter to ensure that the most current draft of this document, which is located at <http://www.in.gov/ide/5284.htm> (<http://www.in.gov/ide/5284.htm>), is used.

Signature(s) of the Applicant

I acknowledge that the following proposed roadway project will be financed in part, or in whole, by public monies.

Project Description

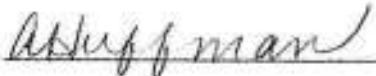
Resurfacing of Blaine Pike and the construction of a multi-use path adjacent to the corridor from CR 150 South to 131 feet south of the Norfolk Southern Railroad

With my signature, I do hereby affirm that I have read the letter from the Indiana Department of Environment that appears directly above. In addition, I understand that in order to complete that project in which I am interested, with a minimum of impact to the environment, I must consider all the issues addressed in the aforementioned letter, and further, that I must obtain any required permits.

Date: 6/12/18

Signature of the INDOT

Project Engineer or Other Responsible Agent



Ami Huffman

Date: 6/22/18

Signature of the

For Hire Consultant



Jeff Vlach



Indiana Department of Environmental Management

We Protect Hoosiers and Our Environment.

100 N. Senate Avenue • Indianapolis, IN 46204

(800) 451-6027 • (317) 232-8603 • www.idem.IN.gov

Eric J. Holcomb
Governor

Bruno Pigott
Commissioner

June 12, 2018

66-33

Beam Longest Neff
Attention: Mr. Jeffrey A. Vlach
8126 Castleton Road
Indianapolis, Indiana 46250

Dear Mr. Jeffrey A. Vlach,

RE: Wellhead Protection Area
Proximity Determination
Des No 1600965
Blaine Pike Resurfacing and
Multi-Use Path Construction
CR 150 South to South of Norfolk
Southern Railroad
Portland, Jay County, Indiana

Upon review of the above referenced project site, it has been determined that the proposed project area **is not located within** a Wellhead Protection Area. However, the project is located within a tenth of a mile of a Wellhead Protection Area. If the contact information is needed for the WHPA, please contact the reference located at the bottom of the letter for the appropriate information. The information is accurate to the best of our knowledge; however, there are in some cases a few factors that could impact the accuracy of this determination. Some Wellhead Protection Area Delineations have not been submitted, and many have not been approved by this office. In these cases we use a 3,000 foot fixed radius buffer to make the proximity determination. To find the status of a Public Water Supply System's (PWSS's) Wellhead Protection Area Delineation please visit our tracking database at <http://www.in.gov/ide/cleanwater/2456.htm> and scroll to the bottom of the page.

Note: the Drinking Water Branch has launched a new self service feature which allows one to determine wellhead proximity without submitting the application form. Use the following instructions:

1. Go to <http://idemmaps.idem.in.gov/whpa2/>
2. Use the search tool located in the upper left hand corner of the application to zoom to your site of interest by way of city, county, or address; or use the mouse to click on the site of interest displayed on the map.
3. Once the site of interest has been located and selected, use the print tool to create a .pdf of a wellhead protection area proximity determination response.

In the future please consider using this self service feature if it suits your needs.

If you have any additional questions please feel free to contact me at the address above or at (317) 233-9158 and aturnbow@idem.in.gov.

Sincerely,

Alisha Turnbow, Environmental Manager,
Ground Water Section, Drinking Water
Branch, Office of Water Quality



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United States Department of the Interior

FISH AND WILDLIFE SERVICE

Indiana Ecological Services Field Office

620 South Walker Street

Bloomington, IN 47403-2121

Phone: (812) 334-4261 Fax: (812) 334-4273

<http://www.fws.gov/midwest/Endangered/section7/s7process/step1.html>



In Reply Refer To:

July 19, 2019

Consultation Code: 03E12000-2019-SLI-1354

Event Code: 03E12000-2019-E-06031

Project Name: Des #1600965 Blaine Pike Road Resurface and Non-motorized Path Project

Subject: List of threatened and endangered species that may occur in your proposed project location, and/or may be affected by your proposed project

To Whom It May Concern:

The attached species list identifies any federally threatened, endangered, proposed and candidate species that may occur within the boundary of your proposed project or may be affected by your proposed project. The list also includes designated critical habitat if present within your proposed project area or affected by your project. This list is provided to you as the initial step of the consultation process required under section 7(c) of the Endangered Species Act, also referred to as Section 7 Consultation.

Section 7 of the Endangered Species Act of 1973 requires that actions authorized, funded, or carried out by Federal agencies not jeopardize federally threatened or endangered species or adversely modify designated critical habitat. To fulfill this mandate, Federal agencies (or their designated non-federal representative) must consult with the Service if they determine their project "may affect" listed species or critical habitat.

Under 50 CFR 402.12(e) (the regulations that implement Section 7 of the Endangered Species Act) the accuracy of this species list should be verified after 90 days. This verification can be completed formally or informally. You may verify the list by visiting the ECOS-IPaC website <http://ecos.fws.gov/ipac/> at regular intervals during project planning and implementation and completing the same process you used to receive the attached list. As an alternative, you may contact this Ecological Services Field Office for updates.

Please use the species list provided and visit the U.S. Fish and Wildlife Service's Region 3 Section 7 Technical Assistance website at - <http://www.fws.gov/midwest/endangered/section7/s7process/index.html>. This website contains step-by-step instructions which will help you

determine if your project will have an adverse effect on listed species and will help lead you through the Section 7 process.

For all **wind energy projects** and **projects that include installing towers that use guy wires or are over 200 feet in height**, please contact this field office directly for assistance, even if no federally listed plants, animals or critical habitat are present within your proposed project or may be affected by your proposed project.

Although no longer protected under the Endangered Species Act, be aware that bald eagles are protected under the Bald and Golden Eagle Protection Act (16 U.S.C. 668 *et seq.*) and Migratory Bird Treaty Act (16 U.S.C. 703 *et seq.*), as are golden eagles. Projects affecting these species may require measures to avoid harming eagles or may require a permit. If your project is near an eagle nest or winter roost area, see our Eagle Permits website at <http://www.fws.gov/midwest/midwestbird/EaglePermits/index.html> to help you determine if you can avoid impacting eagles or if a permit may be necessary.

We appreciate your concern for threatened and endangered species. Please include the Consultation Tracking Number in the header of this letter with any request for consultation or correspondence about your project that you submit to our office.

Attachment(s):

- Official Species List

Official Species List

This list is provided pursuant to Section 7 of the Endangered Species Act, and fulfills the requirement for Federal agencies to "request of the Secretary of the Interior information whether any species which is listed or proposed to be listed may be present in the area of a proposed action".

This species list is provided by:

Indiana Ecological Services Field Office

620 South Walker Street

Bloomington, IN 47403-2121

(812) 334-4261

Counties: Jay, IN

Endangered Species Act Species

There is a total of 2 threatened, endangered, or candidate species on this species list.

Species on this list should be considered in an effects analysis for your project and could include species that exist in another geographic area. For example, certain fish may appear on the species list because a project could affect downstream species. Note that 1 of these species should be considered only under certain conditions.

IPaC does not display listed species or critical habitats under the sole jurisdiction of NOAA Fisheries¹, as USFWS does not have the authority to speak on behalf of NOAA and the Department of Commerce.

See the "Critical habitats" section below for those critical habitats that lie wholly or partially within your project area under this office's jurisdiction. Please contact the designated FWS office if you have questions.

-
1. [NOAA Fisheries](#), also known as the National Marine Fisheries Service (NMFS), is an office of the National Oceanic and Atmospheric Administration within the Department of Commerce.

Mammals

NAME	STATUS
Indiana Bat <i>Myotis sodalis</i> There is final critical habitat for this species. Your location is outside the critical habitat. Species profile: https://ecos.fws.gov/ecp/species/5949 Species survey guidelines: https://ecos.fws.gov/ipac/guideline/survey/population/1/office/31440.pdf	Endangered
Northern Long-eared Bat <i>Myotis septentrionalis</i> No critical habitat has been designated for this species. This species only needs to be considered under the following conditions: <ul style="list-style-type: none">▪ Incidental take of the NLEB is not prohibited here. Federal agencies may consult using the 4(d) rule streamlined process. Transportation projects may consult using the programmatic process. See www.fws.gov/midwest/endangered/mammals/nleb/index.html Species profile: https://ecos.fws.gov/ecp/species/9045	Threatened

Critical habitats

THERE ARE NO CRITICAL HABITATS WITHIN YOUR PROJECT AREA UNDER THIS OFFICE'S JURISDICTION.



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Phone: (812) 334-4261 Fax: (812) 334-4273

<http://www.fws.gov/midwest/Endangered/section7/s7process/step1.html>

In Reply Refer To:

July 21, 2019

Consultation Code: 03E12000-2019-I-1354

Event Code: 03E12000-2019-E-06088

Project Name: Des #1600965 Blaine Pike Road Resurface and Non-motorized Path Project

Subject: Concurrence verification letter for the 'Des #1600965 Blaine Pike Road Resurface and Non-motorized Path Project' project under the revised February 5, 2018, FHWA, FRA, FTA Programmatic Biological Opinion for Transportation Projects within the Range of the Indiana Bat and Northern Long-eared Bat.

To whom it may concern:

The U.S. Fish and Wildlife Service (Service) has received your request dated to verify that the **Des #1600965 Blaine Pike Road Resurface and Non-motorized Path Project** (Proposed Action) may rely on the concurrence provided in the February 5, 2018, FHWA, FRA, FTA Programmatic Biological Opinion for Transportation Projects within the Range of the Indiana Bat and Northern Long-eared Bat (PBO) to satisfy requirements under Section 7(a)(2) of the Endangered Species Act of 1973 (ESA) (87 Stat. 884, as amended; 16 U.S.C 1531 *et seq.*).

Based on the information you provided (Project Description shown below), you have determined that the Proposed Action is within the scope and adheres to the criteria of the PBO, including the adoption of applicable avoidance and minimization measures, may affect, but is not likely to adversely affect (NLAA) the endangered Indiana bat (*Myotis sodalis*) and/or the threatened Northern long-eared bat (*Myotis septentrionalis*).

The Service has 14 calendar days to notify the lead Federal action agency or designated non-federal representative if we determine that the Proposed Action does not meet the criteria for a NLAA determination under the PBO. If we do not notify the lead Federal action agency or designated non-federal representative within that timeframe, you may proceed with the Proposed Action under the terms of the NLAA concurrence provided in the PBO. This verification period allows Service Field Offices to apply local knowledge to implementation of the PBO, as we may identify a small subset of actions having impacts that were unanticipated. In such instances, Service Field Offices may request additional information that is necessary to verify inclusion of the proposed action under the PBO.

For Proposed Actions that include bridge/structure removal, replacement, and/or maintenance activities: If your initial bridge/structure assessments failed to detect Indiana bats, but you later detect bats during construction, please submit the Post Assessment Discovery of Bats at Bridge/Structure Form (User Guide Appendix E) to this Service Office. In these instances, potential incidental take of Indiana bats may be exempted provided that the take is reported to the Service.

If the Proposed Action is modified, or new information reveals that it may affect the Indiana bat and/or Northern long-eared bat in a manner or to an extent not considered in the PBO, further review to conclude the requirements of ESA Section 7(a)(2) may be required. If the Proposed Action may affect any other federally-listed or proposed species, and/or any designated critical habitat, additional consultation is required. If the proposed action has the potential to take bald or golden eagles, additional coordination with the Service under the Bald and Golden Eagle Protection Act may also be required. In either of these circumstances, please contact this Service Office.

Project Description

The following project name and description was collected in IPaC as part of the endangered species review process.

Name

Des #1600965 Blaine Pike Road Resurface and Non-motorized Path Project

Description

The proposed project involves non-motorized path improvements and road resurfacing along Blaine Pike between CR W 150 S and the Norfolk Southern Railroad in the City of Portland, Indiana. The scope of the work involves construction of a new shared use path from Bittersweet Lane to approximately 180 feet south of the Norfolk Southern Railroad. Blaine Pike will be milled and resurfaced from CR 150 S to the same terminus as the shared use path. The project length is approximately 1.2 miles and is in sections 20, 29, and 30, Township 23 North, Range 14 East, Wayne Township, Jay County, Indiana.

Approximately 2.79 acres of permanent and 0.003 acres of temporary right-of-way will be needed to complete this project. 24 trees will be cleared for this project during the bat inactive season. No permanent lighting will be installed for this project.

Construction is estimated to begin in the Spring of 2020.

Determination Key Result

Based on your answers provided, this project(s) may affect, but is not likely to adversely affect the endangered Indiana bat and/or the threatened Northern long-eared bat. Therefore, consultation with the U.S. Fish and Wildlife Service pursuant to Section 7(a)(2) of the Endangered Species Act of 1973 (ESA) (87 Stat. 884, as amended 16 U.S.C. 1531 *et seq.*) is required. However, also based on your answers provided, this project may rely on the concurrence provided in the revised February 5, 2018, FHWA, FRA, FTA Programmatic Biological Opinion for Transportation Projects within the Range of the Indiana Bat and Northern Long-eared Bat.

Qualification Interview

1. Is the project within the range of the Indiana bat^[1]?

[1] See [Indiana bat species profile](#)

Automatically answered

Yes

2. Is the project within the range of the Northern long-eared bat^[1]?

[1] See [Northern long-eared bat species profile](#)

Automatically answered

Yes

3. Which Federal Agency is the lead for the action?

A) Federal Highway Administration (FHWA)

4. Are *all* project activities limited to non-construction^[1] activities only? (examples of non-construction activities include: bridge/abandoned structure assessments, surveys, planning and technical studies, property inspections, and property sales)

[1] Construction refers to activities involving ground disturbance, percussive noise, and/or lighting.

No

5. Does the project include *any* activities that are **greater than** 300 feet from existing road/rail surfaces^[1]?

[1] Road surface is defined as the actively used [e.g. motorized vehicles] driving surface and shoulders [may be pavement, gravel, etc.] and rail surface is defined as the edge of the actively used rail ballast.

No

6. Does the project include *any* activities **within** 0.5 miles of a known Indiana bat and/or NLEB hibernaculum^[1]?

[1] For the purpose of this consultation, a hibernaculum is a site, most often a cave or mine, where bats hibernate during the winter (see suitable habitat), but could also include bridges and structures if bats are found to be hibernating there during the winter.

No

7. Is the project located **within** a karst area?

No

8. Is there *any* suitable^[1] summer habitat for Indiana Bat or NLEB **within** the project action area^[2]? (includes any trees suitable for maternity, roosting, foraging, or travelling habitat)

[1] See the Service's [summer survey guidance](#) for our current definitions of suitable habitat.

[2] The action area is defined as all areas to be affected directly or indirectly by the Federal action and not merely the immediate area involved in the action (50 CFR Section 402.02). Further clarification is provided by the [national consultation FAQs](#).

Yes

9. Will the project remove *any* suitable summer habitat^[1] and/or remove/trim any existing trees **within** suitable summer habitat?

[1] See the Service's [summer survey guidance](#) for our current definitions of suitable habitat.

Yes

10. Will the project clear more than 20 acres of suitable habitat per 5-mile section of road/rail?

No

11. Have presence/probable absence (P/A) summer surveys^{[1][2]} been conducted^{[3][4]} **within** the suitable habitat located within your project action area?

[1] See the Service's [summer survey guidance](#) for our current definitions of suitable habitat.

[2] Presence/probable absence summer surveys conducted within the fall swarming/spring emergence home range of a documented Indiana bat hibernaculum (contact local Service Field Office for appropriate distance from hibernacula) that result in a negative finding requires additional consultation with the local Service Field Office to determine if clearing of forested habitat is appropriate and/or if seasonal clearing restrictions are needed to avoid and minimize potential adverse effects on fall swarming and spring emerging Indiana bats.

[3] For projects within the range of either the Indiana bat or NLEB in which suitable habitat is present, and no bat surveys have been conducted, the transportation agency will assume presence of the appropriate species. This assumption of presence should be based upon the presence of suitable habitat and the capability of bats to occupy it because of their mobility.

[4] Negative presence/probable absence survey results obtained using the [summer survey guidance](#) are valid for a minimum of two years from the completion of the survey unless new information (e.g., other nearby surveys) suggest otherwise.

No

12. Does the project include activities **within documented Indiana bat habitat**^{[1][2]}?

[1] Documented roosting or foraging habitat – for the purposes of this consultation, we are considering documented habitat as that where Indiana bats and/or NLEB have actually been captured and tracked using (1) radio telemetry to roosts; (2) radio telemetry biangulation/triangulation to estimate foraging areas; or (3) foraging areas with repeated use documented using acoustics. Documented roosting habitat is also considered as suitable summer habitat within 0.25 miles of documented roosts.)

[2] For the purposes of this key, we are considering documented corridors as that where Indiana bats and/or NLEB have actually been captured and tracked to using (1) radio telemetry; or (2) treed corridors located directly between documented roosting and foraging habitat.

No

13. Will the removal or trimming of habitat or trees occur **within** suitable but **undocumented Indiana bat** roosting/foraging habitat or travel corridors?

Yes

14. What time of year will the removal or trimming of habitat or trees **within** suitable but **undocumented Indiana bat** roosting/foraging habitat or travel corridors occur^[1]?

[1] Coordinate with the local Service Field Office for appropriate dates.

B) During the inactive season

15. Does the project include activities **within documented NLEB habitat**^{[1][2]}?

[1] Documented roosting or foraging habitat – for the purposes of this consultation, we are considering documented habitat as that where Indiana bats and/or NLEB have actually been captured and tracked using (1) radio telemetry to roosts; (2) radio telemetry biangulation/triangulation to estimate foraging areas; or (3) foraging areas with repeated use documented using acoustics. Documented roosting habitat is also considered as suitable summer habitat within 0.25 miles of documented roosts.)

[2] For the purposes of this key, we are considering documented corridors as that where Indiana bats and/or NLEB have actually been captured and tracked to using (1) radio telemetry; or (2) treed corridors located directly between documented roosting and foraging habitat.

No

16. Will the removal or trimming of habitat or trees occur **within** suitable but **undocumented NLEB** roosting/foraging habitat or travel corridors?

Yes

17. What time of year will the removal or trimming of habitat or trees **within** suitable but **undocumented NLEB** roosting/foraging habitat or travel corridors occur?

B) During the inactive season

18. Will *any* tree trimming or removal occur **within** 100 feet of existing road/rail surfaces?

Yes

19. Will the tree removal alter *any* **documented** Indiana bat or NLEB roosts and/or alter any surrounding summer habitat **within** 0.25 mile of a documented roost?

No

20. Will *any* tree trimming or removal occur **between** 100-300 feet of existing road/rail surfaces?

No

21. Are *all* trees that are being removed clearly demarcated?

Yes

22. Will the removal of habitat or the removal/trimming of trees include installing new or replacing existing **permanent** lighting?

No

23. Does the project include wetland or stream protection activities associated with compensatory wetland mitigation?

No

24. Does the project include slash pile burning?

No

25. Does the project include *any* bridge removal, replacement, and/or maintenance activities (e.g., any bridge repair, retrofit, maintenance, and/or rehabilitation work)?

No

26. Does the project include the removal, replacement, and/or maintenance of *any* structure other than a bridge? (e.g., rest areas, offices, sheds, outbuildings, barns, parking garages, etc.)

No

27. Will the project involve the use of **temporary** lighting *during* the active season?

Yes

28. Is there *any* suitable habitat **within** 1,000 feet of the location(s) where **temporary** lighting will be used?

Yes

29. Will the project install new or replace existing **permanent** lighting?

No

30. Does the project include percussives or other activities (**not including tree removal/trimming or bridge/structure work**) that will increase noise levels above existing traffic/background levels?

No

31. Are *all* project activities that are **not associated with** habitat removal, tree removal/trimming, bridge and/or structure activities, temporary or permanent lighting, or use of percussives, limited to actions that DO NOT cause any additional stressors to the bat species?

Examples: lining roadways, unlighted signage , rail road crossing signals, signal lighting, and minor road repair such as asphalt fill of potholes, etc.

Yes

32. Will the project raise the road profile **above the tree canopy**?

No

33. Are the project activities that are not associated with habitat removal, tree removal/trimming, bridge and/or structure activities, temporary or permanent lighting, or use of percussives consistent with a No Effect determination in this key?

Automatically answered

Yes, other project activities are limited to actions that DO NOT cause any additional stressors to the bat species as described in the BA/BO

34. Is the habitat removal portion of this project consistent with a Not Likely to Adversely Affect determination in this key?

Automatically answered

Yes, because the tree removal/trimming that occurs outside of the active season occurs greater than 0.5 miles from the nearest hibernaculum, is less than 100 feet from the existing road/rail surface, includes clear demarcation of the trees that are to be removed, and does not alter documented roosts and/or surrounding summer habitat within 0.25 miles of a documented roost

35. Is the habitat removal portion of this project consistent with a Not Likely to Adversely Affect determination in this key?

Automatically answered

Yes, because the tree removal/trimming that occurs outside of the active season occurs greater than 0.5 miles from the nearest hibernaculum, is less than 100 feet from the existing road/rail surface, includes clear demarcation of the trees that are to be removed, and does not alter documented roosts and/or surrounding summer habitat within 0.25 miles of a documented roost

36. **General AMM 1**

Will the project ensure *all* operators, employees, and contractors working in areas of known or presumed bat habitat are aware of *all* FHWA/FRA/FTA (Transportation Agencies) environmental commitments, including all applicable Avoidance and Minimization Measures?

Yes

37. **Tree Removal AMM 1**

Can *all* phases/aspects of the project (e.g., temporary work areas, alignments) be modified, to the extent practicable, to avoid tree removal^[1] in excess of what is required to implement the project safely?

Note: Tree Removal AMM 1 is a minimization measure, the full implementation of which may not always be practicable. Projects may still be NLAA as long as Tree Removal AMMs 2, 3, and 4 are implemented and LAA as long as Tree Removal AMMs 3, 5, 6, and 7 are implemented.

[1] The word “trees” as used in the AMMs refers to trees that are suitable habitat for each species within their range. See the USFWS’ current summer survey guidance for our latest definitions of suitable habitat.

Yes

38. **Tree Removal AMM 2**

Can *all* tree removal activities be restricted to when Indiana bats are not likely to be present (e.g., the inactive season)^[1]?

[1] Coordinate with the local Service Field Office for appropriate dates.

Automatically answered

Yes

39. **Tree Removal AMM 2**

Can *all* tree removal activities be restricted to when Northern long-eared bats are not likely to be present (e.g., the inactive season)^[1]?

[1] Coordinate with the local Service Field Office for appropriate dates.

Automatically answered

Yes

40. **Tree Removal AMM 3**

Can tree removal be limited to that specified in project plans and ensure that contractors understand clearing limits and how they are marked in the field (e.g., install bright colored flagging/fencing prior to any tree clearing to ensure contractors stay within clearing limits)?

Yes

41. **Tree Removal AMM 4**

Can the project avoid cutting down/removal of *all* (1) **documented**^[1] Indiana bat or NLEB roosts^[2] (that are still suitable for roosting), (2) trees **within** 0.25 miles of roosts, and (3) documented foraging habitat any time of year?

[1] The word documented means habitat where bats have actually been captured and/or tracked.

[2] Documented roosting or foraging habitat – for the purposes of this consultation, we are considering documented habitat as that where Indiana bats and/or NLEB have actually been captured and tracked using (1) radio telemetry to roosts; (2) radio telemetry biangulation/triangulation to estimate foraging areas; or (3) foraging areas with repeated use documented using acoustics. Documented roosting habitat is also considered as suitable summer habitat within 0.25 miles of documented roosts.)

Yes

42. **Lighting AMM 1**

Will *all* **temporary** lighting used during the removal of suitable habitat and/or the removal/trimming of trees within suitable habitat be directed away from suitable habitat during the active season?

Yes

43. **Lighting AMM 1**

Will *all* **temporary** lighting be directed away from suitable habitat during the active season?

Yes

Project Questionnaire

1. Have you made a No Effect determination for *all* other species indicated on the FWS IPaC generated species list?

N/A

2. Have you made a May Affect determination for *any* other species on the FWS IPaC generated species list?

N/A

3. How many acres^[1] of trees are proposed for removal between 0-100 feet of the existing road/rail surface?

[1] If described as number of trees, multiply by 0.09 to convert to acreage and enter that number.

2.16

Avoidance And Minimization Measures (AMMs)

This determination key result includes the commitment to implement the following Avoidance and Minimization Measures (AMMs):

GENERAL AMM 1

Ensure all operators, employees, and contractors working in areas of known or presumed bat habitat are aware of all FHWA/FRA/FTA (Transportation Agencies) environmental commitments, including all applicable AMMs.

LIGHTING AMM 1

Direct temporary lighting away from suitable habitat during the active season.

TREE REMOVAL AMM 1

Modify all phases/aspects of the project (e.g., temporary work areas, alignments) to avoid tree removal.

TREE REMOVAL AMM 2

Apply time of year restrictions for tree removal when bats are not likely to be present, or limit tree removal to 10 or fewer trees per project at any time of year within 100 feet of existing road/rail surface and **outside of documented** roosting/foraging habitat or travel corridors; visual emergence survey must be conducted with no bats observed.

TREE REMOVAL AMM 3

Ensure tree removal is limited to that specified in project plans and ensure that contractors understand clearing limits and how they are marked in the field (e.g., install bright colored flagging/fencing prior to any tree clearing to ensure contractors stay within clearing limits).

TREE REMOVAL AMM 4

Do not remove **documented** Indiana bat or NLEB roosts that are still suitable for roosting, or trees within 0.25 miles of roosts, or **documented** foraging habitat any time of year.

Determination Key Description: FHWA, FRA, FTA Programmatic Consultation For Transportation Projects Affecting NLEB Or Indiana Bat

This key was last updated in IPaC on March 16, 2018. Keys are subject to periodic revision.

This decision key is intended for projects/activities funded or authorized by the Federal Highway Administration (FHWA), Federal Railroad Administration (FRA), and/or Federal Transit Administration (FTA), which require consultation with the U.S. Fish and Wildlife Service (Service) under Section 7 of the Endangered Species Act (ESA) for the endangered **Indiana bat** (*Myotis sodalis*) and the threatened **Northern long-eared bat** (NLEB) (*Myotis septentrionalis*).

This decision key should only be used to verify project applicability with the Service's [February 5, 2018, FHWA, FRA, FTA Programmatic Biological Opinion for Transportation Projects](#). The programmatic biological opinion covers limited transportation activities that may affect either bat species, and addresses situations that are both likely and not likely to adversely affect either bat species. This decision key will assist in identifying the effect of a specific project/activity and applicability of the programmatic consultation. The programmatic biological opinion is not intended to cover all types of transportation actions. Activities outside the scope of the programmatic biological opinion, or that may affect ESA-listed species other than the Indiana bat or NLEB, or any designated critical habitat, may require additional ESA Section 7 consultation.

May 26, 2020

Brian Shaw
Beam, Longest and Neff, L.L.C.
8320 Craig Street
Indianapolis, Indiana 46250

Dear Mr. Shaw:

The proposed project to construct a multi-use path along County Road 150 South in Portland, Jay County, Indiana (Des. No. 1600965), as referred to in your letter received June 11, 2018, will not cause a conversion of prime farmland.

If you need additional information, please contact John Allen at 317-295-5859.

Sincerely,

RICK NEILSON
State Soil Scientist



Appendix D:

Section 106 of the NHPA

Blaine Pike Pedestrian Improvements Project

Portland, Wayne Township, Jay County, Indiana

Des. No. 1600965; DHPA No. 23454



800.11(d) Documentation and Effects Finding

January 6, 2019

Prepared for:

Beam, Longest, and Neff

By:

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**FEDERAL HIGHWAY ADMINISTRATION'S
SECTION 4(f) COMPLIANCE REQUIREMENTS (for historic properties)
AND SECTION 106 FINDINGS AND DETERMINATIONS
AREA OF POTENTIAL EFFECT
ELIGIBILITY DETERMINATIONS
EFFECTS FINDING
Blaine Pike Pedestrian Improvements,
Portland, Wayne Township, Jay County, Indiana
Des. No. 1600965; DHPA 23454**

**AREA OF POTENTIAL EFFECTS
(Pursuant to 36 CFR Section 800.4(a)(1))**

The APE of the project includes all properties adjacent to the project and those with a proximate viewshed of the project. The APE consists of recently developed residential neighborhoods, a religious institution and school, and scattered single-dwelling houses not part of a development. Please see Appendix A for a map of the APE.

**ELIGIBILITY DETERMINATIONS
(Pursuant to 36 CFR 800.4(c)(2))**

No properties within the APE are listed in or recommended eligible for listing in the National Register of Historic Places (NRHP).

**EFFECT FINDING
(Pursuant to 36 CFR 800.4(d)(1))**

The Indiana Department of Transportation (INDOT), acting on behalf of the Federal Highway Administration (FHWA), determined a "No Historic Properties Affected" finding was appropriate for this undertaking.

INDOT respectfully requests the Indiana State Historic Preservation Officer provide written concurrence with the Section 106 determination of effect.

**SECTION 106/SECTION 4(f) COMPLIANCE REQUIREMENTS
(For historic properties)**

This undertaking will not convert property from any Section 4(f) historic property to a transportation use; the INDOT, acting on FHWA's behalf, has determined the appropriate Section 106 finding is "No Historic Properties Affected"; therefore, no Section 4(f) evaluation is required.

Anuradha Kumar V.

Anuradha V. Kumar, for FHWA
Manager
INDOT Cultural Resources

01/31/2020
Approval Date

**FEDERAL HIGHWAY ADMINISTRATION
DOCUMENTATION OF SECTION 106 FINDING OF
NO HISTORIC PROPERTIES AFFECTED
SUBMITTED TO THE STATE HISTORIC PRESERVATION OFFICER
PURSUANT TO 36 CFR Section 800.4(d)(1)
Blaine Pike Pedestrian Improvements,
Portland, Wayne Township, Jay County, Indiana
Des. No. 1600965; DHPA 23454**

1. DESCRIPTION OF THE UNDERTAKING

The City of Portland, with funding from the Federal Highway Administration (FHWA) and administrative oversight from the Indiana Department of Transportation (INDOT), proposes to proceed with the Blaine Pike Pedestrian Improvements Project, Des. No. 1600965; DHPA 23454. The proposed undertaking is located on Blaine Pike from CR 150 S to approximately 175 feet (ft.) south of the Norfolk Southern Railroad crossing in Jay County, Indiana.

The purpose of this project is to improve safety of the Blaine Pike corridor for both the increasing vehicle traffic and pedestrians and non-motorized traffic. The need for this project is to address the deficiencies of the roadway and lack of connectivity in pedestrian facilities along Blaine Pike.

Blaine Pike is classified as a minor arterial facility consisting of a 10-10.5-foot-wide, two-lane asphalt roadway with little to no shoulders. Blaine Pike serves as one of the primary routes for access into the City of Portland from rural Jay County. A curbed parking lane affords on-street parking from 7th St. to Western Avenue on the east side of Blaine Pike; a second curbed parking lane exists on the west side of the Blaine Pike at Sesame St.

The project proposes the milling and resurfacing of Blaine Pike along the entire project corridor and construction of a 10-foot-wide multi-use trail on the east side of the roadway from Bittersweet Lane to approximately 235 ft. south of the Norfolk Southern Railroad crossing. The trail will cross side roads and driveways in which the project proposes to construct crosswalks and curb ramps that meet Americans with Disabilities Act (ADA) compliance. A mid-block crossing with solar-powered flashing beacons will be constructed just north of the intersection of Blaine Pike and Western Avenue adjacent to Memorial Park. Ditches on the east side of Blaine Pike will be relocated due to the multi-use trail.

The previous report distribution letter dated October 7, 2019, stated that the existing corrugated metal pipe (CMP), located north of 3rd St. will be extended to accommodate the inclusion of the new multi-use trail. While the proposed work is correct, the location of the CMP was inaccurate. The existing CMP that will be extended is located approximately 450 ft. *south* of 3rd St. For this culvert extension, there will be approximately 400 square feet (0.0092 acre) of permanent right-of-way will be taken from the west side of Blaine Pike (see Appendix E-29).

Currently, it is anticipated that approximately 2.79 acres of permanent and 0.003 acre of temporary right-of-way will be acquired. Construction is anticipated to begin in 2022. No relocations of residents or businesses are expected for this project. See the most current plans in Appendix E.

The proposed project is located within an urban setting of the City of Portland. Land use surrounding the project area is agricultural at the southern end of the project, residential throughout the middle of the project area, and commercial and light industrial at the northern end of the project. The Green Park Cemetery is located on the west side of Blaine Pike. The topography of the project area is mostly flat on the east side of

Blaine Pike. Landscaped lawns and mature trees run along both the west and east sides of Blaine Pike. A total of thirteen (13) residential and city streets, bisect the project area.

The Area of Potential Effects (APE) is the area in which the proposed project may cause alterations in the character or use of historic resources. The APE of the project includes all properties adjacent to the project and those with a proximate viewshed of the project. The APE extends approximately 0.12 mile east of the project at its widest point, 0.10 mile west of the project at its widest point, 0.05 mile north, and 0.05 south of the project. The additional right-of-way acquisition for the existing CMP as mentioned above does not warrant an expansion of the above-ground APE because the original APE is large enough to encompass its physical and visual impacts. See Appendix A for a map of the APE.

2. EFFORTS TO IDENTIFY HISTORIC PROPERTIES

The National Register of Historic Places (NRHP), Indiana Register of Historic Sites and Structures (State Register), the State Historic Architectural and Archaeological Research Database (SHAARD and the Indiana Historic Building, Bridges and Cemeteries map), and the *Jay County Interim Report* (1985) were consulted. Approximately 80 above-ground resources were identified within the APE. There are two previously surveyed above-ground resources within the project APE: the Green Park Cemetery, IHSSI No. 075-521-33093; CR-38-64, and the Roscoe Wheat House, IHSSI No. 075-521-33092, rated Outstanding.

A Historic Properties Report (HPR) was prepared by Green 3, LLC, and contained the results of this survey and literature review. The APE contains no properties listed in the National Register and, as a result of identification and evaluation efforts for this HPR, Green 3's qualified historian identified eight newly inventoried properties that retained sufficient integrity to have been given a rating of Contributing. The only property evaluated for listing in the National Register was the Roscoe Wheat House, IHSSI No. 075-521-33092, rated Outstanding, in which the HPR concluded that it was not recommended eligible for listing in the National Register.

Karen Wood conducted a site visit of the project area on March 13, 2019. The historian drove the entire project alignment and the APE. She took general photographs of the project area. All resources that will be 50 years of age by the time of project letting (expected 2022) were surveyed, and photographic documentation of "contributing" resources and representative "non-contributing" resources was prepared. See Appendix B for Photographs.

Cultural Resource Analysts, Inc.'s qualified professional archaeologist who meets the Secretary of the Interior's Professional Qualification Standards identified no sites within the project area (Curran, 1/2/2019). No further work was recommended.

The following parties/agencies were invited to become consulting parties (CP) to this project and were sent an early coordination letter and archaeology short report (Tribes only) on January 23, 2019. The State Historic Preservation Officer (SHPO) received a paper copy of the early coordination letter and archaeology short report. All potential CPs were invited to view the information on IN SCOPE <http://erms.indot.in.gov/Section106Documents/>. The SHPO is an automatic consulting party; that office and others accepting consulting party status are shown in boldface type.

- **Indiana State Historic Preservation Officer**
- Indiana Landmarks, Eastern Regional Field Office
- Jay County Commissioners
- Mayor of Portland
- Jay County Commissioners
- Jay County Highway Department

- Jay County Historian
- Jay County Historical Society
- Portland Historic Preservation Commission
- **Eastern Shawnee Tribe of Oklahoma**
- **Miami Tribe of Oklahoma**
- Peoria Tribe of Indians of Oklahoma
- Pokagon Band of Potawatomi Indians
- Delaware Tribe of Indians, Oklahoma
- Forest County Potawatomi Community

In a letter dated February 8, 2019, the Miami Tribe of Oklahoma agreed to serve as a consulting party for the undertaking.

In a letter dated March 6, 2019, the SHPO staff concurred with the list of consulting parties whom INDOT invited. In regards to archaeological resources, the SHPO staff concurred with the conclusions of the archaeology report (Curran, 1/2/2019). Additionally, the SHPO staff noted “that portions of the proposed project area lie immediately adjacent to Green Park Cemetery/Stephenson [*sic*] Cemetery (CR-38-64 in the Indiana DHPA SHAARD database system). Please note that, if the proposed project area includes any areas within 100 feet of a cemetery, then a cemetery development plan will be necessary under IC 14-21-1-26.5.”

In a letter dated June 20, 2019, the Eastern Shawnee Tribe agreed to serve as a consulting party for the undertaking. No other consulting party comments were received.

The HPR (Wood, 10/7/2019) and report distribution letter were sent to the participating consulting parties on October 7, 2019. No above-ground resources were recommended as eligible for inclusion in the NRHP. The SHPO received a paper copy of the HPR and letter. All participating CPs were invited to view the information on IN SCOPE <http://erms.indot.in.gov/Section106Documents/>.

The report distribution letter that accompanied the HPR dated October 7, 2019, noted that the Green Park Cemetery is located within 100 feet of the project area. The letter also stated “since no temporary or permanent right-of-way is being acquired from the cemetery and the boundaries of the cemetery are well-defined, the proposed project will not have an impact upon the cemetery. Therefore, the preparation of a cemetery development plan is not warranted.”

In a letter dated November 6, 2019, the SHPO staff concurred with the size of the APE and the conclusion of the HPR “that none of the above-ground properties identified within the APE are eligible for inclusion in the National Register of Historic Places.” No other consulting party comments were received.

The archaeological report (Curran, 1/2/2019) did not include the additional right-of-way acquisition for the culvert extension as well as the north corner of Blaine Pike at Western Ave in its survey area. An addendum to the archaeology report (Kelley and Szmuto, 12/30/2019) was prepared to include these additional areas. The report identified no sites within the additional survey project area and no further work is recommended.

Please see Appendix C for consulting party correspondence and Appendix D for summaries of the HPR and the archaeology short report.

A public notice of the “No Historic Properties Affected” finding will be published in *the Commercial Review* (Portland, Jay Co.) and the public will be afforded thirty (30) days to respond. This document will be revised, if necessary, after the expiration of the public comment period.

3. BASIS FOR FINDING

Based on identification efforts, a finding of “No Historic Properties Affected” is appropriate because no historic properties are present within the APE.

APPENDICES

A—Maps

B—Photographs

C—Consulting Party Correspondence

D—Report Summaries

E—Most Current Plans

Blaine Pike Pedestrian Improvements Project

HISTORIC PROPERTY REPORT

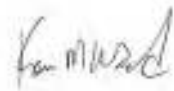
Portland, Wayne Township, Jay County, Indiana
Des. No. 1600965

October 7, 2019



Prepared for:
Beam, Longest, and Neff
8320 Craig Rd.
Indianapolis, IN 46260

By:



Karen Wood
Environmental and Cultural Resource Manager
Green 3, LLC
Historic Fountain Square
1104 Prospect Street
Indianapolis, IN 46203

p. 317.634.4110 f. 866.422.2046 karen@green3studio.com



Management Summary

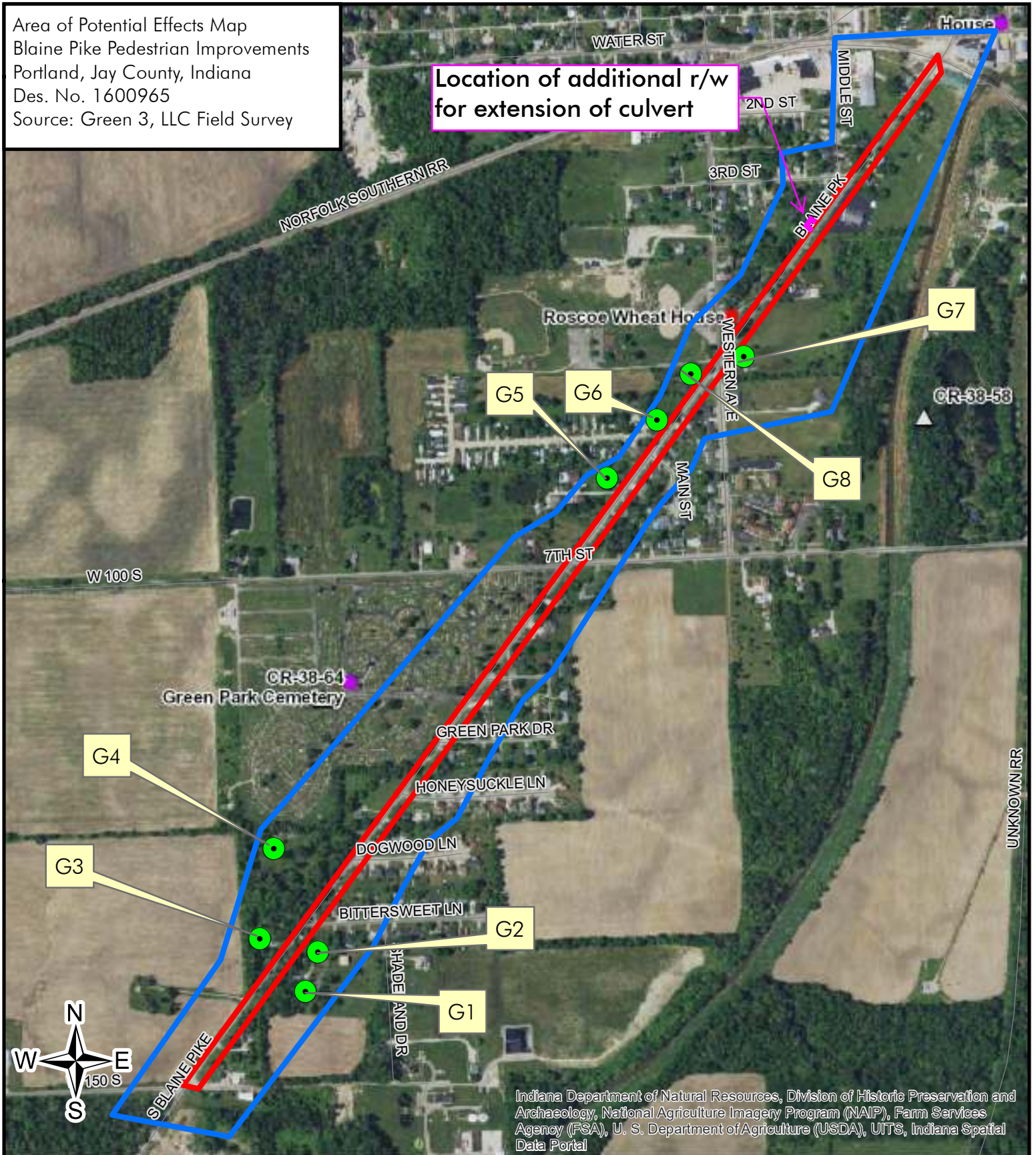
This report documents the identification and evaluation efforts for properties included in the Area of Potential Effects (APE) for the Blaine Pike Pedestrian Improvements Project, in Portland, Wayne Township, Jay County, Indiana (Des. No. 1600695). Above-ground resources located within the project APE were identified and evaluated in accordance with Section 106, National Historic Preservation Act (NHPA) of 1966, as amended, and the regulations implementing Section 106 (36 CFR Part 800).

As a result of the NHPA, as amended, and CFR Part 800, federal agencies are required to take into account the impact of federal undertakings upon historic properties in the area of the undertaking. Historic properties include buildings, structures, sites, objects, and/or districts that are eligible for or listed in the National Register of Historic Places (National Register). As this project is receiving funding from the Federal Highway Administration (FHWA), it is subject to a Section 106 review.

The APE contains no properties listed in the National Register.

The APE contains no properties that are recommended eligible for listing in the National Register.

Area of Potential Effects Map
 Blaine Pike Pedestrian Improvements
 Portland, Jay County, Indiana
 Des. No. 1600965
 Source: Green 3, LLC Field Survey



- | | | | | | |
|--|----------------|--|------------|--|----------------------|
| | Green 3 Survey | | Cemeteries | | Outstanding |
| | Project Area | | | | Notable |
| | Project APE | | | | Contributing |
| | | | | | Non-Contributing |
| | | | | | Demolished |
| | | | | | Unknown or Not Rated |





INDIANA ARCHAEOLOGICAL SHORT REPORT

State Form 54566 (1-11)

INDIANA DEPARTMENT OF NATURAL RESOURCES DIVISION OF HISTORIC PRESERVATION AND ARCHAEOLOGY

402 West Washington Street, Room W274
Indianapolis, Indiana 46204-2739
Telephone Number: (317) 232-1646
Fax Number: (317) 232-0693
E-mail: dhp@dnr.IN.gov

Where applicable, the use of this form is recommended but not required by the Division of Historic Preservation and Archaeology.

Author: Michael J. Curran

Date (month, day, year): January 2, 2019

Project Title: A Phase Ia Archaeological Survey for a Proposed Road Improvement Project along Blaine Pike in the City of Portland, Jay County, Indiana (CRA Publication Series 18-626)

PROJECT OVERVIEW

Project Description:

The survey was conducted for a proposed road improvement project along Blaine Pike in the City of Portland, Jay County, Indiana (Figure 1). The City of Portland plans to resurface Blaine Pike and construct a new shared use path. The survey area is located on the east side of Blaine Pike, south of its intersection with the Norfolk Southern Railroad and north of the intersection with Bittersweet Lane. Between Bittersweet Lane and W County Road S, the survey area and construction limits are confined to the existing right-of-way. The survey area measures approximately 1,930 m (6,330 ft) in length, with a width between 6.1 and 10.5 m (20.0 and 34.4 ft), covering 1.5 ha (3.6 acres) (Figures 2 and 3). The survey area was determined by the construction limits.

INDOT Designation Number/ Contract Number: Des. No. 1600965

Project Number: CRA No. I18B013

DHPA Number: N/A

Approved DHPA Plan Number: N/A

Prepared For: Beam, Longest, & Neff, LLC

Contact Person: Brian Shaw

Address: 8320 Craig Street

City: Indianapolis

State: IN

ZIP Code: 46250

Telephone Number: (317) 849-5832

Email Address: bshaw@b-l-n.com

Principal Investigator: Andrew V. Martin, RPA

Signature:

Company/Institution: Cultural Resource Analysts, Inc. (CRA)

Address: 201 Northwest Fourth Street, Suite 204

City: Evansville

State: IN

ZIP Code: 47708

Telephone Number: (812) 253-3009

Email Address: amartin@crai-ky.com

☒ The Phase Ia archaeological reconnaissance has located no archaeological sites within the project area and it is recommended that the project be allowed to proceed as planned.

☐ The Phase Ia archaeological reconnaissance has determined that the project area includes landforms which have the potential to contain buried archaeological deposits. It is recommended that Phase Ic archaeological subsurface reconnaissance be conducted before the project is allowed to proceed.

☐ The Phase Ia archaeological reconnaissance has determined that the project area is within 100 feet of a cemetery and a Cemetery Development Plan is required per IC-14-21-1-26.5.

Cemetery Name:

The Green Park Cemetery (CR 38-64) is located within 100 ft of the survey area, on the west side of Blaine Pike.

Other Recommendations/Commitments:

The Green Park Cemetery is well marked and maintained. Due to the cemetery's location on the opposite side of Blaine Pike and its defined boundary, the preparation of a cemetery development plan is considered not warranted.

Pursuant to IC-14-21-1, if any archaeological artifacts or human remains are uncovered during construction, demolition, or earthmoving activities, state law (Indiana Code 14-21-1-27 and 29) requires that the discovery must be reported to the Department of Natural Resources within two (2) business days. In that event, please call (317) 232-1646.

Attachments

☒ Figure showing project location within Indiana.

☒ USGS topographic map showing the project area (*1:24,000 scale*).

☒ Aerial photograph showing the project area, land use and survey methods.

☒ Photographs of the project area.

☐ Project plans (*if available*)

Other Attachments:

Legal Locations; Owner Names; Previously Recorded Archaeological Sites within 1.0 mi; Previously Recorded Archaeological Investigations within 1.0 mi; References Cited

References Cited:

See attachment

Comments:

None

Curation

Curation Facility for Project Documentation:

Cultural Resource Analysts, Inc., Evansville, Indiana



INDIANA ARCHAEOLOGICAL SHORT REPORT

State Form 54566 (1-11)

INDIANA DEPARTMENT OF NATURAL RESOURCES DIVISION OF HISTORIC PRESERVATION AND ARCHAEOLOGY

402 West Washington Street, Room W274
Indianapolis, Indiana 46204-2739
Telephone Number: (317) 232-1646
Fax Number: (317) 232-0693
E-mail: dhp@dnr.IN.gov

Where applicable, the use of this form is recommended but not required by the Division of Historic Preservation and Archaeology.

Author: Lisa Kelley and Cecilia Szmuto

Date (month, day, year): December 30, 2019

Project Title: An Addendum Report for the Proposed Road Improvement Project along Blaine Pike in the City of Portland, Jay County, Indiana (INDOT Des. No. 1600965)

PROJECT OVERVIEW

Project Description:

The City of Portland plans to construct a new shared use path and resurface portions of Blaine Pike in Portland, Jay County, Indiana (Figure 1). The proposed project will primarily be on the east side of Blaine Pike and extend south of the Norfolk Southern Railroad crossing and north of Bittersweet Lane (Figure 2). A survey covering 1.5 ha (3.6 acres) on the east side of Blaine Pike was conducted in January 2019 for the proposed project (Curran 2019). Two additional areas that are the focus of this addendum report have since been identified as extending outside of the existing right-of-way. The two areas are located on the west side of Blaine Pike between South Western Avenue and West Third Street, and cover a total of 372 sq m (4,004 sq ft) (Figure 3). Proposed construction associated with these two areas include a pedestrian crossing with a sidewalk, ramp, and beacon, as well as a culvert extension.

INDOT Designation Number/ Contract Number: 1600965

Project Number: CRA No. I18B013

DHPA Number: N/A

Approved DHPA Plan Number: N/A

Prepared For: Beam, Longest, & Neff, LLC

Contact Person: Brian Shaw

Address: 8320 Craig Street

City: Indianapolis

State: IN

ZIP Code: 46250

Telephone Number: (317) 849-5832

Email Address: bshaw@b-l-n.com

Principal Investigator: Andrew V. Martin, RPA

Signature:

Company/Institution: Cultural Resource Analysts, Inc.

Address: 201 Northwest Fourth Street, Suite 204

City: Evansville

State: IN

ZIP Code: 47708

Telephone Number: (812) 253-3009

Email Address: amartin@crai-ky.com

- ☐ Archaeological records check has determined that the project area does not have the potential to contain archaeological resources.
- ☐ Archaeological records check has determined that the project area has the potential to contain archaeological resources.
- ☒ Phase Ia reconnaissance has located no archaeological resources in the project area.
- ☐ Phase Ia reconnaissance has identified landforms conducive to buried archaeological deposits.

Actual Area Surveyed hectares: acres:

The shovel tests conducted in the survey areas did not identify any archaeological material from a intact depositional context. Some pieces of brick were identified in the northern survey area in all three of the shovel tests. However, the soil profiles suggest that this area has been previously disturbed. The profiles in the two shovel tests furthest from the road consisted of a very dark brown (10YR 2/2) silt loam with brick pieces to approximately 30 cm bgs. From 30 to 50 cm bgs was a mottled very dark brown (10YR 2/2) and yellowish brown (10YR 5/6) silty clay over a dark grayish brown (10YR 4/2) clay. The top two soil zones described above are inconsistent with the soil map units (Blount and Glynwood) described for this landform. The last soil zone is consistent with the poorly drained Bt-horizon of the Blount series.

Comments: As mentioned above, there is a historic structure just north of the northern survey area that may have been built between 1910 and 1936. However, the structure is extant in it's original location, and the map data does not suggest that there was an earlier building near this locale. Therefore, the brick seems to be secondarily deposited without a primary physical context or associated location that would possess research value. Rather, the bricks are likely associated with the culvert/road construction. Therefore, they were not collected or recorded as an archaeological site.

The shovel test profiles in the southern survey area consisted of a 27 cm dark grayish brown (10YR 4/2) silt loam over a pale brown (10YR 6/3) silty clay loam to 36 cm bgs that was underlain by a brownish yellow (10YR 6/8) silty clay. This profile is consistent with both the Glynwood and Blount soil series, which would have archaeological deposits only at or near the surface in this environment.

Recommendation

- ☐ The archaeological records check has determined that the project area has the potential to contain archaeological resources and a Phase Ia archaeological reconnaissance is recommended.
- ☐ The archaeological records check has determined that the project area does not have the potential to contain archaeological resources and no further work is recommended before the project is allowed to proceed.
- ☒ The Phase Ia archaeological reconnaissance has located no archaeological sites within the project area and it is recommended that the project be allowed to proceed as planned.

☐ The Phase Ia archaeological reconnaissance has determined that the project area includes landforms which have the potential to contain buried archaeological deposits. It is recommended that Phase Ic archaeological subsurface reconnaissance be conducted before the project is allowed to proceed.

☐ The Phase Ia archaeological reconnaissance has determined that the project area is within 100 feet of a cemetery and a Cemetery Development Plan is required per IC-14-21-1-26.5.

Cemetery Name:

Other Recommendations/Commitments:

Pursuant to IC-14-21-1, if any archaeological artifacts or human remains are uncovered during construction, demolition, or earthmoving activities, state law (Indiana Code 14-21-1-27 and 29) requires that the discovery must be reported to the Department of Natural Resources within two (2) business days. In that event, please call (317) 232-1646.

Attachments

- ☒ Figure showing project location within Indiana.
- ☒ USGS topographic map showing the project area (*1:24,000 scale*).
- ☒ Aerial photograph showing the project area, land use and survey methods.
- ☒ Photographs of the project area.
- ☐ Project plans (*if available*)

Other Attachments: References Cited; Table 1; Table 2.

References Cited: See attachment.

Comments: None.

Curation

Curation Facility for Project Documentation: All project documentation will be retained at Cultural Resource Analysts, Inc., Evansville, Indiana.



INDIANA DEPARTMENT OF TRANSPORTATION

100 North Senate Avenue
Room N642
Indianapolis, Indiana 46204

PHONE: (317) 233-6795

Eric J. Holcomb, Governor
Joe McGuinness, Commissioner

January 23, 2019

This letter was sent to the listed parties

Re: Blaine Pike Pedestrian Improvements Project, Des. No. 1600965

Dear Consulting Party,

The City of Portland, with funding from the Federal Highway Administration (FHWA) and administrative oversight from the Indiana Department of Transportation (INDOT), propose to proceed with the Blaine Pike Pedestrian Improvements Project, Des. No. 1600965. Beam, Longest, and Neff is under contract with the City of Portland to advance the environmental documentation for the referenced project.

This letter is part of the early coordination phase of the environmental review process requesting comments associated with this project. We are requesting comments from your area of expertise regarding any possible environmental effects associated with this project. Please use the above Des. Number and project description in your reply and your comments will be incorporated into the formal environmental study.

The proposed undertaking is located on Blaine Pike from CR 150 S to approximately 130 feet south of the Norfolk Southern Railroad crossing in Jay County, Indiana. It is within Wayne Township, Indiana 7.5-minute USGS Topographic Quadrangle in Portland, in Sections 20, 29, and 30, Township 23 North, Range 14 East.

The purpose of this project is to improve safety of the Blaine Pike corridor for both the increasing vehicle traffic and pedestrians and non-motorized traffic. The need for this project is to address the deficiencies of the roadway and lack of connectivity in pedestrian facilities along Blaine Pike.

Blaine Pike is classified as a minor arterial facility consisting of a 10-10.5-foot-wide, two-lane asphalt roadway with little to no shoulders. Blaine Pike serves as one of the primary routes for access into the City of Portland from rural Jay County. A curbed parking lane affords on-street parking from 7th St. to Western Avenue on the east side of Blaine Pike; a second curbed parking lane exists on the west side of the Blaine Pike at Sesame St.

The project proposes the milling and resurfacing of Blaine Pike along the entire project corridor and construction of a 10-foot-wide multi-use trail on the east side of the roadway from Bittersweet Lane to approximately 130 feet south of the Norfolk Southern Railroad crossing. The trail will cross side roads and driveways in which the project proposes to construct crosswalks and curb ramps that meet Americans with Disabilities Act (ADA) compliance. A mid-block crossing with solar-powered flashing beacons will be constructed just north of the intersection of Blaine Pike and Western Avenue adjacent to Memorial Park. Ditches on the east side of Blaine Pike will be relocated due to the multi-use trail. The existing corrugated metal pipe (CMP) north of 3rd St. will be extended to accommodate the inclusion of the new multi-use trail.

The project is approximately 1.2 miles long. Land use surrounding the project area is primarily residential with some agricultural land use at the southern end of the project and some commercial land use at the northern end of the project. The Green Park Cemetery is located adjacent to Blaine Pike on the west side. The project lies entirely within the corporation limits of the City of Portland with the downtown area located to the north. It is anticipated that approximately 3.1 acres of permanent right-of-way and 0.1 acre of temporary right-of-way will be acquired for this project. Construction is anticipated to begin in 2022.

Section 106 of the National Historic Preservation Act requires that Federal agencies take into account the effects of their undertakings on historic properties. In accordance with 36 CFR 800.2(c), you are hereby requested to be a consulting party to participate in the Section 106 process. Entities that have been invited to participate in the Section 106 consultation process for this project are identified in the attached list. Per 36 CFR 800.3(f), we hereby request that the Indiana State Historic Preservation Officer (SHPO) notify this office if the SHPO staff is aware of any other parties that may be entitled to be consulting parties or should be contacted as potential consulting parties for the project.

The Section 106 process involves efforts to identify historic properties potentially affected by the undertaking, assess the undertaking's effects, and seek ways to avoid, minimize or mitigate any adverse effects on historic properties. For more information regarding the protection of historic resources, please see the Advisory Council on Historic Preservation's Guide *Protecting Historic Properties: A Citizen's Guide to Section 106 Review* available online at <http://www.achp.gov/citizensguide.pdf>.

The Area of Potential Effects (APE) is the area in which the proposed project may cause alterations in the character or use of historic resources. At this time, no above-ground cultural resource investigation has occurred; however, the results of the above-ground cultural resource identification and evaluation efforts will be forthcoming. Consulting parties will receive notification when this report is completed.

With regards to archaeological resources, an archaeologist who meets the Secretary of the Interior's Professional Qualification Standards identified no sites within the project area (Curran, 1/2/2019). No further work is recommended.

The archaeology report (tribes only), and this letter are available for review in IN SCOPE at <http://erms.indot.in.gov/Section106Documents/> (the Des. No. is the most efficient search term, once in IN SCOPE). You are invited to review these documents and respond with comments on any historic resource impacts incurred as a result of this project so that an environmental report can be completed. We also welcome your related opinions and other input to be considered in the preparation of the environmental document. If you prefer a hard copy of this material, please respond to this email with your request within seven (7) days.

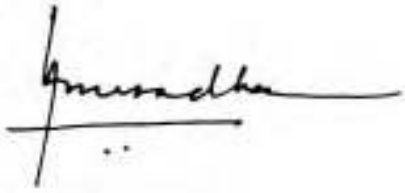
Please review the information and comment within thirty (30) calendar days of receipt. If you indicate that you do not desire to be a consulting party, or if you do not respond, you will not be included on the list of consulting parties for this project. If we do not receive your response in the time allotted, the project will proceed consistent with the proposed design and you will not receive further information about the project unless the design changes.

For questions concerning specific project details, you may contact Karen Wood at (317) 634 – 4110 or e-mail to karen@green3studio.com. All future responses regarding the proposed project should be forwarded to Green 3, LLC at the following address:

Karen Wood
Environmental and Cultural Resources Manager
Green 3, LLC
1104 Prospect St.
Indianapolis, IN 46203

Tribal contacts may contact Shaun Miller at smiller@indot.in.gov or 317-233-6795 or Michelle Allen at FHWA at michelle.allen@dot.gov or 317-226-7492. Thank you for your time, consideration and feedback.

Sincerely,



Anuradha V. Kumar, Manager
Cultural Resources Office, INDOT Environmental Services

Enclosures:
Topographic Map

Distribution List:
Indiana State Historic Preservation Officer
INDOT-CRO
Federal Highway Administration
Indiana Landmarks, Eastern Regional Office
Mayor of Portland
Jay County Commissioners
Jay County Highway Department
Jay County Historian
Jay County Historical Society
Portland Historic Preservation Commission
Eastern Shawnee Tribe
Miami Tribe of Oklahoma
Peoria Tribe of Indians of Oklahoma
Pokagon Band of Potawatomi Indians
Delaware Tribe of Indians, Oklahoma
Forest County Potawatomi Community



INDIANA DEPARTMENT OF TRANSPORTATION

100 North Senate Avenue
Room N642
Indianapolis, Indiana 46204

PHONE: (317) 233-6795

Eric J. Holcomb, Governor
Joe McGuinness, Commissioner

October 7, 2019

This letter was sent to the listed parties

Re: Blaine Pike Pedestrian Improvements Project, Des. No. 1600965 and DHPA No. 23454

Dear Consulting Party,

The City of Portland, with funding from the Federal Highway Administration (FHWA) and administrative oversight from the Indiana Department of Transportation (INDOT), proposes to proceed with the Blaine Pike Pedestrian Improvements Project, Des. No. 1600965. Green 3, LLC is a subconsultant to Beam, Longest, and Neff, who is under contract with the City of Portland to advance the environmental documentation for the referenced project.

This letter is part of the Section 106 review process for this project. A Section 106 early coordination letter and the archaeology report were distributed on January 23, 2019. The State Historic Preservation Officer (SHPO) staff in their letter dated March 6, 2019, requested to be informed on which consulting parties have accepted the invitation to become a consulting party. The consulting parties who accepted the invitation are the Miami Tribe of Oklahoma and Eastern Shawnee Tribe.

The proposed undertaking is located on Blaine Pike from CR 150 S to approximately 175 feet (ft.) south of the Norfolk Southern Railroad crossing in Jay County, Indiana. It is within Wayne Township, Portland USGS Topographic Quadrangle, in Sections 20, 29, and 30, Township 23 North, Range 14 East.

The purpose of this project is to improve safety of the Blaine Pike corridor for both the increasing vehicle traffic and pedestrians and non-motorized traffic. The need for this project is to address the deficiencies of the roadway and lack of connectivity in pedestrian facilities along Blaine Pike.

Blaine Pike is classified as a minor arterial facility consisting of a 10-10.5-ft.-wide, two-lane asphalt roadway with little to no shoulders. Blaine Pike serves as one of the primary routes for access into the City of Portland from rural Jay County. A curbed parking lane affords on-street parking from 7th St. to Western Avenue on the east side of Blaine Pike; a second curbed parking lane exists on the west side of the Blaine Pike at Sesame St.

The project proposes the milling and resurfacing of Blaine Pike along the entire project corridor and construction of a 10-foot-wide multi-use trail on the east side of the roadway from Bittersweet Lane to approximately 235 ft. south of the Norfolk Southern Railroad crossing. The early coordination letter dated January 23, 2019 indicated that both the project corridor and the trail ended at the northeastern terminus approximately 130 ft. from the railroad; that length was inaccurate. The mill and overlay of Blaine Pike ends approximately 175 ft. south of the railroad; the trail ends approximately 235 ft. south of the railroad. The trail will cross side roads and driveways in which the project proposes to construct crosswalks and curb ramps that meet Americans with Disabilities Act (ADA) compliance. A mid-block crossing with solar-powered flashing beacons will be constructed just north of the intersection of Blaine Pike and Western Avenue adjacent to Memorial Park. Ditches on the east side of Blaine Pike will be relocated due to the multi-use trail. The existing corrugated metal pipe (CMP) north of 3rd St. will be extended to accommodate the inclusion of the new multi-use trail.

The project is approximately 1.2 miles long. Land use surrounding the project area is primarily residential with some agricultural land use at the southern end of the project and some commercial land use at the northern end of the project. The project lies entirely within the corporation limits of the City of Portland with the downtown area located to the north.

The Green Park Cemetery is located approximately 14 ft. west from the edge of pavement of Blaine Pike, running adjacent to the roadway for approximately 0.22 mile within the proposed project area. The cemetery's boundaries are well-defined. Since no temporary or permanent right-of-way is being acquired from the cemetery and the boundaries of the cemetery are well-defined, the proposed project will not have an impact upon the cemetery. Therefore, the preparation of a cemetery development plan is not warranted.

Originally, it was anticipated that approximately 3.1 acres of permanent and 0.1 acre of temporary right-of-way will be acquired for this project. Currently, it is anticipated that approximately 2.79 acres of permanent and 0.003 acre of temporary right-of-way will be acquired. Construction is anticipated to begin in 2022.

Section 106 of the National Historic Preservation Act requires federal agencies to take into account the effects of their undertakings on historic and archaeological properties. In accordance with 36 CFR 800.2 (c), you were invited to become a consulting party as part of the Section 106 process. Entities that have accepted consulting party status are identified in the attached list.

The Section 106 process involves efforts to identify historic properties potentially affected by the undertaking, assess the undertaking's effects, and seek ways to avoid, minimize or mitigate any adverse effects on historic properties. For more information regarding the protection of historic resources, please see the Advisory Council on Historic Preservation's Guide *Protecting Historic Properties: A Citizen's Guide to Section 106 Review* available online at <https://www.achp.gov/sites/default/files/documents/2017-01/CitizenGuide.pdf>.

The Area of Potential Effects (APE) is the area in which the proposed project may cause alterations in the character or use of historic resources. The APE contains no resources listed in the National Register of Historic Places (NRHP).

A historian who meets the Secretary of the Interior's Professional Qualification Standards identified and evaluated above-ground resources within the APE for potential eligibility for the NRHP. As a result of the historic property identification and evaluation efforts, no above-ground resources are recommended as eligible for listing in the NRHP.

With regards to archaeological resources, an archaeologist who meets the Secretary of the Interior's Professional Qualification Standards identified no sites within the project area (Curran, 1/2/2019). No further work is recommended.

The Historic Property Report and this letter are available for review in IN SCOPE at <http://erms.indot.in.gov/Section106Documents/> (the Des. No. is the most efficient search term, once in IN SCOPE). You are invited to review these documents and respond with comments on any historic resource impacts incurred as a result of this project so that an environmental report can be completed. We also welcome your related opinions and other input to be considered in the preparation of the environmental document. If you prefer a hard copy of this material, please respond to this email with your request within seven (7) days.

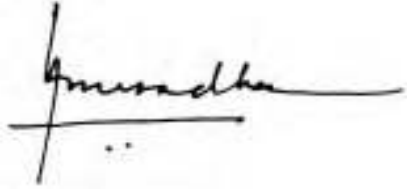
Please review the information and comment within thirty (30) calendar days of receipt.

For questions concerning specific project details, you may contact Karen Wood at (317) 634 – 4110 or e-mail to karen@green3studio.com. All future responses regarding the proposed project should be forwarded to Green 3, LLC at the following address:

Karen Wood
Environmental and Cultural Resources Manager
Green 3, LLC
1104 Prospect St.
Indianapolis, IN 46203

Tribal contacts may contact Shaun Miller at smiller@indot.in.gov or 317-233-6795 or Michelle Allen at FHWA at michelle.allen@dot.gov or 317-226-7492. Thank you for your time, consideration and feedback.

Sincerely,



Anuradha V. Kumar, Manager
Cultural Resources Office,
INDOT Environmental Services

Enclosures:

Topographic Map

Historic Properties Report (this document may be found on the IN SCOPE site if you are receiving this letter by email)

Distribution List:

Indiana State Historic Preservation Officer
Miami Tribe of Oklahoma
Eastern Shawnee Tribe



INDIANA DEPARTMENT OF TRANSPORTATION

100 North Senate Avenue
Room N642
Indianapolis, Indiana 46204

PHONE: (317) 234-5168

Eric Holcomb, Governor
Joe McGuinness, Commissioner

January 31, 2020

This letter was sent to the listed parties.

RE: Blaine Pike Pedestrian Improvements Project, Des. No. 1600965 and DHPA No. 23454

Dear Consulting Party,

The City of Portland, with funding from the Federal Highway Administration (FHWA) and administrative oversight from the Indiana Department of Transportation (INDOT), proposes to proceed with the Blaine Pike Pedestrian Improvements Project, Des. No. 1600965. Green 3, LLC is a subconsultant to Beam, Longest, and Neff, who is under contract with the City of Portland to advance the environmental documentation for the referenced project.

This letter is part of the Section 106 review process for this project. A Section 106 early coordination letter and the archaeology report were distributed on January 23, 2019. The State Historic Preservation Officer (SHPO) staff in their letter dated March 6, 2019, requested to be informed on which consulting parties have accepted the invitation to become a consulting party. The consulting parties who accepted the invitation are the Miami Tribe of Oklahoma and Eastern Shawnee Tribe.

The proposed undertaking is located on Blaine Pike from CR 150 S to approximately 175 feet (ft.) south of the Norfolk Southern Railroad crossing in Jay County, Indiana. It is within Wayne Township, Portland USGS Topographic Quadrangle, in Sections 20, 29, and 30, Township 23 North, Range 14 East.

The purpose of this project is to improve safety of the Blaine Pike corridor for both the increasing vehicle traffic and pedestrians and non-motorized traffic. The need for this project is to address the deficiencies of the roadway and lack of connectivity in pedestrian facilities along Blaine Pike.

Blaine Pike is classified as a minor arterial facility consisting of a 10-10.5-ft.-wide, two-lane asphalt roadway with little to no shoulders. Blaine Pike serves as one of the primary routes for access into the City of Portland from rural Jay County. A curbed parking lane affords on-street parking from 7th St. to Western Avenue on the east side of Blaine Pike; a second curbed parking lane exists on the west side of the Blaine Pike at Sesame St.

The project proposes the milling and resurfacing of Blaine Pike along the entire project corridor and construction of a 10-foot-wide multi-use trail on the east side of the roadway from Bittersweet Lane to approximately 235 ft. south of the Norfolk Southern Railroad crossing. The trail will cross side roads and driveways in which the project proposes to construct crosswalks and curb ramps that meet Americans with Disabilities Act (ADA) compliance. A mid-block crossing with solar-powered flashing beacons will be constructed just north of the intersection of Blaine Pike and Western Avenue adjacent to Memorial Park. As part of the proposed ditch relocations on the east side of Blaine Pike, several pipe catch basins and one CIP trench drain will be installed where necessary. It is anticipated that approximately 2.79 acres of permanent and 0.003 acre of temporary right-of-way will be acquired. Construction is anticipated to begin in 2022.

The previous report distribution letter dated October 7, 2019, stated that the existing corrugated metal pipe (CMP), located north of 3rd St. will be extended to accommodate the inclusion of the new multi-use trail. While the proposed work is correct, the location of the CMP was inaccurate. The existing CMP that will be extended is located approximately 450 ft. south of 3rd St. For this culvert extension, there will also be approximately 400 square feet (0.0092 acre) of permanent

right-of-way taken from the west side of Blaine Pike. See most current plan sheet in Appendix E-29 of the 800.11 documentation that shows the right-of-way acquisition.

Green 3, LLC is under contract with the City of Portland to advance the environmental documentation for the referenced project.

In accordance with 36 CFR 800.2 (c), you were invited to become a consulting party as part of the Section 106 process, or you are hereby invited to become a consulting party as part of the Section 106 process. Entities that have previously accepted consulting party status--as well as additional entities that are currently being invited to become consulting parties--are identified in the attached list.

The Section 106 process involves efforts to identify historic properties potentially affected by the undertaking, assess its effects and seek ways to avoid, minimize or mitigate any adverse effects on historic properties. For more information regarding the protection of historic resources, please see the Advisory Council on Historic Preservation's guide: *Protecting Historic Properties: A Citizen's Guide to Section 106 Review* available online at <https://www.achp.gov/sites/default/files/documents/2017-01/CitizenGuide.pdf>.

The Area of Potential Effects (APE) is the area in which the proposed project may cause alterations in the character or use of historic resources. The APE contains no resources listed in the National Register of Historic Places (NRHP).

A historian who meets the Secretary of the Interior's Professional Qualification Standards identified and evaluated above-ground resources within the APE for potential eligibility for the NRHP. As a result of the historic property identification and evaluation efforts, no above-ground resources are recommended as eligible for listing in the NRHP. Regarding the additional right-of-way acquisition for the culvert extension, the above-ground APE remains sufficient. No further work is recommended. See the APE map enclosed showing the additional right-of-way acquisition.

With regard to archaeological resources, an archaeologist who meets the Secretary of the Interior's Professional Qualification Standards identified no sites within the project area (Curran, 1/2/2019) and no further work was recommended. The archaeological report's survey area did not include the additional right-of-way acquisition for the culvert extension as well as the north corner of Blaine Pike at Western Ave. An addendum to the archaeology report (Kelley and Szmuto, 12/30/2019) was prepared to include these additional areas. The report identified no sites within the additional survey project area and no further work is recommended.

The 800.11(d) documentation and Addendum to the Archaeology Report (SHPO and tribes only) are available for review in IN SCOPE at <http://erms.indot.in.gov/Section106Documents/> (the Des. No. is the most efficient search term, once in IN SCOPE). You are invited to review these documents and respond with comments on any historic resource impacts incurred as a result of this project so that an environmental report can be completed. We also welcome your related opinions and other input to be considered in the preparation of the environmental document. If you prefer a hard copy of this material, please respond to this email with your request within seven (7) days.

Please review the information and comment within thirty (30) calendar days of receipt. If you indicate that you do not desire to be a consulting party or if you have not previously accepted consulting party status and you do not respond to this letter, you will not be included on the list of consulting parties for this project and will not receive further information about the project unless the design changes.

For questions concerning specific project details, you may contact Karen Wood of Green 3, LLC at 317.634.4110 or karen@green3studio.com. All future responses regarding the proposed project should be forwarded to Green 3, LLC at the following address:

Karen Wood
Environmental and Cultural Resources Manager
Green 3, LLC
1104 Prospect St.
Indianapolis, IN 46203

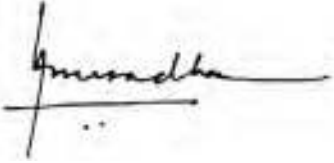
www.in.gov/dot/
An Equal Opportunity Employer



karen@green3studio.com

Tribal contacts may contact Shaun Miller at smiller@indot.in.gov or 317-233-6795 or Michelle Allen at FHWA at michelle.allen@dot.gov or 317-226-7344.

Sincerely,



Anuradha V. Kumar, Manager
Cultural Resources Office
Environmental Services

Enclosures:

APE Map showing location of additional right-of-way

800.11(d) Documentation (this document may be found on the IN SCOPE site if you are receiving this letter by email)

Addendum to Archaeology Report (this document may be found on the IN SCOPE site if you are receiving this letter by email)

Distribution List:

Indiana State Historic Preservation Officer, jcarr@dnr.in.gov, WTharp1@dnr.IN.gov

Miami Tribe of Oklahoma

Eastern Shawnee Tribe of Oklahoma



Division of Historic Preservation & Archaeology · 402 W. Washington Street, W274 · Indianapolis, IN 46204-2739
Phone 317-232-1646 · Fax 317-232-0693 · dhpa@dnr.IN.gov · www.IN.gov/dnr/historic



March 6, 2019

Karen Wood
Environmental and Cultural Resources Manager
Green 3, LLC
1104 Prospect Street
Indianapolis, Indiana 46203

Federal Agency: Indiana Department of Transportation ("INDOT"),
on behalf of Federal Highway Administration, Indiana Division ("FHWA")

Re: Early coordination letter, and Indiana archaeological short report (Curran, 01/02/2019), for the
Blaine Pike Pedestrian Improvements Project, in the City of Portland and Wayne Township, Jay
County, Indiana (Des. No. 1600965, DHPA No. 23454)

Dear Ms. Wood:

Pursuant to Section 106 of the National Historic Preservation Act of 1966, as amended (54 U.S.C. § 306108), 36 C.F.R. Part 800, and the "Programmatic Agreement (PA) Among the Federal Highway Administration, the Indiana Department of Transportation, the Advisory Council on Historic Preservation and the Indiana State Historic Preservation Officer Regarding the Implementation of the Federal Aid Highway Program In the State of Indiana," the staff of the Indiana State Historic Preservation Officer ("Indiana SHPO staff" or "INDNR-DHPA") has reviewed Green 3, LLC's review request submittal form dated January 23, 2019, which enclosed INDOT's early coordination letter of the same date and the Indiana archaeological short report (Curran, 01/02/2019), all of which we received on February 4, 2019, for the project that would occur on and along Blaine Pike from CR 150 S to approximately 130 feet south of the Norfolk Southern Railway crossing.

We are not aware of any parties who should be invited to participate in the Section 106 consultation for this federal undertaking, beyond those whom INDOT has invited. In your next correspondence on this project, please inform us as to which of the invited consulting parties have accepted the invitation and provide the e-mail or postal addresses of those that have accepted.

In regard to archaeological resources, based on the submitted information and the documentation available to the staff of the Indiana SHPO, we have not identified any currently known archaeological resources listed in or eligible for inclusion in the National Register of Historic Places ("NRHP") within the proposed project area; and we concur with the opinion of the archaeologist, as expressed in the Indiana archaeological short report (Curran, 01/02/2019), that no further archaeological investigations appear necessary at this location.

Additionally, we note that portions of the proposed project area lie immediately adjacent to Green Park Cemetery/Stephenson Cemetery (CR-38-64 in the Indiana DHPA SHAARD database system). Please note that, if the proposed project area includes any areas within 100 feet of a cemetery, then a cemetery development plan will be necessary under IC 14-21-1-26.5. The aforementioned cemetery must be avoided by all project activities, and provisions of relevant state statutes regarding cemeteries (including IC 14-21-1 and IC 23-14) must be adhered to. Please also be aware of Indiana Code 23-14-44-1 and Indiana Code 23-14-44-2, regarding restrictions on roads and utility construction in cemeteries.

If any prehistoric or historic archaeological artifacts or human remains are uncovered during construction, demolition, or earthmoving activities, state law (Indiana Code 14-21-1-27 and -29) requires that the discovery be reported to the INDNR-DHPA within two (2) business days. In that event, please call (317) 232-1646. Be advised that adherence to Indiana Code 14-21-1-27

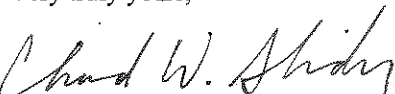
and -29 does not obviate the need to adhere to applicable federal statutes and regulations, including but not limited to 36 C.F.R. Part 800.

We look forward to receiving the historic property report for this project that INDOT's letter said will be forthcoming.

The structures reviewer for the Indiana SHPO staff is John Carr, and the archaeological reviewer is Wade T. Tharp. However, if you have questions about the status of our review of a submission, about the kind of information to submit, or about a procedural issue, please contact initially an INDOT Cultural Resources Office staff member who is assigned to the project.

In all future correspondence regarding the Blaine Pike Pedestrian Improvements Project in the City of Portland and Wayne Township, Jay County (Des. No. 1600965), please refer to DHPA No. 23454.

Very truly yours,



Beth K. McCord
Deputy State Historic Preservation Officer

BKM:JLC:WTT:wt

emc: Anuradha Kumar, INDOT
Susan Branigin, INDOT
Shaun Miller, INDOT
Shirley Clark, INDOT
Karen Wood, Green 3, LLC
Andrew Martin, RPA, Cultural Resource Analysts, Inc.
Brian Shaw, Beam, Longest and Neff, LLC
Wade T. Tharp, INDNR-DHPA
John Carr, INDNR-DHPA



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November 6, 2019

Karen Wood
Environmental and Cultural Resources Manager
Green 3, LLC
1104 Prospect Street
Indianapolis, Indiana 46203

Federal Agency: Indiana Department of Transportation ("INDOT"),
on behalf of Federal Highway Administration, Indiana Division ("FHWA")

Re: Historic structures report (Wood, 10/7/2019), for the Blaine Pike Pedestrian Improvements
Project, in the City of Portland and Wayne Township, Jay County, Indiana (Des. No. 1600965;
DHPA No. 23454)

Dear Ms. Wood:

Pursuant to Section 106 of the National Historic Preservation Act of 1966, as amended (54 U.S.C. § 306108), 36 C.F.R. Part 800, and the "Programmatic Agreement (PA) Among the Federal Highway Administration, the Indiana Department of Transportation, the Advisory Council on Historic Preservation and the Indiana State Historic Preservation Officer Regarding the Implementation of the Federal Aid Highway Program In the State of Indiana," the staff of the Indiana State Historic Preservation Officer ("Indiana SHPO staff" or "INDNR-DHPA") has reviewed INDOT's letter dated October 7, 2019, enclosing the historic property report ("HPR"; Wood, 10/7/2019), which we received on October 8 for the project that would occur on or along Blaine Pike from CR 150 S to approximately 175 feet south of the Norfolk Southern Railway crossing.

Thank you for advising us of the other parties who have accepted INDOT's consulting party status for this Section 106 consultation.

The area of potential effects ("APE") proposed in the HPR appears to be of appropriate dimensions to encompass the geographic area in which the project could have effects on historic properties.

We agree with the conclusion of the HPR that none of the above-ground properties identified within the APE are eligible for inclusion in the National Register of Historic Places ("NRHP").

As previously indicated, in regard to archaeological resources, based on the submitted information and the documentation available to the staff of the Indiana SHPO, we have not identified any currently known archaeological resources listed in or eligible for inclusion in the NRHP within the proposed project area; and we concur with the opinion of the archaeologist, as expressed in the Indiana archaeological short report (Curran, 01/02/2019), that no further archaeological investigations appear necessary at this location.

Additionally, as previously indicated, we note that portions of the proposed project area lie immediately adjacent to Green Park Cemetery/Stephenson Cemetery (CR-38-64 in the Indiana DHPA SHAARD database system). If ground disturbing activities will be within 100 feet of this cemetery, please be aware of the cemetery development requirements in Indiana Code 14-21-1-26.5 (<http://iga.in.gov/legislative/laws/2016/ic/titles/014/articles/021/chapters/001/#section-26.5>). The aforementioned cemetery must be avoided by all project activities, and provisions of relevant state statutes regarding cemeteries (including IC 14-21-1 and IC 23-14) must be adhered to. Please also be aware of Indiana Code 23-14-44-1 and Indiana Code 23-14-44-2, regarding restrictions on roads and utility construction in cemeteries.

If any prehistoric or historic archaeological artifacts or human remains are uncovered during construction, demolition, or earthmoving activities, state law (Indiana Code 14-21-1-27 and -29) requires that the discovery be reported to the INDNR-DHPA within two (2) business days. In that event, please call (317) 232-1646. Be advised that adherence to Indiana Code 14-21-1-27 and -29 does not obviate the need to adhere to applicable federal statutes and regulations, including but not limited to 36 C.F.R. Part 800.

We agree that no properties that are listed in or eligible for inclusion in the NRHP lie within the APE.

Unless another consulting party disagrees with the conclusion of the HPR, it might now be appropriate to ask INDOT for a finding.

The structures reviewer for the Indiana SHPO staff is John Carr, and the archaeological reviewer is Wade T. Tharp. However, if you have questions about the status of our review of a submission, about the kind of information to submit, or about a procedural issue, please contact initially an INDOT Cultural Resources Office staff member who is assigned to the project.

In all future correspondence regarding the Blaine Pike Pedestrian Improvements Project in the City of Portland and Wayne Township, Jay County (Des. No. 1600965), please refer to DHPA No. 23454.

Very truly yours,



Beth K. McCord
Deputy State Historic Preservation Officer

BKM:JLC:WTT:wt

emc: Anuradha Kumar, INDOT
Anthony Ross, Ph.D., INDOT
Susan Branigin, INDOT
Shaun Miller, INDOT
Shirley Clark, INDOT
Miami Tribe of Oklahoma
Eastern Shawnee Tribe of Oklahoma
Karen Wood, Green 3, LLC
Andrew Martin, RPA, Cultural Resource Analysts, Inc.
Brian Shaw, Beam, Longest and Neff, LLC
Wade T. Tharp, INDNR-DHPA
John Carr, INDNR-DHPA

RECORD OF TELEPHONE CONVERSATION

ENGINEER: Brian Shaw DATE: July 20, 2020
CALL: Mr. Dave Teeter, President, Green Park Cemetery TIME: 3:50 pm
☒ TO ☐ FROM
PHONE: 260-726-7171 JOB NO. / P.I.: 170084-701
SUBJECT OF CALL: Blaine Pike Improvements Project

CONTEXT OF CONVERSATION:

BLN contacted Mr. Dave Teeter, President of the Green Park Cemetery in Portland Indiana to coordinate on the Blaine Pike Improvements project. The Green Park Cemetery is within the project limits along the west side of Blaine Pike. The project has no direct impact to the Green Park Cemetery but coordination was conducted to determine if Mr. Teeter had any concerns with the project or potential indirect impacts. During the conversation Mr. Shaw discussed the project scope and indicated that the project would not impact the cemetery property and that access to the Green Park Cemetery would be maintained to during construction. Mr. Teeter indicated he lived in the area, was already familiar with of the project and was in support for the proposed work . He saw no potential impacts to the Green Park Cemetery. Mr. Shaw indicated if Mr. Teeter would like a pdf of the current plans to review, or other project information, that would be provided. Mr. Teeter indicated that was not necessary and he was in support of the project and thought the work should be done. Mr, Shaw thanked Mr. Teeter for his time and indicated he could contact him if he had any questions or concerns about the project.

ACTION REQUIRED:

No further action required. A copy of this Record of Telephone Conversation will be part of the project file and included in the environmental documentation.

From: [Miller, Shaun \(INDOT\)](#)
To: [Brian Shaw](#); [Korzeniewski, Patricia J](#)
Cc: [Dan Gibson](#)
Subject: RE: Des. No. 1600965 Blaine Pike Pedestrian system Improvements - City of Portland, Jay County
Date: Monday, June 15, 2020 7:57:41 AM

EXTERNAL EMAIL

Brian,

I agree with Patty Jo that a cemetery development plan is not needed for the reasons she provided.

Thank you,

Shaun Miller
INDOT, Cultural Resources Office
Archaeology Team Lead
(317)233-6795

From: Brian Shaw <bshaw@b-l-n.com>
Sent: Friday, June 12, 2020 9:20 AM
To: Korzeniewski, Patricia J <PKorzeniewski@indot.IN.gov>
Cc: Miller, Shaun (INDOT) <smiller@indot.IN.gov>; Dan Gibson <dgibson@b-l-n.com>
Subject: RE: Des. No. 1600965 Blaine Pike Pedestrian system Improvements - City of Portland, Jay County

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Patricia,
Thank you I appreciate the information.

BRIAN SHAW

Environmental Services Department Manager

o: 317.849.5832
c: 317.709.3440
B-L-N.com

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From: Korzeniewski, Patricia J <PKorzeniewski@indot.IN.gov>
Sent: Friday, June 12, 2020 9:17 AM
To: Brian Shaw <bshaw@b-l-n.com>
Cc: Korzeniewski, Patricia J <PKorzeniewski@indot.IN.gov>; Miller, Shaun (INDOT) <smiller@indot.IN.gov>

Subject: RE: Des. No. 1600965 Blaine Pike Pedestrian system Improvements - City of Portland, Jay County

EXTERNAL EMAIL

Brian,

Generally, CDP's are not required for projects that occur within 100' of a cemetery if there is no work or excavation on the west side of the roadway. The work is only milling and resurfacing of the roadway without impacting the cemetery. CDP's are required when there are impacts to a cemetery (usually temporary r/w for driveway reconstruction), when boundaries of a known cemetery are undefined and adjacent to a project area, or when there are unmarked cemeteries or family burial grounds within 100' of a project area. A CDP is not required for this project unless the project scope has changed, and the updated project scope will impact the cemetery. If the project scope is the same, it will not impact the cemetery and thus does not require a CDP. Shaun is out of the office today, but I am fairly certain he will agree with me on the above. However, he will be back on Monday and will be able to confirm.

Thank you for your time,

Patricia Jo Korzeniewski
Archaeologist and Environmental Manager
INDOT, Cultural Resources Office
PKorzeniewski@indot.in.gov
(317) 233-2093

From: Brian Shaw <bshaw@b-l-n.com>
Sent: Friday, June 12, 2020 8:57 AM
To: Korzeniewski, Patricia J <PKorzeniewski@indot.IN.gov>
Cc: Miller, Shaun (INDOT) <smiller@indot.IN.gov>; Dan Gibson <dgibson@b-l-n.com>
Subject: Des. No. 1600965 Blaine Pike Pedestrian system Improvements - City of Portland, Jay County

****** This is an EXTERNAL email. Exercise caution. DO NOT open attachments or click links from unknown senders or unexpected email. ******

Patty / Shaun,

We received comments back from INDOT-ES on our draft CE document of the Blaine Pike Improvements - City of Portland, Jay County Des. No/1600965. One of the comments asked if a Cemetery Development Plan would be required as the project is within 100 feet of the Green Park Cemetery. This project has been going on for a while and we had coordinated with INDOT Cultural Resources previously that a CDP was not required as there is no work or excavation on the west side of the roadway. The work on Blaine Pike at the Green Park Cemetery location is only milling and resurfacing of the roadway. The pedestrian path is along the east side of Blaine Pike, see attached plans. The archaeological report, see attached, indicated that the Green Park Cemetery is well marked and maintained. Due to the cemetery's location on the opposite side of Blaine Pike and its

defined boundary, the preparation of a cemetery development plan is considered not warranted.

I wanted to confirm that a Cemetery Development Plan is not required for this project based on the scope of work adjacent to the Green Park Cemetery location. Please call or email if you have questions or need additional information.

Thanks

BRIAN SHAW

Environmental Services Department Manager

o: 317.849.5832

c: 317.709.3440

B-L-N.com

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Phone 317-232-1646 · Fax 317-232-0693 · dhpa@dnr.IN.gov · www.IN.gov/dnr/historic



March 4, 2020

Karen Wood
Environmental and Cultural Resources Manager
Green 3, LLC
1104 Prospect Street
Indianapolis, Indiana 46203

Federal Agency: Indiana Department of Transportation ("INDOT"),
on behalf of Federal Highway Administration, Indiana Division ("FHWA")

Re: Addendum Indiana archaeological short report (Kelley and Szmuto, 12/30/2019), and
INDOT's finding of No Historic Properties Affected, with supporting documentation, for the
Blaine Pike Pedestrian Improvements, on Blaine Pike from CR 150 S to approximately 175
feet south of the Norfolk Southern Railroad crossing, in the City of Portland, Wayne
Township, Jay County, Indiana (Des. No. 1600965; DHPA No. 23454)

Dear Ms. Wood:

Pursuant to Section 106 of the National Historic Preservation Act of 1966, as amended (54 U.S.C. § 306108), 36 C.F.R. Part 800, and the "Programmatic Agreement (PA) Among the Federal Highway Administration, the Indiana Department of Transportation, the Advisory Council on Historic Preservation and the Indiana State Historic Preservation Officer Regarding the Implementation of the Federal Aid Highway Program In the State of Indiana," the staff of the Indiana State Historic Preservation Officer ("Indiana SHPO staff" or "INDNR-DHPA") has reviewed INDOT's January 31, 2020, letter, enclosing the addendum Indiana archaeological short report, finding, and documentation, which we received on February 2, 2020.

Based on the submitted information and the documentation available to the staff of the Indiana SHPO, we have not identified any currently known archaeological resources listed in or eligible for inclusion in the National Register of Historic Places ("NRHP") within the additional portions of the proposed project area; and we concur with the opinion of the archaeologist, as expressed in the addendum Indiana archaeological short report (Kelley and Szmuto, 12/30/2019), that no further archaeological investigations appear necessary at the additional portions of the proposed project area.

Additionally, as previously indicated, based on the submitted information and the documentation available to the staff of the Indiana SHPO, we have not identified any currently known archaeological resources listed in or eligible for inclusion in the NRHP within the original portions of the proposed project area; and we concur with the opinion of the archaeologist, as expressed in the Indiana archaeological short report (Curran, 01/02/2019), that no further archaeological investigations appear necessary at the original portions of the proposed project area.

Furthermore, as previously indicated, we note that portions of the proposed project area lie immediately adjacent to Green Park Cemetery/Stephenson Cemetery (CR-38-64 in the Indiana DHPA SHAARD database system). If ground disturbing activities will be within 100 feet of this cemetery, please be aware of the cemetery development requirements in Indiana Code 14-21-1-26.5 (<http://iga.in.gov/legislative/laws/2016/ic/titles/014/articles/021/chapters/001/#section-26.5>). The aforementioned cemetery must be avoided by all project activities, and provisions of relevant state statutes regarding cemeteries (including IC 14-21-1 and IC 23-14) must be adhered to. Please also be aware of Indiana Code 23-14-44-1 and Indiana Code 23-14-44-2, regarding restrictions on roads and utility construction in cemeteries.

If any prehistoric or historic archaeological artifacts or human remains are uncovered during construction, demolition, or earthmoving activities, state law (Indiana Code 14-21-1-27 and -29) requires that the discovery be reported to the INDNR-DHPA within two (2) business days. In that event, please call (317) 232-1646. Be advised that adherence to Indiana Code 14-21-1-27 and -29 does not obviate the need to adhere to applicable federal statutes and regulations, including but not limited to 36 C.F.R. Part 800.s

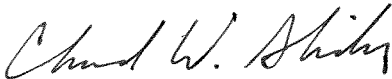
As we said in our November 6, 2019, letter, we agree with the historic property report (Wood, 10/7/2019) that none of the above-ground properties identified by that report as lying within the area of potential effects are eligible for inclusion in the NRHP.

Accordingly, we concur with INDOT's January 31, 2020, Section 106 finding, on behalf of FHWA, of No Historic Properties Affected for this federal undertaking.

The structures reviewer for the Indiana SHPO staff is John Carr, and the archaeological reviewer is Wade T. Tharp. However, if you have questions about the status of our review of a submission, about the kind of information to submit, or about a procedural issue, please contact initially an INDOT Cultural Resources Office staff member who is assigned to the project.

In all future correspondence regarding the Blaine Pike Pedestrian Improvements in the City of Portland and Wayne Township, Jay County (Des. No. 1600965), please continue to refer to DHPA No. 23454.

Very truly yours,



Beth K. McCord
Deputy State Historic Preservation Officer

BKM:JLC:WTT:wt

emc: Anuradha Kumar, INDOT
Anthony Ross, Ph.D., INDOT
Susan Branigin, INDOT
Shaun Miller, INDOT
Shirley Clark, INDOT
Miami Tribe of Oklahoma
Eastern Shawnee Tribe of Oklahoma
Karen Wood, Green 3, LLC
Andrew Martin, RPA, Cultural Resource Analysts, Inc.
Brian Shaw, Beam, Longest and Neff, LLC
Wade T. Tharp, INDNR-DHPA
John Carr, INDNR-DHPA

ATTACH COPY OF ADVERTISEMENT HERE

Form Prescribed by State Board of Accounts
City of Portland
Public Notice

(Governmental Unit)

Jay County, Indiana

General Form No. 99P (Revised 2009A)
The Commercial Review

To: The Graphic Printing Co., Inc.

P.O. Box 1049

Portland, IN 47371

PUBLISHER'S CLAIM

LINE COUNT

Display Master (Must not exceed two actual lines, neither of which shall
total more than four solid lines of type in which the body of the advertisement
is set) – number of equivalent lines 3

Head – number of lines 5

Body – number of lines 108

Tail – number of lines 0

Total number of lines in notice 116

COMPUTATION OF CHARGES

116 lines, 1 columns wide equals 116 equivalent lines 49.07
at 0.423 cents per line \$

Additional charge for notices containing rule or tabular work
(50 percent of above amount) \$

Charge for extra proofs of publication (\$1.00 for each proof in excess of two) \$

TOTAL AMOUNT OF CLAIM \$ 49.07

DATA FOR COMPUTING COST

Width of single column in picas: 8.5 Size of type 7 point

Number of insertions 1

Pursuant to the provisions and penalties of IC 5-11-10-1,

I hereby certify that the foregoing account is just and correct, that the amount claimed is legally due, after allowing all just credits, and that no part of the same has been paid.

I also certify that the printed matter attached hereto is a true copy, of the same column width and type size, which was duly published in said paper 1 times. The dates of publication being as follows:

March 20, 2020

Additionally, the statement checked below is true and correct:

..... Newspaper does not have a website.

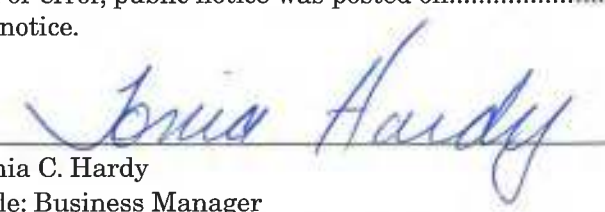
☒..... Newspaper has a website and this public notice was posted on the same day as it was published in the newspaper.

..... Newspaper has a website, but due to technical problem or error, public notice was posted on.....

..... Newspaper has a website but refuses to post the public notice.

March 23, 2020

Date:


Tonia C. Hardy

Title: Business Manager

Claim No. _____ Warrant No. _____

IN FAVOR OF

The Graphic Printing Co., Inc.

P.O. Box 1049

Portland, IN 47371

\$ 49.07

ON ACCOUNT OF APPROPRIATION FOR

Appropriation No. _____

Allowed _____

In the sum of \$ _____

I have examined the within claim and hereby certify as follows:

That it is in proper form.

That it is duly authenticated as required by law.

That it is based upon statutory authority.

That it is apparently ☐ correct.
☐ incorrect.

I certify that the within claim is true and correct; that the services therein itemized and for which charge is made were ordered by me and were necessary to the public business.

Attest

Public Notice

Public Notice
Des. No. 1600965
Blaine Pike
City of Portland
Jay County, Indiana

The City of Portland is planning to undertake the Blaine Pike Pedestrian Improvements Project, Des. No. 1600965, funded in part by the Federal Highway Administration (FHWA). The project is located on Blaine Pike from CR 150 South to approximately 175 feet south of the Norfolk Southern Railroad crossing in Jay County, Indiana. Under the preferred alternative, the proposed project would involve the milling and resurfacing of Blaine Pike along the entire project corridor and

construction of a 10-foot-wide multi-use trail on the east side of the roadway from Bittersweet Lane to south of the Norfolk Southern Railroad crossing. The trail will cross side roads and driveways in which the project proposes to construct crosswalks and curb ramps that meet Americans with Disabilities Act (ADA) compliance. A mid-block crossing with solar-powered flashing beacons will be constructed just north of the intersection of Blaine Pike and Western Avenue adjacent to Memorial Park. Ditches on the east side of Blaine Pike will be relocated due to the multi-use trail. The existing corrugated metal pipe (CMP) north of 3rd St. will be extended to accommodate the inclusion of the new multi-use trail.

The project is approximately 1.203 miles long. The Green Park Cemetery is located adjacent to Blaine Pike on the west side and will not be impacted by the project. The project lies entirely within the corporation limits of the City of Portland.

The project will require 3.79 acres of permanent right-of-way and 0.003 acre of temporary right-of-way. No relocations are required.

The proposed action does not impact properties listed in or eligible for the National Register of Historic Places. The Indiana Department of Transportation (INDOT), on behalf of the FHWA, has issued a "No Historic Properties Affected" finding for the project. In accordance with the National Historic Preservation Act, the

views of the public are being sought regarding the effect of the proposed project on the historic elements as per 36 CFR 800.2(d), 800.3(e) and 800.6(a)(4). Pursuant to 36 CFR 800.4(d)(1), the documentation specified in 36 CFR 800. 11(d) is available for inspection in the office of Cooper, Beam, Longest & Neff, LLC, 8320 Craig Street, Indianapolis, Indiana 46250. Additionally, this documentation can be viewed electronically by accessing INDOT's Section 106 document posting website IN SCOPE at <http://erms.indot.in.gov/Section106Documents>. This documentation serves as the basis for the "No Historic Properties Affected" finding. The views of the public on this effect finding are being sought. Please reply with any comments to Aimee

Cooper, Beam, Longest and Neff, LLC, 8320 Craig Street, Indianapolis, Indiana 46250. Phone: 317-806-3060, FAX: 317-841-4280, acooper@b-l-n.com no later than April 18, 2020. In accordance with the "Americans with Disabilities Act", if you have a disability for which the City of Portland, needs to provide accessibility to the document(s) such as interpreters or readers, please contact Ryan Myers 260-726-4077 streetsuperintendent@thecityofportland.us or Cooper, Beam, Longest & Neff, Aimee Cooper 317-806-3060, acooper@b-l-n.com.

CR 3-20-2020- HSPAXLP

Appendix E:

Red Flag and Hazardous Materials

Date: October 16, 2018

To: Site Assessment & Management
Environmental Services
Indiana Department of Transportation
100 N Senate Avenue, Room N642
Indianapolis, IN 46204

The length of the shared-use path
was reduced after RFI approval.
Refer to Appendix B, page B-3
for illustration of revised length.

From: Christian Radcliff
Beam, Longest and Neff, LLC
8320 Craig Street
Indianapolis, IN 46250
cradcliff@b-l-n.com

Re: RED FLAG INVESTIGATION
DES 1600965, Local Project
Road Resurfacing Project
Blaine Pike from CR W 150 S to the Norfolk Southern Railroad, City of Portland
Jay County, Indiana

PROJECT DESCRIPTION

Brief Description of Project: The proposed project involves the resurfacing of Blaine Pike between CR W 150 S and the Norfolk Southern Railroad in the City of Portland, Indiana. The project is locally and federally funded. The scope of work includes milling and resurfacing Blaine Pike and constructing a new pedestrian trail along the corridor. The pedestrian trail will be completed on either the east or west side of the roadway, depending on design factors. Existing sidewalks and curb ramps will be updated to meet ADA compliance.

Bridge and/or Culvert Project: Yes ☐ No ☒ Structure # _____

If this is a bridge project, is the bridge Historical? Yes ☐ No ☐ , Select ☐ Non-Select ☐

(Note: If the project involves a historical bridge, please include the bridge information in the Recommendations Section of the report).

Proposed right of way: Temporary ☐ # Acres N/A Permanent ☒ # Acres 3.3

Type of excavation: Excavation up to a depth of two (2) feet will be necessary for construction of the pedestrian trail on either the east or west side of the corridor. Excavation will occur within 50 feet of the centerline of the roadway.

Maintenance of traffic: Traffic will be maintained through a single lane closure and roadside flaggers to direct traffic.

Work in waterway: Yes ☐ No ☒ Above ordinary high water mark: Yes ☐ No ☐

State Project: ☐ LPA: ☒

Any other factors influencing recommendations: The right-of-way and excavation limits and locations are preliminary estimates based on a worst-case scenario. These will be refined as design progresses. Excavation for the shared use trail may occur on either side of the roadway.

INFRASTRUCTURE TABLE AND SUMMARY

Infrastructure Indicate the number of items of concern found within the 0.5 mile search radius. If there are no items, please indicate N/A:			
Religious Facilities	8*	Recreational Facilities	3
Airports ¹	1*	Pipelines	1
Cemeteries	6	Railroads	5
Hospitals	N/A	Trails	2
Schools	3*	Managed Lands	1

¹In order to complete the required airport review, a review of public airports within 3.8 miles (20,000 feet) is required.

*Items with an asterisk have some resources that are not shown on the infrastructure map.

Explanation:

Religious Facilities: Eight (8) religious facilities are in the 0.5 mile search radius. The nearest facility, River Life Church, is approximately 0.19 mile northwest of the project area. This religious facility is not shown on the attached infrastructure map but can be found on Google Maps. No impact is expected.

Airports: Although not located within the 0.5 mile search radius, One (1) public airport, Portland Municipal Airport, is in the 3.8 mile search radius. Early coordination with INDOT Aviation will occur.

Cemeteries: Six (6) cemeteries are within the 0.5 mile search radius. The nearest cemetery, Green Park Cemetery, is immediately adjacent to the project area. A Cemetery Development Plan may be required since this project is within 100 feet of the cemetery. Coordination with INDOT Cultural Resources is recommended.

Schools: Three (3) schools are within the 0.5 mile search radius. The nearest school, Jay Schools Adult Education, is approximately 0.28 mile northeast of the project area. This school is not shown on the attached infrastructure map but can be found on Google Maps. No impact is expected.

Recreational Facilities: Three (3) recreational facilities are within the 0.5 mile search radius. Sportland Park is immediately adjacent to the project area. Traffic will be maintained through lane closures. Coordination with the Portland Park Board will occur.

Pipelines: One (1) pipeline segment is within the 0.5 mile search radius. The nearest segment is associated with Ohio Valley Gas Corporation and is approximately 0.20 mile southwest of the project area. Coordination with the pipeline owner will occur.

Railroads: Five (5) railroad segments are within the 0.5 mile search radius. The nearest railroad segment is owned by Norfolk Southern and is at the northern project terminus. Coordination with Norfolk Southern will occur.

Trails: Two (2) trail segments are within the 0.5 mile search radius. The Portland River Path is approximately 0.01 mile northeast of the project area. The project involves the creation of a new trail. Coordination with the Portland Park Board will occur.

Managed Lands: One (1) managed land is within the 0.5 mile search radius. Sportland Park is adjacent to the project area. Coordination with the Portland Park Board will occur.

WATER RESOURCES TABLE AND SUMMARY

Water Resources Indicate the number of items of concern found within the 0.5 mile search radius. If there are no items, please indicate N/A:			
NWI - Points	6	Canal Routes - Historic	N/A
Karst Springs	N/A	NWI - Wetlands	16
Canal Structures – Historic	N/A	Lakes	12
NPS NRI Listed	N/A	Floodplain - DFIRM	13
NWI-Lines	5	Cave Entrance Density	N/A
IDEM 303d Listed Streams and Lakes (Impaired)	5	Sinkhole Areas	N/A
Rivers and Streams	7	Sinking-Stream Basins	N/A

Explanation:

NWI – Points: Six (6) NWI points are within the 0.5 mile search radius. The nearest point is approximately 0.18 mile southeast of the project area. No impact is expected.

NWI – Lines: Five (5) NWI line segments are within the 0.5 mile search radius. The nearest line segment is approximately 0.05 mile east of the project area. No impact is expected.

IDEM 303d Listed Streams and Lakes (Impaired): Five (5) IDEM 303d Listed Streams and Lakes are within the 0.5 mile search radius. The nearest stream segment is approximately 0.32 mile east of the project area. The segment can be found on IDEM's 303d stream map tool and is listed for Impaired Biotic Communities (IBC). Due to the scope of work, no impact is expected.

Rivers and Streams: Seven (7) river and stream segments are within the 0.5 mile search radius. The nearest stream segment is approximately 0.05 mile east of the project area. No impact is expected.

NWI – Wetlands: Sixteen (16) NWI Wetland polygons are within the 0.5 mile search radius. The nearest polygon is approximately 0.05 mile east of the project area. No impact is expected.

Lakes: Twelve (12) lakes are within the 0.5 mile search radius. The nearest lake is approximately 0.04 mile southwest of the project area. No impact is expected.

Floodplain – DFIRM: Thirteen (13) floodplain polygons are within the 0.5 mile search radius. The nearest polygon is immediately adjacent to the project area. Coordination with INDOT Ecology and Waterway Permitting will occur.

URBANIZED AREA BOUNDARY SUMMARY

Explanation:

Urbanized Area Boundary (UAB): This project lies within the Portland UAB; however, a Rule 13 Permit from IDEM has not been issued. No further coordination is necessary at this time.

MINING AND MINERAL EXPLORATION TABLE AND SUMMARY

Mining/Mineral Exploration Indicate the number of items of concern found within the 0.5 mile search radius. If there are no items, please indicate N/A:			
Petroleum Wells	3	Mineral Resources	N/A
Mines – Surface	N/A	Mines – Underground	N/A

Explanation:

Petroleum Wells: Three (3) petroleum wells are within the 0.5 mile search radius. The nearest petroleum well is approximately 0.41 mile northwest of the project area. No impact is expected.

HAZARDOUS MATERIAL CONCERNS TABLE AND SUMMARY

Hazardous Material Concerns Indicate the number of items of concern found within the 0.5 mile search radius. If there are no items, please indicate N/A:			
Superfund	N/A	Manufactured Gas Plant Sites	N/A
RCRA Generator/ TSD	3	Open Dump Waste Sites	N/A
RCRA Corrective Action Sites	N/A	Restricted Waste Sites	N/A
State Cleanup Sites	1	Waste Transfer Stations	N/A
Septage Waste Sites	N/A	Tire Waste Sites	N/A
Underground Storage Tank (UST) Sites	18	Confined Feeding Operations (CFO)	N/A
Voluntary Remediation Program	N/A	Brownfields	4
Construction Demolition Waste	N/A	Institutional Controls	1
Solid Waste Landfill	N/A	NPDES Facilities	1
Infectious/Medical Waste Sites	N/A	NPDES Pipe Locations	10
Leaking Underground Storage (LUST) Sites	4	Notice of Contamination Sites	N/A

Explanation:

RCRA Generator/TSD: Three (3) RCRA Generators/TSDs are within the 0.5 mile search radius. The nearest RCRA Generator/TSD, Patriot Paint Manufacturing (304 Blaine Pike, AI #24908), is mapped as a point 0.11 mile west of the project area; however, it appears that the property extends to immediately west of the project area. The most recent IDEM Office of Land Quality Waste Handler Identification Form shows that the site no longer produces hazardous waste. Furthermore, the site received a No Further Action (NFA) determination at site closure on December 3, 2007 and tested below Risk Integrated System of Closure (RISC) residential default closure levels (RDCLs) for soil and groundwater. If excavation occurs in this location, proper removal and disposal of contaminated soil and/or groundwater may be necessary.



State Cleanup Sites: One (1) state cleanup site is within the 0.5 mile search radius. Patriot Paint Manufacturing (304 Blaine Pike, AI #24908) is located adjacent to the project area. The site received a No Further Action determination on December 3, 2007 and tested below Risk Integrated System of Closure (RISC) residential default closure levels (RDCLs) for soil and groundwater. If excavation occurs in this location, proper removal and disposal of soil and/or groundwater may be necessary.

Underground Storage Tank (UST) Sites: Eighteen (18) UST sites are within the 0.5 mile search radius. The nearest tank is mapped approximately 0.02 mile east of the project area on the property of the Portland Wastewater Treatment Plant (FID 22985, Bittersweet Lane). The icon is not mapped correctly and the site is actually located approximately 0.16 mile east of the project area. The site contained a 4,000 gallon diesel tank that was removed on April 1, 1999. No impact is expected.

Leaking Underground Storage (LUST) Sites: Four (4) LUST sites are within the 0.5 mile search radius. The nearest LUST is approximately 0.11 mile northwest of the project area and is associated with Standard Brush and Broom (also listed as O'Cedar Plant #2, FID No. 18686, Middle and Water Street). The site received a No Further Action determination on March 3, 2017 and was designated as an unconditional closure. No impact is expected.

Brownfields: Four (4) brownfields are within the 0.5 mile search radius. The nearest brownfield is associated with Standard Brush and Broom (also listed as O'Cedar Plant #2, Agency ID No. 16982) and is located approximately 0.23 mile west of the project area. No impact is expected.

Institutional Controls: One (1) institutional control is within the 0.5 mile search radius. Former Ken Kunkle Chevrolet (414 N Meridian) is approximately 0.39 mile northeast of the project area. The site received a conditional No Further Action determination on December 3, 2013 due to the presence of toxic vapors above residential indoor air screening levels in the southeast corner of the property. No impact is expected.

NPDES Facilities: One (1) NPDES facility is within the 0.5 mile search radius. The Portland Wastewater Treatment Plant is approximately 0.15 mile southeast of the project area. No impact is expected.

NPDES Pipe Locations: Ten (10) NPDES Pipe locations are in the 0.5 mile search radius. The nearest pipe is the outlet of a Combined Sewage Overflow associated with the Portland Wastewater Treatment Plant and is approximately 0.04 mile east of the project area. No impact is expected.

ECOLOGICAL INFORMATION SUMMARY

The Jay County listing of the Indiana Natural Heritage Data Center information on endangered, threatened, or rare (ETR) species and high quality natural communities is attached with ETR species highlighted. A preliminary review of the Indiana Natural Heritage Database by INDOT Environmental Services did not indicate the presence of endangered species.

Due to the project activities, this project will fall under the guidelines set forth under USFWS Interim Policy for the Review of Highway Transportation Projects in Indiana dated May 29, 2013. No further coordination is necessary.

A review of the USFWS database did not indicate the presence of endangered bat species in or within 0.5 mile of the project area. The range-wide programmatic consultation for the Indiana Bat and Northern Long-eared Bat will be completed according to the "Using the USFWS's IPaC System for Listed Bat Consultation for INDOT Projects".



An inquiry using the USFWS Information for Planning and Consultation (IPaC) website did not indicate the presence of the federally endangered species, the Rusty Patched Bumble Bee, in or within 0.5 mile of the project area. No impact is expected.

RECOMMENDATIONS SECTION

Include recommendations from each section. If there are no recommendations, please indicate N/A:

INFRASTRUCTURE:

One (1) public airport, Portland Municipal Airport, is in the 3.8 mile search radius. Coordination with INDOT Aviation will occur.

Six (6) cemeteries are within the 0.5 mile search radius. A Cemetery Development Plan may be required since this project is within 100 feet of Green Park Cemetery. Coordination with INDOT Cultural Resources is recommended.

The nearest pipeline segment is associated with Ohio Valley Gas Corporation and is approximately 0.20 mile southwest of the project area. Coordination with the pipeline owner will occur.

Three (3) recreational facilities are within the 0.5 mile search radius. Coordination with the Portland Park Board will occur.

Five (5) railroad segments are within the 0.5 mile search radius. Coordination with Norfolk Southern will occur.

One (1) managed land is within the 0.5 mile search radius. Coordination with the Portland Park Board will occur.

WATER RESOURCES: The presence of the following water resources will require the preparation of a Waters of the US Report and coordination with INDOT ES Ecology and Waterway Permitting:

The project is within a floodplain (coordination only).

URBANIZED AREA BOUNDARY: N/A

MINING/MINERAL EXPLORATION: N/A

HAZMAT CONCERNS:

RCRA Generator/TSD: Three (3) RCRA Generators/TSDs are within the 0.5 mile search radius. The nearest RCRA Generator/TSD, Patriot Paint Manufacturing (304 Blaine Pike, AI #24908), is mapped as a point 0.11 mile west of the project area; however, it appears that the property extends to immediately west of the project area. If excavation occurs in this location, proper removal and disposal of contaminated soil and/or groundwater may be necessary.

State Cleanup Sites: One (1) state cleanup site is within the 0.5 mile search radius. Patriot Paint Manufacturing (304 Blaine Pike, AI #24908) is located adjacent to the project area. If excavation occurs in this location, proper removal and disposal of contaminated soil and/or groundwater may be necessary.



ECOLOGICAL INFORMATION: The range-wide programmatic consultation for the Indiana Bat and Northern Long-eared Bat will be completed according to “Using the USFWS’s IPaC System for Listed Bat Consultation for INDOT Projects”.

INDOT Environmental Services concurrence:

Nicole Fohey-
Breting

Digitally signed by Nicole Fohey-Breting
DN: cn=Nicole Fohey-Breting, o=INDOT,
ou=Environmental Services, HazMat,
email=NFoheyBreting@indot.in.gov,
c=US
Date: 2019.01.15 14:39:08 -05'00'

(Signature)

Prepared by:

Christian Radcliff

Environmental Analyst

Beam, Longest and Neff, LLC

Graphics:

A map for each report section with a 0.5 mile search radius buffer around all project area(s) showing all items identified as possible items of concern is attached. If there is not a section map included, please change the YES to N/A:

SITE LOCATION: YES

INFRASTRUCTURE: YES

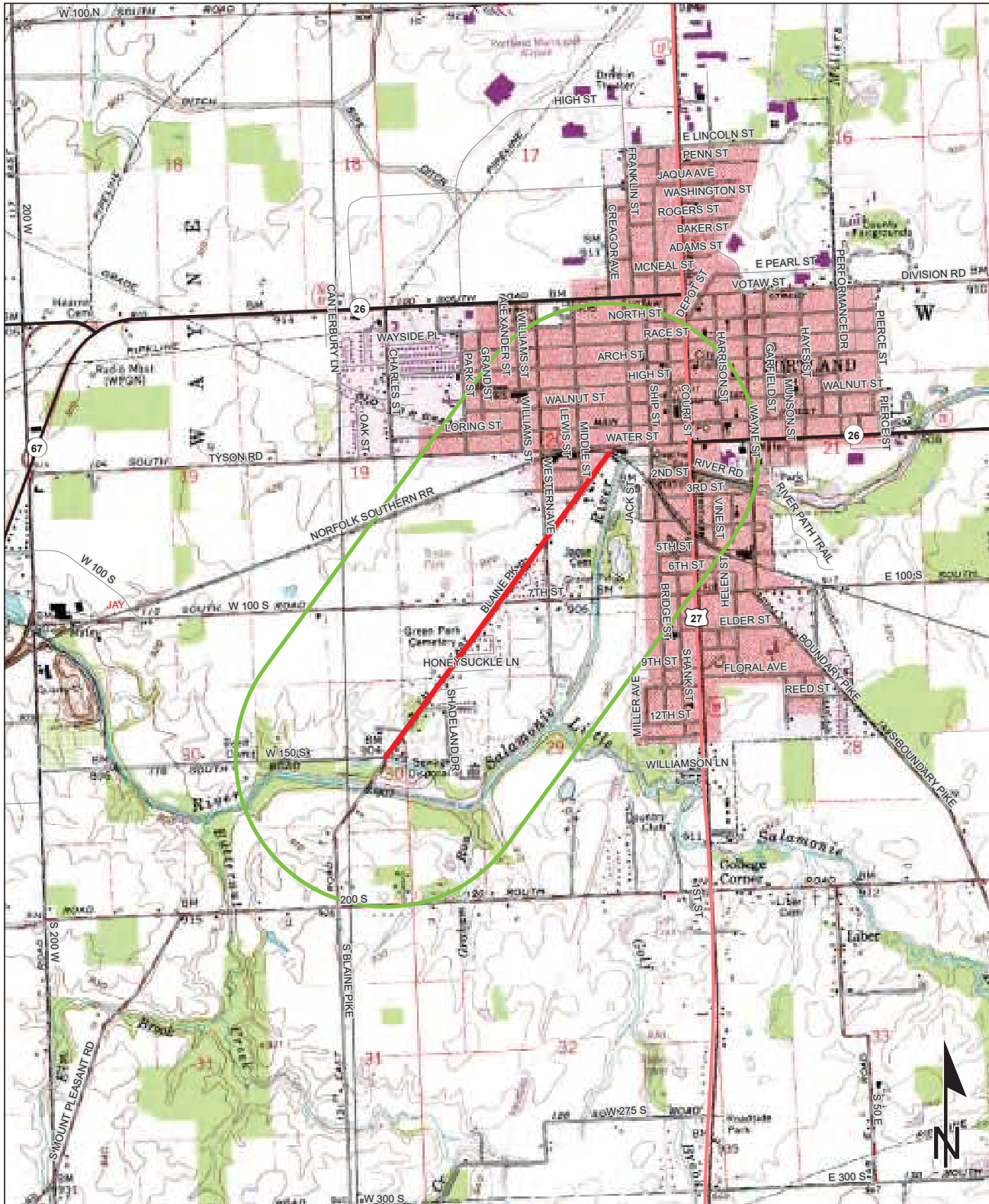
WATER RESOURCES: YES

URBANIZED AREA BOUNDARY: YES

MINING/MINERAL EXPLORATION: YES

HAZMAT CONCERNS: YES

Red Flag Investigation - Site Location
Blaine Pike
Des. No. 1600965, Road Resurfacing Project
Jay County, Indiana



Sources:
Non Orthophotography
Data - Obtained from the State of Indiana Geographical Information Office Library
Orthophotography - Obtained from Indiana Map Framework Data (www.indianamap.org)
Map Projection: UTM Zone 16 N **Map Datum:** NAD83
This map is intended to serve as an aid in graphic representation only. This information is not warranted for accuracy or other purposes.

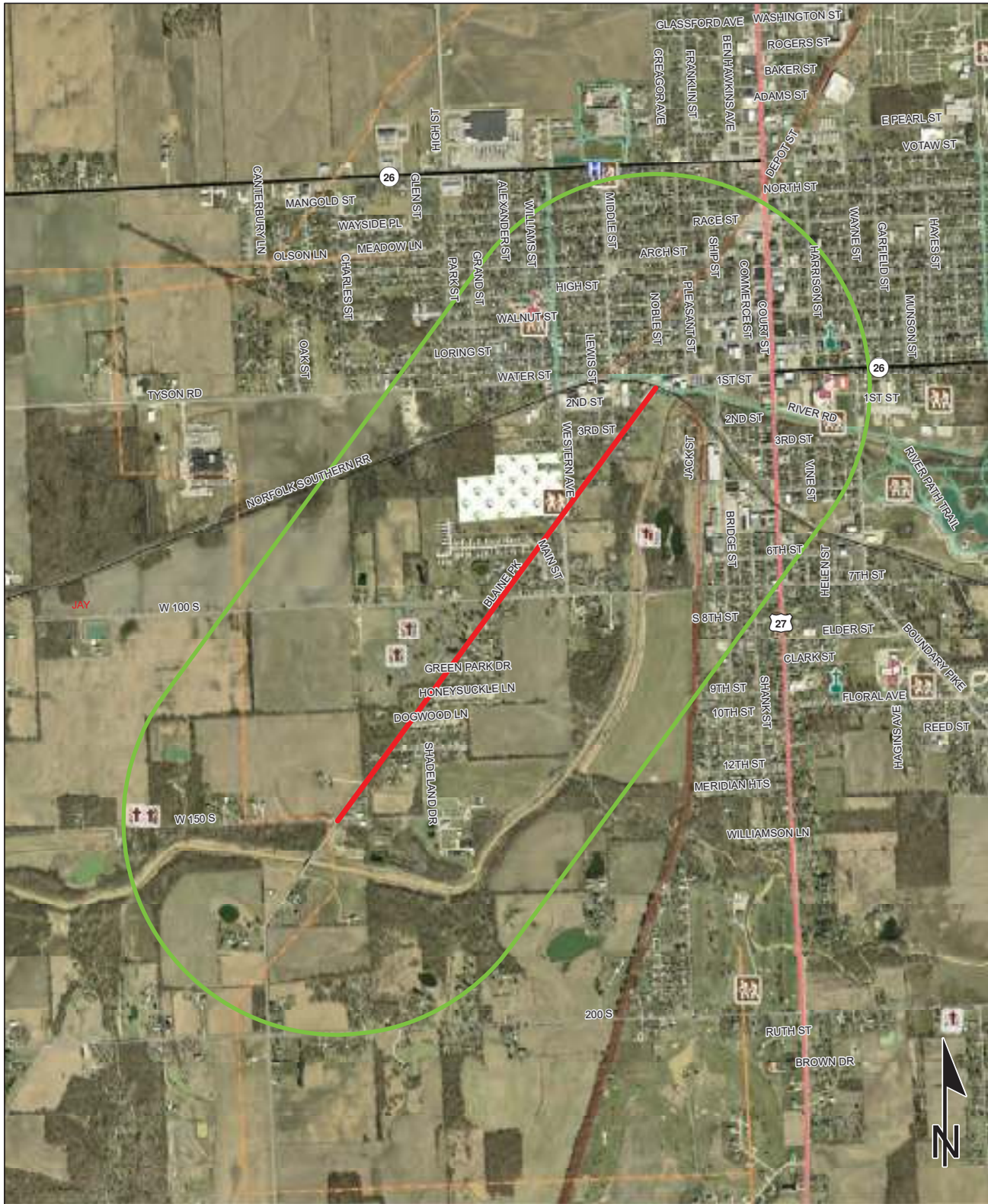
PORTLAND QUADRANGLE
INDIANA
7.5 MINUTE SERIES
(TOPOGRAPHIC)

Red Flag Investigation - Infrastructure

Blaine Pike

Des. No. 1600965, Road Resurfacing Project

Jay County, Indiana



Sources:

Non Orthophotography

Data - Obtained from the State of Indiana Geographical Information Office Library

Orthophotography - Obtained from Indiana Map Framework Data (www.indianamap.org)

Map Projection: UTM Zone 16 N **Map Datum:** NAD83

This map is intended to serve as an aid in graphic representation only. This information is not warranted for accuracy or other purposes.

0 0.125 0.25 0.5 Miles

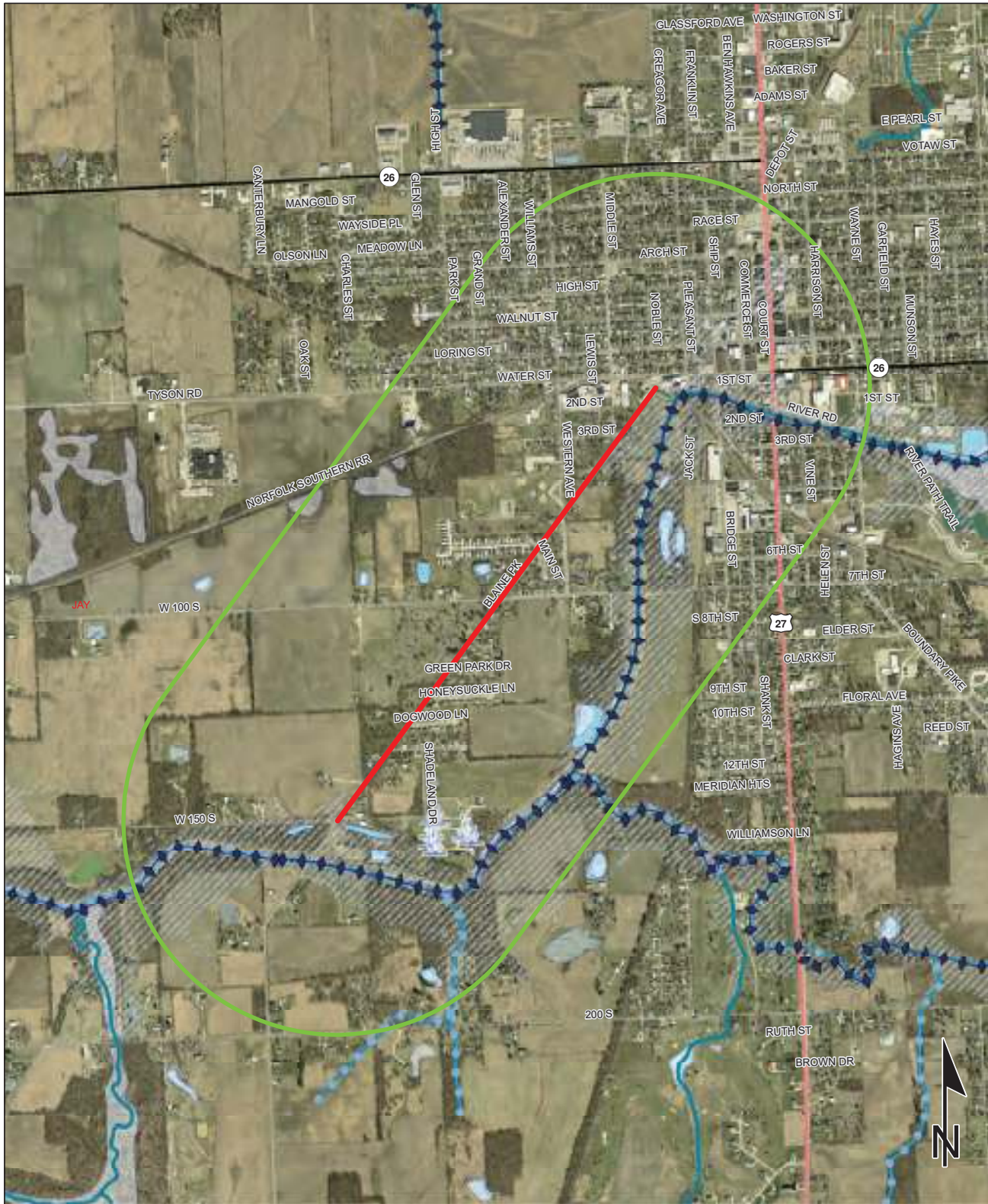
	Religious Facility		Recreation Facility		Project Area
	Airport		Pipeline		Half Mile Radius
	Cemeteries		Railroad		Toll
	Hospital		Trails		Interstate
	School		Managed Lands		State Route
			County Boundary		US Route
					Local Road

Red Flag Investigation - Water Resources

Blaine Pike

Des. No. 1600965, Road Resurfacing Project

Jay County, Indiana



Sources:

Non Orthophotography

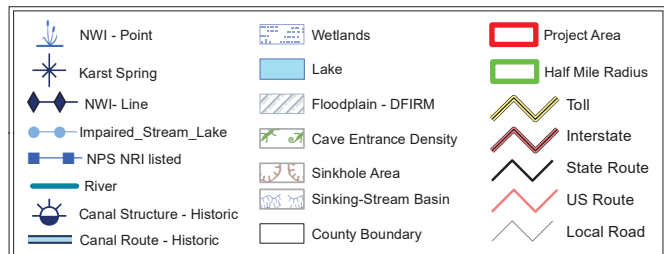
Data - Obtained from the State of Indiana Geographical Information Office Library

Orthophotography - Obtained from Indiana Map Framework Data (www.indianamap.org)

Map Projection: UTM Zone 16 N **Map Datum:** NAD83


This map is intended to serve as an aid in graphic representation only. This information is not warranted for accuracy or other purposes.

0 0.125 0.25 0.5 Miles

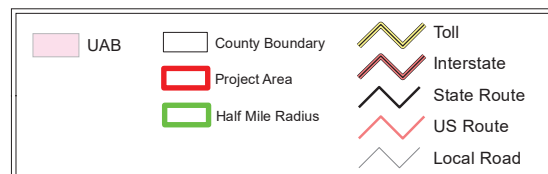


Red Flag Investigation - Urbanized Area Boundary
Blaine Pike
Des. No. 1600965, Road Resurfacing Project
Jay County, Indiana

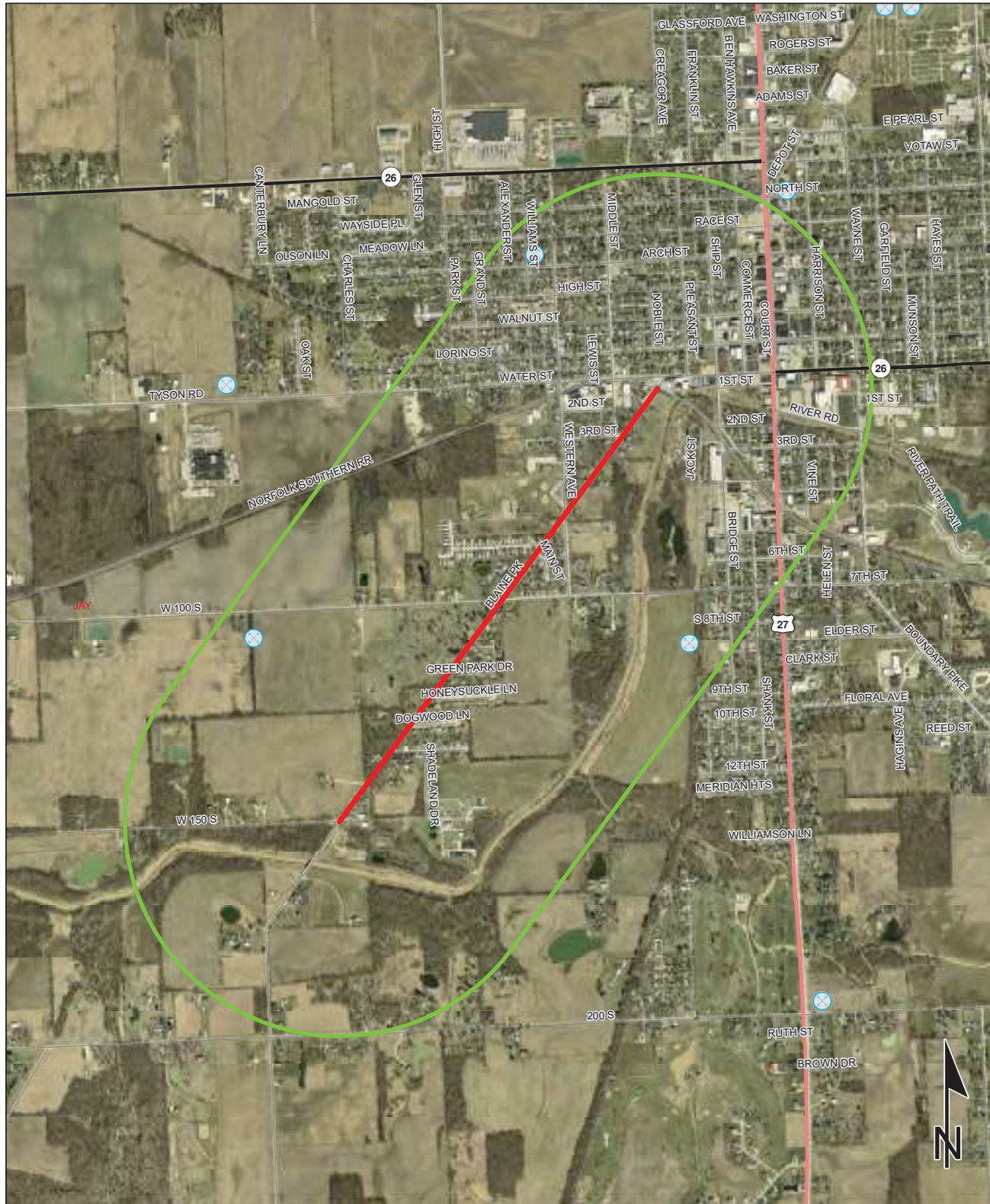



Sources:  Miles
Non Orthophotography
Data - Obtained from the State of Indiana Geographical Information Office Library
Orthophotography - Obtained from Indiana Map Framework Data (www.indianamap.org)
Map Projection: UTM Zone 16 N **Map Datum:** NAD83

This map is intended to serve as an aid in graphic representation only. This information is not warranted for accuracy or other purposes.



Red Flag Investigation - Mining/Mineral Exploration
Blaine Pike
Des. No. 1600965, Road Resurfacing Project
Jay County, Indiana



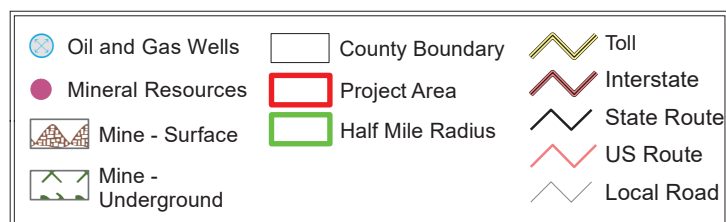
Sources:  Miles

Non Orthophotography
Data - Obtained from the State of Indiana Geographical Information Office Library

Orthophotography - Obtained from Indiana Map Framework Data (www.indianamap.org)

Map Projection: UTM Zone 16 N **Map Datum:** NAD83

This map is intended to serve as an aid in graphic representation only. This information is not warranted for accuracy or other purposes.

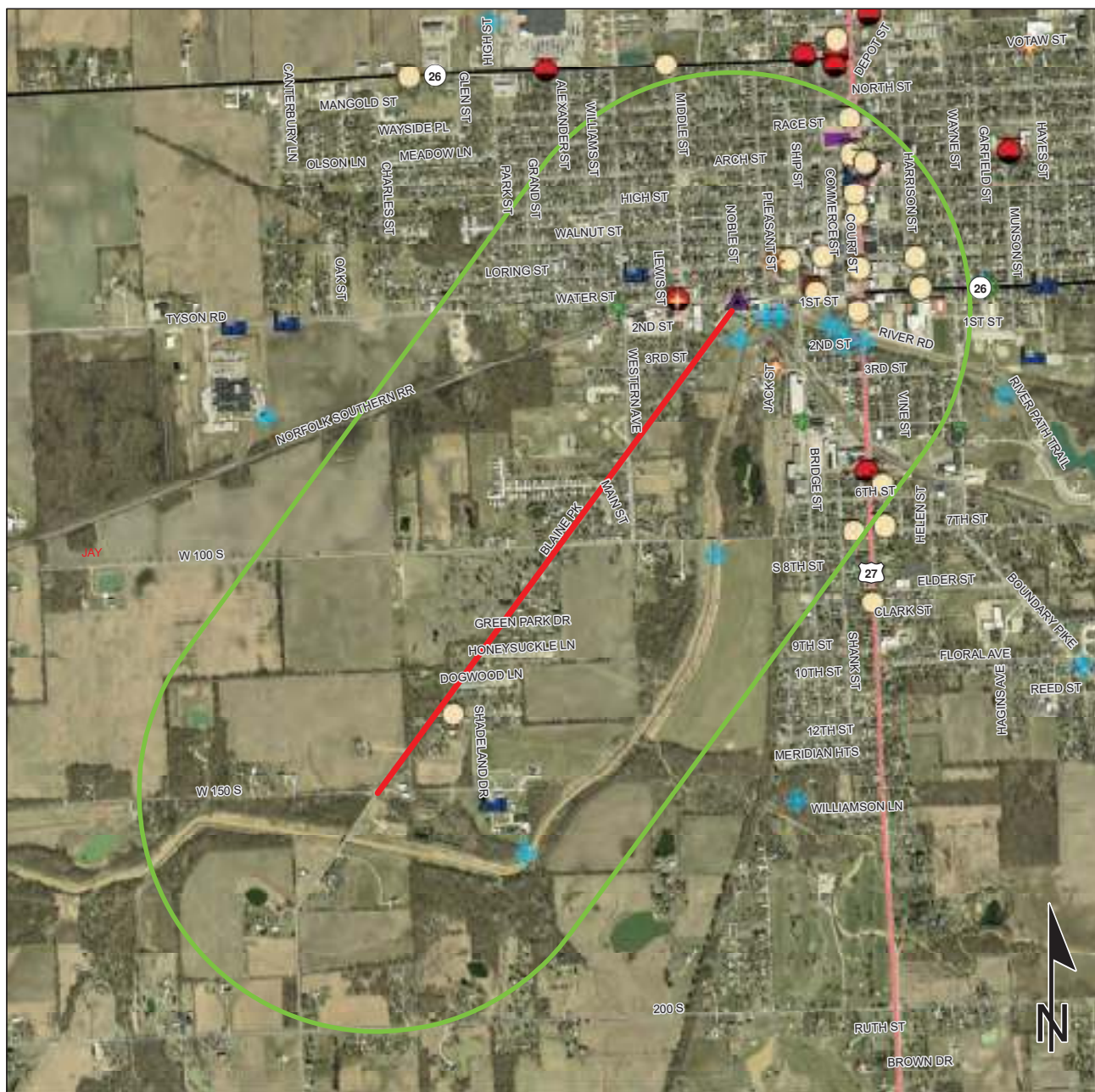


Red Flag Investigation - Hazardous Material Concerns

Blaine Pike

Des. No. 1600965, Road Resurfacing Project

Jay County, Indiana



	Brownfield		RCRA Generator/TSD		Institutional Controls
	RCRA Corrective Action Sites		Restricted Waste Site		County Boundary
	Confined Feeding Operation		Septage Waste Site		Project Area
	Notice_of_Contamination		Solid Waste Landfill		Half Mile Radius
	Construction/Demolition Site		State Cleanup Site		Toll
	Infectious/Medical Waste Site		Superfund		Interstate
	Leaking Underground Storage Tank		Tire Waste Site		State Route
	Manufactured Gas Plant		Underground Storage Tank		US Route
	NPDES Facilities		Voluntary Remediation Program		Local Road
	NPDES Pipe Locations		Waste Transfer Station		
	Open Dump Waste Site				

0 0.125 0.25 0.5 Miles

This map is intended to serve as an aid in graphic representation only. This information is not warranted for accuracy or other purposes.

Sources:
Non Orthophotography
Data - Obtained from the State of Indiana Geographical Information Office Library
Orthophotography - Obtained from Indiana Map Framework Data (www.indianamap.org)
Map Projection: UTM Zone 16 N
Map Datum: NAD83

Indiana County Endangered, Threatened and Rare Species List

County: Jay

Species Name	Common Name	FED	STATE	GRANK	SRANK
Mollusk: Bivalvia (Mussels)					
Epioblasma triquetra	Snuffbox	LE	SE	G3	S1
Pleurobema clava	Clubshell	LE	SE	G1G2	S1
Ptychobranhus fasciolaris	Kidneyshell		SSC	G4G5	S2
Toxolasma lividus	Purple Lilliput	C	SSC	G3Q	S2
Insect: Odonata (Dragonflies & Damselflies)					
Enallagma divagans	Turquoise Bluet		SR	G5	S3
Macromia wabashensis	Wabash River Cruiser		SE	G1G3Q	S1
Amphibian					
Lithobates pipiens	Northern Leopard Frog		SSC	G5	S2
Reptile					
Clonophis kirtlandii	Kirtland's Snake	C	SE	G2	S2
Thamnophis proximus proximus	Western Ribbon Snake		SSC	G5T5	S3
Bird					
Botaurus lentiginosus	American Bittern		SE	G5	S2B
Circus hudsonius	Northern Harrier		SE	G5	S2
Cistothorus platensis	Sedge Wren		SE	G5	S3B
Haliaeetus leucocephalus	Bald Eagle		SSC	G5	S2
Ixobrychus exilis	Least Bittern		SE	G5	S3B
Nycticorax nycticorax	Black-crowned Night-heron		SE	G5	S1B
Tyto alba	Barn Owl		SE	G5	S2
Mammal					
Mustela nivalis	Least Weasel		SSC	G5	S2?
Myotis sodalis	Indiana Bat or Social Myotis	LE	SE	G2	S1
Vascular Plant					
Carex timida	Timid Sedge		SE	G2G4	S1
Coeloglossum viride var. virescens	Long-bract Green Orchis		ST	G5T5	S2
Panax quinquefolius	American Ginseng		WL	G3G4	S3
Viola pedatifida	Prairie Violet		ST	G5	S2
High Quality Natural Community					
Forest - flatwoods central till plain	Central Till Plain Flatwoods		SG	G3	S2
Forest - floodplain mesic	Mesic Floodplain Forest		SG	G3?	S1
Forest - upland dry-mesic Central Till Plain	Central Till Plain Dry-mesic			GNR	S2
	Upland Forest				
Prairie - dry-mesic	Dry-mesic Prairie		SG	G3	S2
Prairie - mesic	Mesic Prairie		SG	G2	S2
Prairie - wet	Wet Prairie		SG	G3	S1
Wetland - marsh	Marsh		SG	GU	S4

Indiana Natural Heritage Data Center
Division of Nature Preserves
Indiana Department of Natural Resources
This data is not the result of comprehensive county surveys.

Fed: LE = Endangered; LT = Threatened; C = candidate; PDL = proposed for delisting
State: SE = state endangered; ST = state threatened; SR = state rare; SSC = state species of special concern; SX = state extirpated; SG = state significant; WL = watch list
GRANK: Global Heritage Rank: G1 = critically imperiled globally; G2 = imperiled globally; G3 = rare or uncommon globally; G4 = widespread and abundant globally but with long term concerns; G5 = widespread and abundant globally; G? = unranked; GX = extinct; Q = uncertain rank; T = taxonomic subunit rank
SRANK: State Heritage Rank: S1 = critically imperiled in state; S2 = imperiled in state; S3 = rare or uncommon in state; G4 = widespread and abundant in state but with long term concern; SG = state significant; SH = historical in state; SX = state extirpated; B = breeding status; S? = unranked; SNR = unranked; SNA = nonbreeding status unranked

Appendix F:

Water Resources

WETLAND AND WATERWAY DETERMINATION BLAINE PIKE

BLAINE PIKE IMPROVEMENTS
PORTLAND, INDIANA

Prepared For:

Beam, Longest, and Neff
8126 Castleton Road
Indianapolis, IN 46250

Prepared By:

LITTLE RIVER CONSULTANTS, LLC
CLAYTON, INDIANA
Project 18-019

July 22, 2019

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1.1 Project Description	1
1.2 Methods	1
2.0 BACKGROUND INFORMATION	1
2.1 Land Use/Landscape	1
2.2 Desktop Review	1
3.0 FINDINGS	2
3.1 Wetlands	2
3.2 Waterways	2
4.0 PERMIT AND MITIGATION REQUIREMENTS	2
5.0 REFERENCES	3

EXHIBITS

Exhibit 1 – Project Vicinity
Exhibit 2 – USGS Quadrangle Map
Exhibit 3 – Wetland and Floodplain Map
Exhibit 4 – Soil Survey Map
Exhibit 5 – LiDAR Hillshade
Exhibit 6 – Current Aerial Photograph
Exhibit 7 – Historic Aerial Photographs
Exhibit 8 – Feature Locations

APPENDICES

Appendix A – Site Photographs



1.0 INTRODUCTION

1.1 Project Description

The subject project is located in central Jay County, Indiana, on the southwest side of the city of Portland. The project involves the construction of a shared use path, crosswalks, curbs, ramps, HMA overlay and preventative maintenance along Blaine Pike.

In preparation for this project, a wetland and stream determination was conducted for areas adjacent to Blaine Pike. The project vicinity is shown on Exhibit 1, and the location is shown on Exhibits 2 through 8. The project is located in S20, 29, and 30, T23N, R14E. To access the site from Indianapolis, take I-69 north to SR-67 N. Continue on SR-67N for 40 miles to CR 200S in Jay County. Turn right/east on CR 200S and continue for one mile to Blaine Pike. Turn left/north on Blaine Pike and continue for 0.5 mile. The project begins at CR 150 south and continues for 1.2 miles northeast, ending 176 feet south of Norfolk and Western railroad.

1.2 Methods

The purpose of the study was to identify and delineate wetland and waterway boundaries within the project limits. The wetland determination was based on interpretation of the technical criteria presented in the 1987 *U.S. Army Corps of Engineers Wetlands Delineation Manual* (1987 Corps Manual) and 2010 *Regional Supplement to the Corps of Engineers Wetland Delineation Manual: Midwest Region Version 2.0* (Regional Supplement). Jurisdictional wetland and stream determinations were based on policies and procedures in place prior to the 2015 Clean Water Rule which was enjoined in Indiana as of the writing of this report.

Prior to conducting field work, Little River staff reviewed the U.S. Geological Survey (USGS) topographic mapping (Exhibit 2), U.S. Fish & Wildlife Service (USFWS), National Wetlands Inventory (NWI) Map (Exhibit 3), Federal Emergency Management Agency (FEMA), Flood Insurance Rate Map (FIRM) (Exhibit 3), U.S. Department of Agriculture, Web Soil Survey (Exhibit 4), LiDAR hill shading (Exhibit 5), current aerial photography (Exhibit 6), and historical aerial photography (Exhibit 7). These resources were used to identify potential wetlands and waterways within the project limits.

Onsite data collection was conducted on July 11, 2019. The study area extended approximately 50 feet east and 20 feet west of the Blaine Pike centerline to accommodate anticipated construction limits. Additional proposed right-of-way at the intersection of Blaine Pike and Western Avenue was also included in the study area. The current condition of the project area was photo-documented. Photographs can be found in Appendix A. The location of all photo points is shown on Exhibit 8.

2.0 BACKGROUND INFORMATION

2.1 Land Use/Landscape

Land use within the project boundary is mainly suburban with medium to high-density residential, along with a cemetery and athletic fields, all with manicured and landscaped lawns within the proposed new right-of-way. Land use in the extended area to the west and south includes agricultural fields and occasional residences, with the central area of Portland to the north and east. The project takes place within the city limits of Portland. Large plots of wooded land are mostly absent on the landscape. Slopes in and near the project area are nearly flat, at 0 to 2 percent. The existing and proposed right-of-way for the project, including side ditches, are nearly all grassed with the exception of areas where construction of a new residence has left the ground bare. The Salamonie River is located east of the project site.

2.2 Desktop Review

The USGS quadrangle map (Exhibit 2) does not indicate enclosed depressions within or adjacent to the project area. The National Wetland Inventory map (Exhibit 3) shows no wetlands mapped within the project boundary. The Flood Insurance Rate Map (Exhibit 3) shows the northern end of the project is within the 1% Annual Chance Flood Hazard area of the Salamonie River. The National Hydrography Dataset (Exhibit 3) shows two potential waterways within the project area. The Soils Map (Exhibit 4) shows approximately 60% of the project area is comprised of non-hydric Blount-Glynwood, thin solum complex, which can have



up to 5% hydric inclusions. Approximately 30% of the project area is comprised of hydric Pewamo silty clay, and 10% is comprised of non-hydric Glynwood silt loam, which can have 6% hydric inclusions. Current and historic aerial photographs (Exhibits 6 and 7) do not indicate potential wet areas within or adjacent to the project limits. The initial desktop review did not indicate wetland conditions were likely within the project limits.

3.0 FINDINGS

3.1 Wetlands

The July 11, 2019 onsite inspection found no potential wetland areas. In the week prior to the onsite visit, the site had received 0.35 inches of rainfall in one event, on July 7, 2019. There are no enclosed depressions onsite. The proposed project area is dominated by lawns and landscaping, and is regularly managed and maintained. As such, the vegetation should be considered significantly disturbed but this is also the normal circumstances for the site. While onsite, no indicators of wetland hydrology or hydrophytic plant communities were observed. Further, the observed landscape and topography of the project area do not indicate the potential for wetland conditions.

3.2 Waterways

A review of National Hydrography Dataset (NHD) showed two potential waterways in the project area. The onsite visit showed that corrugated metal pipes (CMPs) were present at both crossings, however no channel or ordinary high water mark (OHWM) was observed in either case. Both CMPs and the lack of channels were photo documented. Onsite observations confirmed there were no other waterways in the proposed project area. Blaine Pike side ditches are grassed swales, with no defined bed and bank, and no OHWM.

4.0 PERMIT AND MITIGATION REQUIREMENTS

Jurisdictional "Waters of the U.S.", including navigable waterways, their tributaries, and adjacent wetlands, are protected by Sections 404 and 401 of the CWA. Impacts to federal jurisdictional wetlands and streams are regulated in the State of Indiana by the US Army Corps of Engineers (USACE) and Indiana Department of Environmental Management (IDEM). Discharges of dredged or fill material into jurisdictional waters of the United States, including non-isolated wetlands, must obtain a permit from the USACE under the provisions of Section 404 of the CWA. Impacts to these waters also require Section 401 Water Quality Certification through IDEM before a Section 404 permit can be issued by the USACE. Isolated wetlands that are not regulated by the USACE, are still regulated as "Waters of the State", and require a permit from IDEM under IC 13-18-22.

As currently planned, this project will not involve impacts to any wetlands or jurisdictional waterways, and as such, no Clean Water Act Section 404/401 permit, or State Isolated Wetland Permit, is needed.



5.0 REFERENCES

- Cowardin, L. M., V. Carter, and F. C. Golet. 1979. *Classification of Wetlands and Deep Water Habitats of the United States*. U.S. Department of the Interior, Fish and Wildlife Service. Washington D. C. FWS/OBS-79/31.
- U.S. Army Corps of Engineers (USACE), Environmental Laboratory. 1987. *Corps of Engineers Wetlands Delineation Manual, Technical Report Y-87-1*, U.S. Army Engineer Waterway Experiment Station, Vicksburg, Mississippi.
- U.S. Army Corps of Engineers (USACE), Environmental Laboratory. 2010. *Regional Supplement to the Corps of Engineers Wetland Delineation Manual: Midwest Region Version 2.0*. U.S. Army Engineer Research and Development Center, Vicksburg, Mississippi.
- United States Department of Agriculture, Natural Resources Conservation Service (USDA). Web Soil Survey.



Exhibits

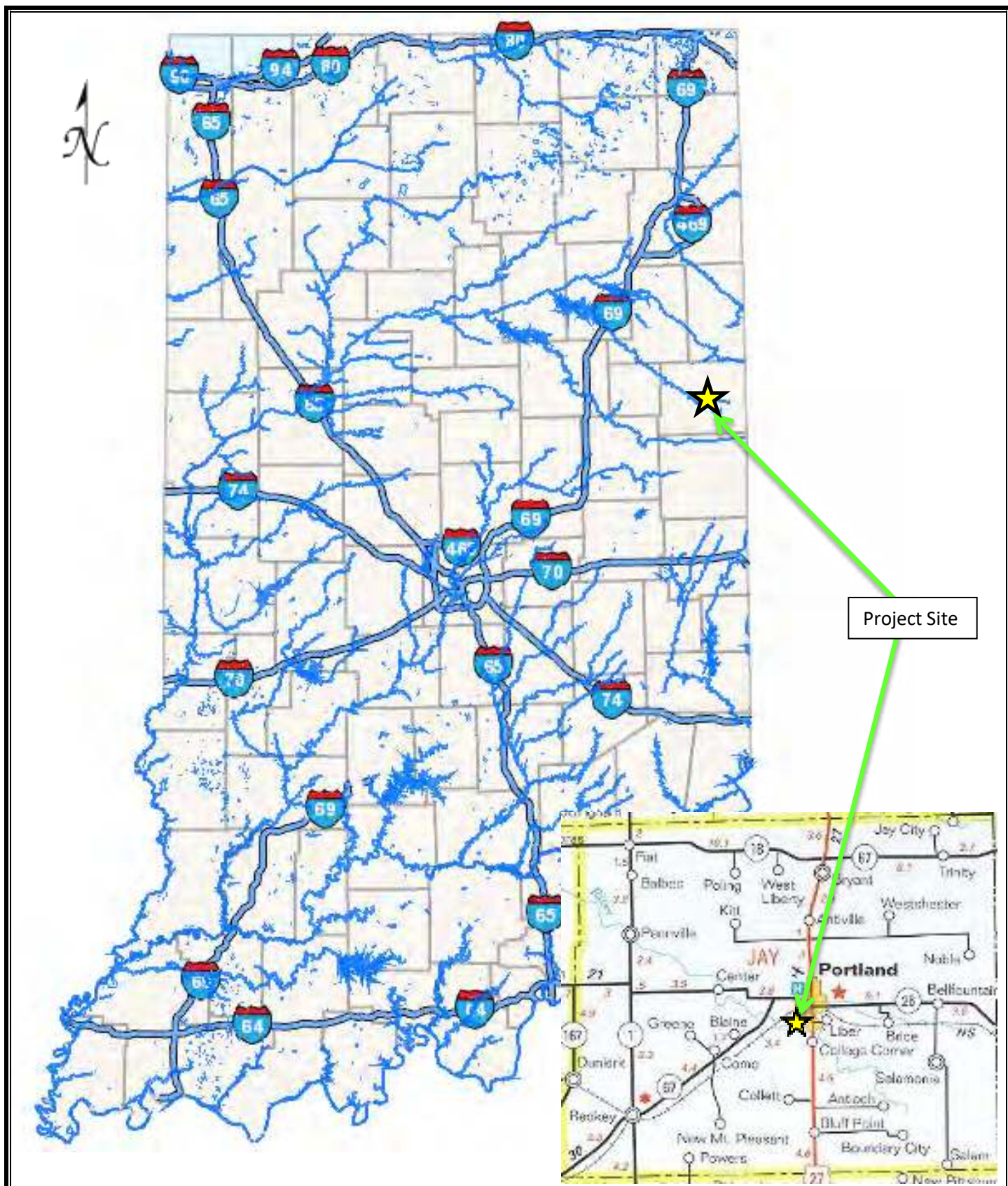


Exhibit 1 – Project Vicinity

Wetland and Waterways Determination

Created: July 1, 2019
 Source: Indiana Transportation Map
 Scale: No Scale

Improvements to Blaine Pike
 Blaine Pike, Portland, Indiana
 Project No: 18-019





Exhibit 2 – USGS Quadrangle Map Wetland and Waterways Delineation

Created: July 1, 2019
Source: IndianaMAP, USGS Portland Quad
Scale: As shown

Improvements to Blaine Pike
Blaine Pike, Portland, IN
Project No: 18-019

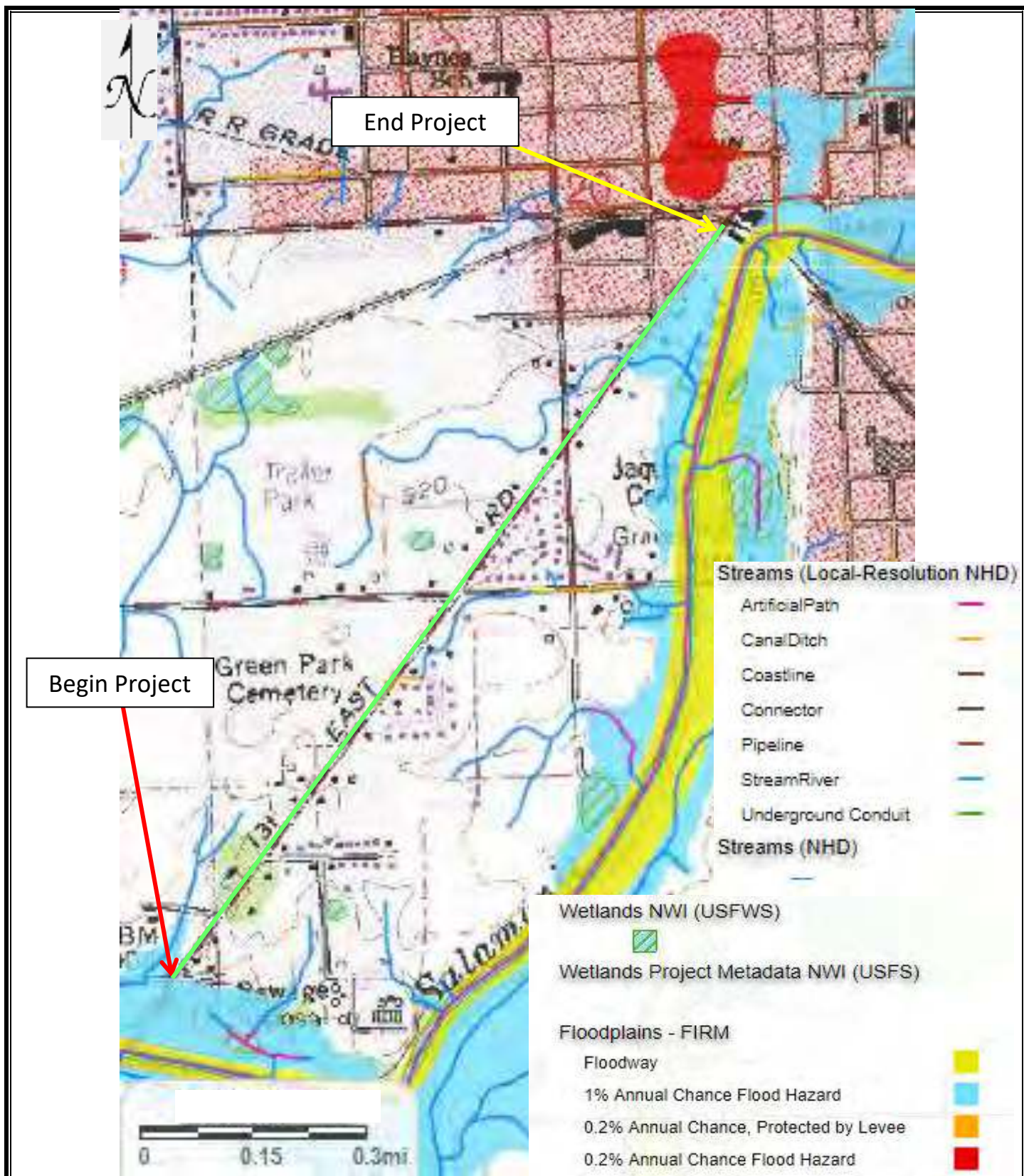


Exhibit 3 – Wetland and Floodplain Map Wetland and Waterways Delineation



Created: July 1, 2019
Source: USFWS National Wetland Inventory Map
FEMA Flood Insurance Rate Map
Scale: As Shown

Improvements to Blaine Pike
Blaine Pike, Portland, IN
Project No: 18-019

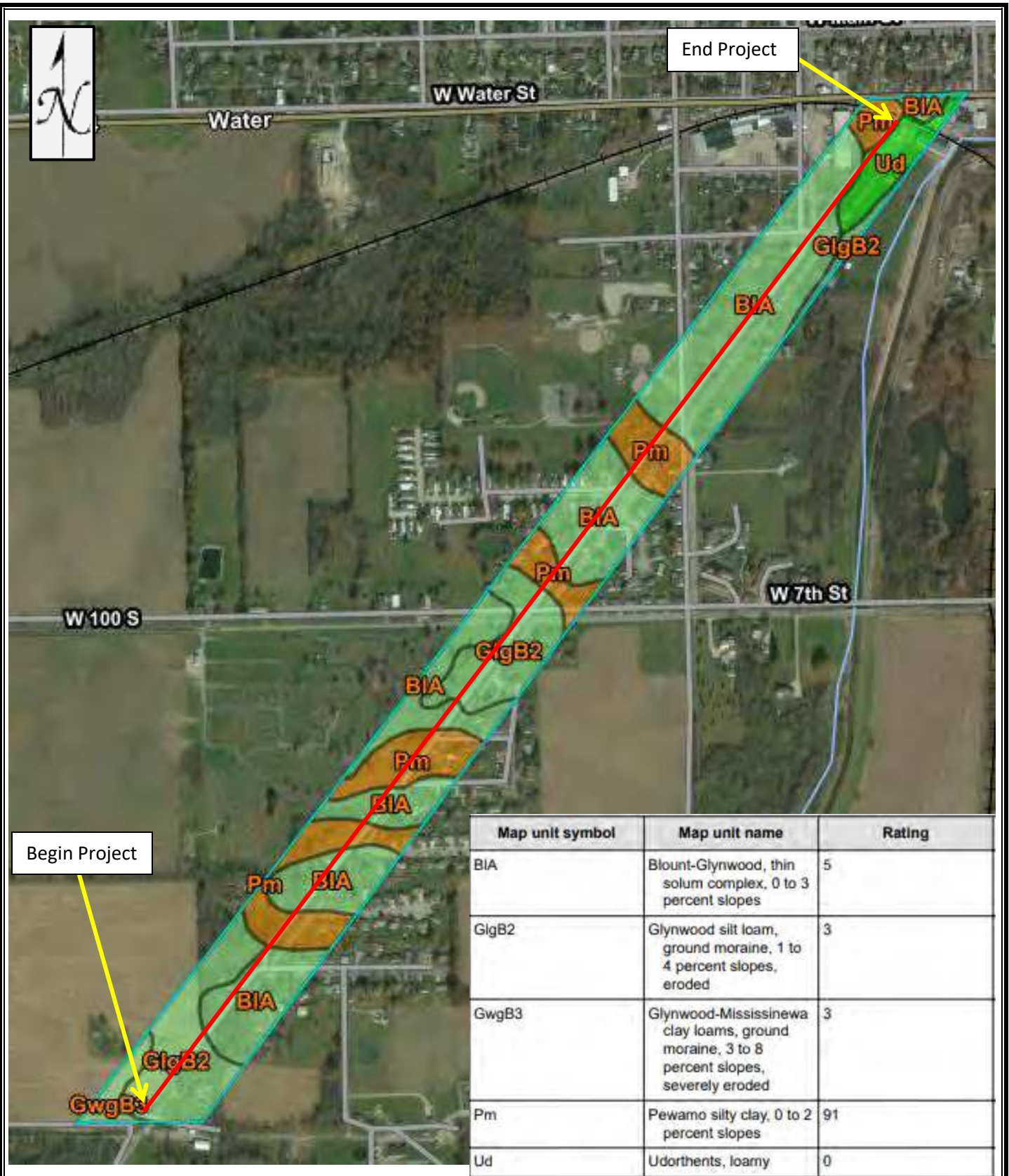


Exhibit 4 – Soil Survey

Wetland and Waterways Delineation

Created: July 1, 2019

Source: NRCS Web Soil Survey

Scale: As Shown

Improvements to Blaine Pike

Blaine Pike, Portland, IN

Project No: 18-019

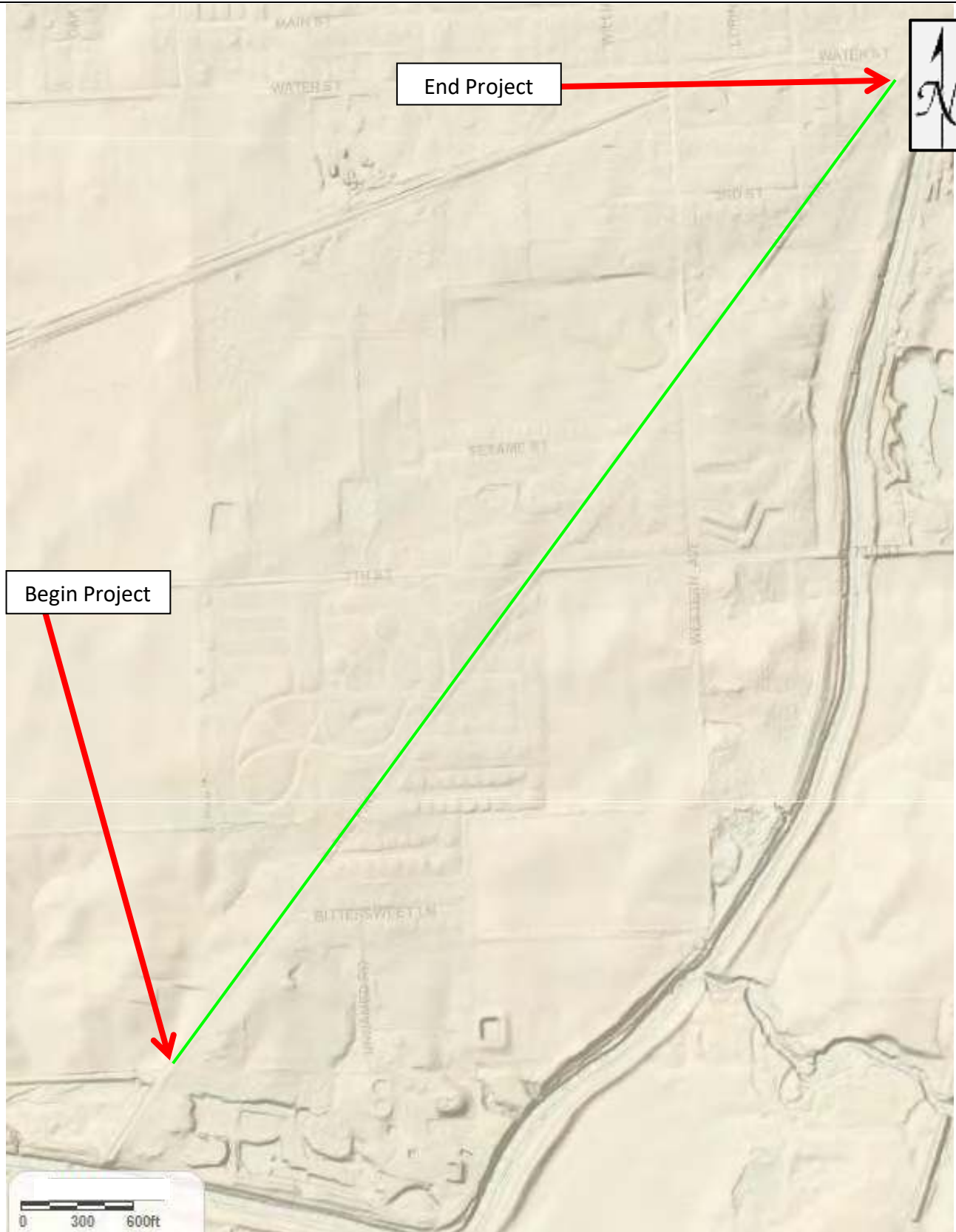
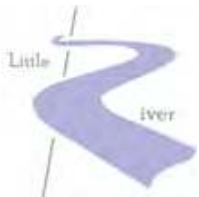


Exhibit 5 – LiDAR Color Hillshade Wetland and Waterways Delineation



Created: July 1, 2019
Source: IndianaMAP, LiDAR Color Hillshade
Scale: As Shown

Improvements to Blaine Pike
Blaine Pike, Portland, IN
Project No: 18-019



Exhibit 6 – Aerial Photograph Wetland and Waterways Delineation



Created: July 1, 2019
Source: GoogleEarth, Image Date June 2016
Scale: As Shown

Improvements to Blaine Pike
Blaine Pike, Portland, IN
Project No: 18-019

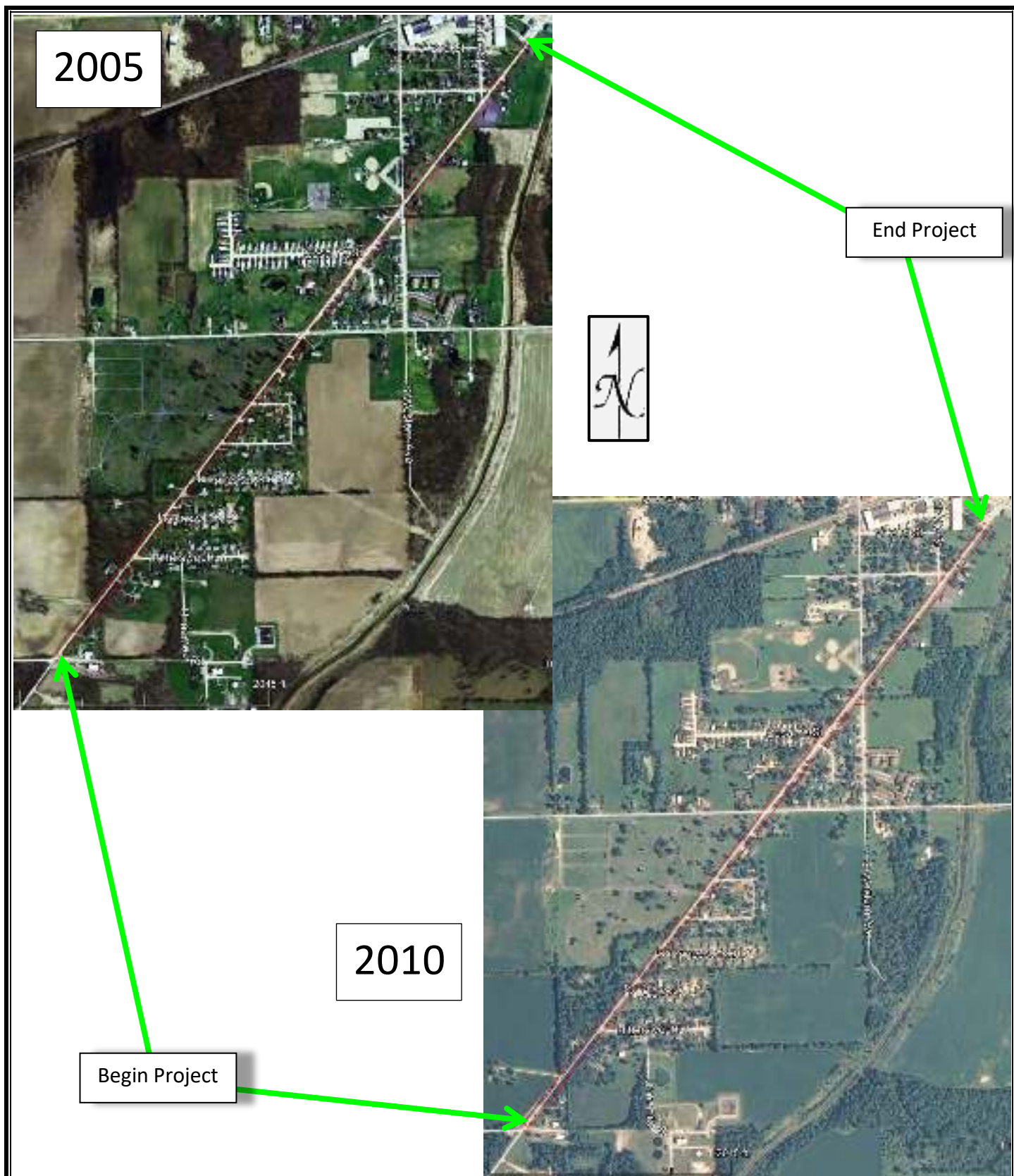
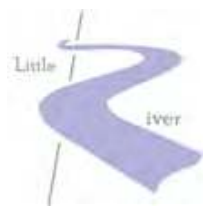


Exhibit 7 – Historic Aerial Photographs Wetland and Waterways Delineation



Created: July 1, 2019
Source: Google Earth (2005), Google Earth (2010)
Scale: As Shown

Improvements to Blaine Pike
Blaine Pike, Portland, IN
Project No: 18-019

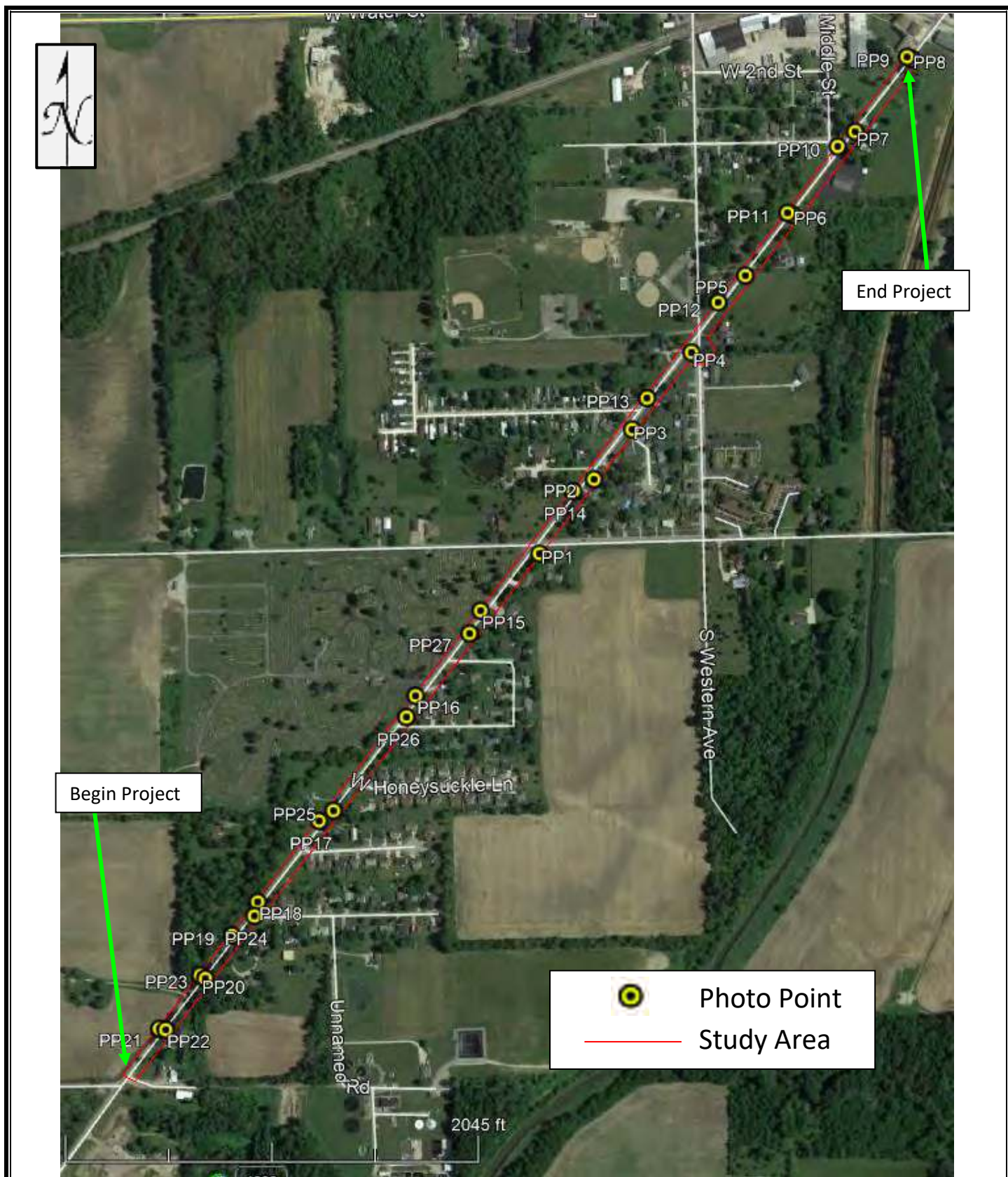


Exhibit 8 – Feature Locations Wetland and Waterways Delineation

Created: July 22, 2019

Source: GoogleEarth, Imagery Data June 2016

GPS Data: Trimble Geo XT 2008 Series

Scale: As Shown

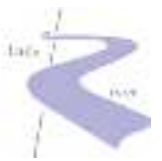
Blaine Pike Improvements

Portland, IN

Project No: 19-019

Appendix A

Photographs



PP1 – Looking NE along Blaine Pike



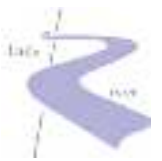
PP1 – Looking SW along Blaine Pike



PP2 – Looking NE along Blaine Pike



PP2 – Looking SW along Blaine Pike



PP3 – Looking NE along Blaine Pike



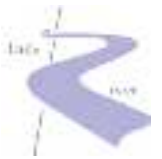
PP3 – Looking SW along Blaine Pike



PP4 – Looking NE along Blaine Pike



PP4 – Looking SW along Blaine Pike



PP5 – Looking NE along Blaine Pike



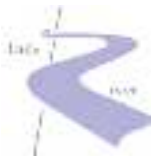
PP5 - Looking SW along Blaine Pike



PP6 - Looking NE along Blaine Pike



PP6 - Looking SW along Blaine Pike



PP7 - Looking NE along Blaine Pike



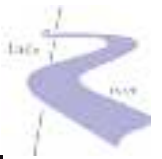
PP7 - Looking SW along Blaine Pike



PP8 - Looking NE along Blaine Pike



PP8 - Looking SW along Blaine Pike



PP9 Looking NE along Blaine Pike



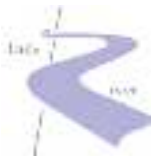
PP9 - Looking SW along Blaine Pike



PP10 - Looking NE along Blaine Pike



PP10 - Looking SW along Blaine Pike



PP11 - Looking NE along Blaine Pike



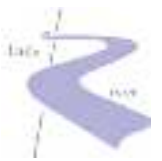
PP11 - Looking SW along Blaine Pike



PP12 - Looking NE along Blaine Pike



PP12 - Looking SW along Blaine Pike



PP13 - Looking NE along Blaine Pike



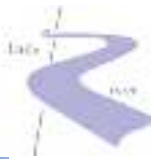
PP13 - Looking NE along Blaine Pike



PP14 - Looking NE along Blaine Pike



PP14 - Looking SW along Blaine Pike



PP15 - Looking NE along Blaine Pike



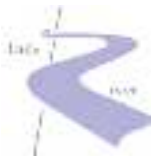
PP15 - Looking SW along Blaine Pike



PP16 - Looking NE along Blaine Pike



PP16 - Looking SW along Blaine Pike



PP17 - Looking NE along Blaine Pike



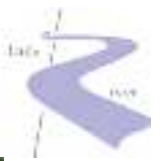
PP17 - Looking SW along Blaine Pike



PP18 - Looking NE along Blaine Pike



PP18 - Looking SW along Blaine Pike



PP19 - Looking NE along Blaine Pike



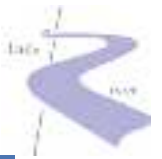
PP19 - Looking SW along Blaine Pike



PP20 - Looking NE along Blaine Pike



PP20 - Looking SW along Blaine Pike



PP21 - Looking NE along Blaine Pike



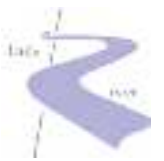
PP21 - Looking SW along Blaine Pike



PP22 - Looking NE along Blaine Pike



PP22 - Looking SW along Blaine Pike



PP23 - Looking NE along Blaine Pike



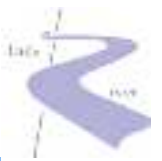
PP23 - Looking SW along Blaine Pike



PP24 - Looking NE along Blaine Pike



PP24 - Looking SW along Blaine Pike



PP25 - Looking NE along Blaine Pike



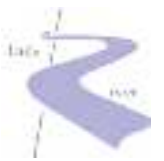
PP25 - Looking SW along Blaine Pike



PP26 - Looking NE along Blaine Pike



PP26 - Looking SW along Blaine Pike



PP27 - Looking NE along Blaine Pike



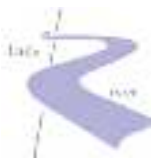
PP27 - Looking SW along Blaine Pike



PP6 – Looking at pipe and riprap on E side of Blaine Pike



PP11 – Looking at pipe and riprap on W side of Blaine Pike



PP18 – Looking at concrete structure on W side of Blaine Pike



PP18 – Looking at pipe that feeds into concrete structure

Appendix G:

Public Involvement



Example

NOTICE OF SURVEY
December 20, 2017

Michael L. & Lea Ann Schreiber
1302 Blaine Pike
Portland, IN 47371

Re: Blaine Pike Shared Use Path
and Resurfacing Project

Dear Property Owner:

Our information indicates that you own or occupy property near this proposed shared use path and resurfacing project. Our employees will be doing a survey of the project area in the near future. It may be necessary for them to come onto your property to complete this work. This is permitted by law per Indiana Code IC 8-23-7-26. They will show you their identification, if you are available, before coming onto your property. If you have sold this property, or it is occupied by someone else, please let us know the name and address of the new owner or current occupant so we can contact them about the survey.

At this stage, we generally do not know what effect, if any, our project may eventually have on your property. If it is determined that your property is involved, you will be contacted with additional information.

The survey work will include mapping the location of features such as trees, buildings, fences and drives, and obtaining ground elevations. The survey is needed for the proper planning and design of this proposed shared use path and resurfacing project. Please be assured of our sincere desire to cause you as little inconvenience as possible during this survey. If any problems do occur, please speak to our field crew or contact me at the telephone number or address shown above.

Sincerely,

BEAM, LONGEST AND NEFF, L.L.C.

A handwritten signature in blue ink, appearing to be "Michael L. Schreiber", written over a horizontal line.

Survey Department Manager

xc:

Appendix H:

Air Quality

INDIANA DEPARTMENT OF TRANSPORTATION

STATE TRANSPORTATION IMPROVEMENT PROGRAM

(STIP) FY2020 - FY2024



Indiana Department of Transportation (INDOT)
State Preservation and Local Initiated Projects FY 2020 - 2024

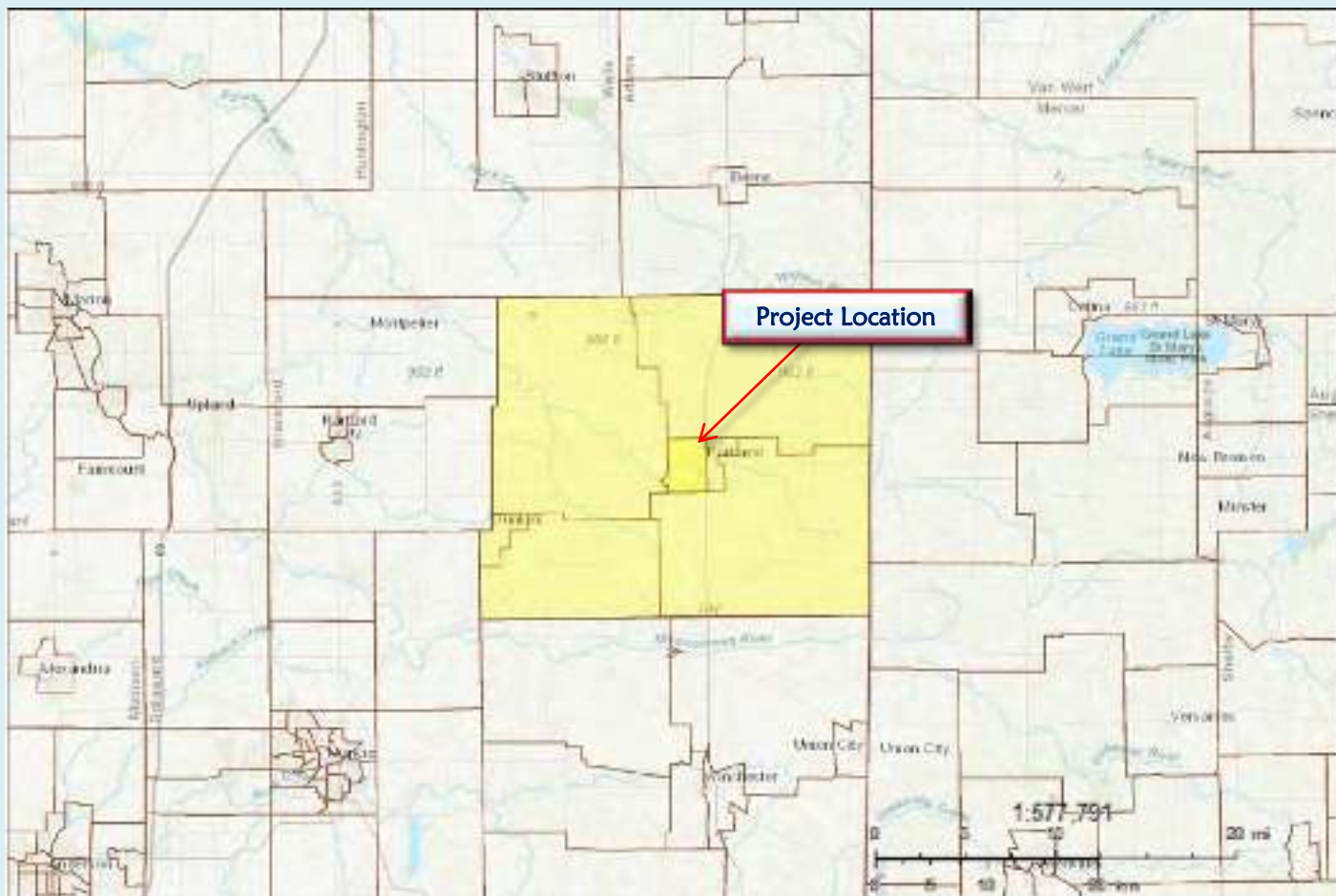
SPONSOR	CONTR ACT # / LEAD DES	STIP NAME	ROUTE	WORK TYPE	LOCATION	DISTRICT	MILES	FEDERAL CATEGORY	Estimated Cost left to Complete Project*	PROGRAM	PHASE	FEDERAL	MATCH	2020	2021	2022	2023	2024
Indiana Department of Transportation	38604 / 1401835	Init.	SR 18	Small Structure Replacement	3.49 miles E of SR 1, over Bourne-Williams Ditch.	Fort Wayne	.041	STPBG		Bridge Construction	CN	\$1,942,482.40	\$485,620.60	\$2,428,103.00				
Indiana Department of Transportation	39734 / 1600624	Init.	US 27	Vertical Sight Correction	4 miles N of SR26/SR67 (Vota w St) at CR 400N	Greenfield	.22	NHPP		Bridge Construction	CN	\$357,381.60	\$89,345.40		\$446,727.00			
										Bridge ROW	RW	\$16,000.00	\$4,000.00	\$20,000.00				
										Safety Construction	CN	\$730,054.40	\$182,513.60		\$912,568.00			
Indiana Department of Transportation	39818 / 1600828	Init.	SR 26	Truss Reconstruction Or Repair	Over Salamonie River, .78 miles E of US 27	Greenfield	0	STPBG		Bridge Construction	CN	\$1,538,696.00	\$384,674.00		\$1,923,370.00			
										Bridge ROW	RW	\$40,000.00	\$10,000.00	\$50,000.00				
Indiana Department of Transportation	39818 / 1600828	M 10	SR 26	Bridge Replacement	Over Salamonie River, .78 miles E of US 27	Greenfield	0	STBG	\$2,012,120.00	Bridge ROW	RW	\$0.00	\$0.00	(\$50,000.00)	\$50,000.00			
Comments:Moving FY 2020 ROW \$50,000 to FY 2021 ROW \$50,000																		
Portland	40318 / 1600946	Init.	IR 1015	Bike/Pedestrian Facilities	City of Portland Sidewalk Project	Greenfield	.37	STPBG		Local Funds	CN	\$0.00	\$144,400.00			\$144,400.00		
										Local Transportation Alternatives	CN	\$337,600.00	\$0.00			\$337,600.00		
Portland	40319 / 1600965	Init.	IR 1023	HMA Overlay, Preventive Maintenance	Blaine Pike Project- Water St on N to CR 150 W on S	Greenfield	1.24	STPBG		Group III Program	CN	\$1,084,000.00	\$0.00			\$1,084,000.00		
										Local Funds	RW	\$0.00	\$494,600.00	\$494,600.00				
										Local Funds	CN	\$0.00	\$531,000.00			\$531,000.00		
Portland	40319 / 1600965	M 04	IR 1023	HMA Overlay, Preventive Maintenance	Blaine Pike Project- Water St on N to CR 150 W on S	Greenfield	1.24	STBG	\$2,103,160.00	Group III Program	RW	\$395,680.00	\$0.00	\$395,680.00				
										Local Funds	RW	\$0.00	-\$402,120.00	(\$402,120.00)				
Comments:NO MPO - Please reduce RW FY 20 to 92,480 (a reduction of 402,120) and add Federal FY 20 RW 395,680.																		
Portland	40319 / 1600965	M 07	IR 1023	Road Rehabilitation (3 R/4R Standards)	Blaine Pike Project- Water St on N to CR 150 W on S	Greenfield	1.24	STBG	\$247,800.00	Group III Program	RW	\$0.00	\$0.00	(\$395,680.00)	\$395,680.00			
										Local Funds	RW	\$0.00	\$0.00	(\$98,920.00)	\$98,920.00			
Comments:NO MPO - Moving RW from FY 2020 to FY 2021 - Federal 395,680 and Local 98,920 - Work type was incorrect when project began. Changing from HMA Overlay to Road Rehabilitation (3R / 4R) -change needs made due to PO request was opened under HMA Overlay and to get any further PO requests with corrected work type need to get adjusted in STIP .																		
Redkey	40320 / 1600972	Init.	ST 1035	Bike/Pedestrian Facilities	Town Park Sidewalk in Redkey	Greenfield	.265	STPBG		Group IV Program	CN	\$89,600.00	\$0.00			\$89,600.00		

*Estimated Costs left to Complete Project column is for costs that may extend beyond the four years of a STIP. This column is not fiscally constrained and is for information purposes.

Appendix I:

Additional Studies





Source: US Census Bureau 2017



AC: Census Tract 9631

COC: Jay County

	Jay County, Indiana		Census Tract 9631, Jay County, Indiana	
	Estimate	Margin of Error	Estimate	Margin of Error
Total:	21,107	*****	4,171	+/-268
Not Hispanic or Latino:	20,470	*****	3,807	+/-246
White alone	20,072	+/-21	3,676	+/-246
Black or African American alone	70	+/-35	33	+/-29
American Indian and Alaska Native alone	29	+/-25	0	+/-11
Asian alone	111	+/-56	0	+/-11
Native Hawaiian and Other Pacific Islander alone	1	+/-3	1	+/-3
Some other race alone	0	+/-21	0	+/-11
Two or more races:	187	+/-72	97	+/-57
Two races including Some other race	0	+/-21	0	+/-11
Two races excluding Some other race, and three or more races	187	+/-72	97	+/-57
Hispanic or Latino:	637	*****	364	+/-185
White alone	512	+/-125	273	+/-217
Black or African American alone	0	+/-21	0	+/-11
American Indian and Alaska Native alone	0	+/-21	0	+/-11
Asian alone	0	+/-21	0	+/-11
Native Hawaiian and Other Pacific Islander alone	0	+/-21	0	+/-11
Some other race alone	92	+/-120	82	+/-121
Two or more races:	33	+/-45	9	+/-19
Two races including Some other race	9	+/-19	9	+/-19
Two races excluding Some other race, and three or more races	24	+/-40	0	+/-11

	Jay County, Indiana		Census Tract 9631, Jay County, Indiana	
	Estimate	Margin of Error	Estimate	Margin of Error
Total:	20,693	+/-113	3,923	+/-237
Income in the past 12 months below poverty level:	3,705	+/-459	873	+/-260

Source: US Census Bureau
2013 - 2017 American Community Survey 5 Year Estimates

Environmental Justice

Road Resurfacing and Non-motorized Path
Des No. 1600965
Blaine Pike
Jay County, Indiana

Minority by Race	COC Jay County	AC Census Tract 9631
Total Population	21107	4171
Black or African American alone	70	33
American Indian & Alaska Native alone	29	0
Asian alone	111	0
Native Hawaiian & other Pacific Islander	1	1
Some other race alone	0	0
Two or more races	187	97
Hispanic or Latino	637	364
Poverty Status		
Total Population	20693	3923
Income below poverty level	3705	873
Table: Minority and Low-Income Data (ACS, 2013-2017)		
	COC Jay County	AC Census Tract 9631
Percent Minority	4.90	11.87
125% of COC	6.13	
EJ Population of Concern		yes
Percent Low-Income	17.90	22.25
125% of COC	22.38	
EJ Population of Concern		no

Source: US Census Bureau
2013 - 2017 American Community Survey 5 Year Estimates

Environmental Justice

Road Resurfacing and Non-motorized Path
Des No. 1600965
Blaine Pike
Jay County, Indiana

August 14, 2018

Portland Water
205 South Wayne Street
Portland, IN 47371
Attn: Doug Jackson

Subject: Initial Notice of Proposed Improvement Project Des. No. 1600965

Dear Mr. Jackson;

Beam, Longest and Neff, LLC (BLN) has been assigned the task of utility coordination for the project referenced above by the Indiana Department of Transportation. In accordance with 105 IAC 13-3-1(c), this letter serves as your initial notice of the proposed improvement project Des. No. 1600965 on Blaine Pike in Jay County, Indiana.

In accordance with 105 IAC 13-3-1(c), the following information is provided. The dates listed in items (4) and (5) below are the currently scheduled dates.

- | | |
|---|--|
| (1) Name or route number: | Blaine Pike |
| (2) Geographical limits: | From County Road 150 South to South of Norfolk Southern Railroad |
| (3) General description of work: | Road Resurfacing and Multi-Use Path Construction |
| (4) Date approved work plan will be needed: | 6/25/2021 |
| (5) Ready for contracts date: | 9/7/2021 |
| (6) Name of designer and contact information: | Dan Gibson, P.E., BLN
317-849-5832 |
| (7) Major or minor project: | Major |

In accordance with 105 IAC 13-3-1(d), within 30 days after receiving the initial notice or by September 14, 2018, the utility shall respond in writing with a:

- (1) description of the type and location of its facilities within the geographical limits of the proposed improvement project; or
- (2) statement that the utility has no facilities within the geographical limits of the improvement project.

Additionally, please provide us the name, telephone number, postal address and email address of the person selected as your designated contact for this project to expedite future communications. We will contact Indiana 811 and request locates for this project prior to our survey. If you would prefer to provide us location information by some other means, please contact this office to discuss.

Please send your response to Travis Foerg, Utility Coordinator, Beam Longest and Neff, 8126 Castleton Road, Indianapolis, IN, 46250, telephone: 317-849-5832, fax: 317-841-4280, tfoerg@b-l-n.com. Thank you for your attention to these matters.

Sincerely;

BEAM, LONGEST AND NEFF, LLC



Travis Foerg
Utility Coordinator

Cc: Ami Huffman, Director of Jay County Community Development
Michelle Loveall, INDOT Project Manager
Dan Gibson, BLN Designer
File #170084-906

From: [Brian Shaw](#)
To: [Brian Shaw](#)
Subject: FW: Blaine Pike Improvement Project - Portland, Indiana
Date: Friday, July 31, 2020 4:15:58 PM

From: Tom Leonhard <streetsuperintendent@thecityofportland.net>
Sent: Tuesday, June 16, 2020 2:50 PM
To: Brian Shaw <bshaw@b-l-n.com>
Cc: Dan Gibson <DGibson@b-l-n.com>; ahuffman@jaycodev.org; Patti Smith <psmith@b-l-n.com>
Subject: Re: Blaine Pike Improvement Project - Portland, Indiana

EXTERNAL EMAIL

No, I do not at this time.

Thank You!!
Tom Leonhard
City of Portland Street Department
(260)726-4077
(260)726-5618

From: Tom Leonhard <streetsuperintendent@thecityofportland.net>
Sent: Monday, June 15, 2020 3:32 PM
To: Brian Shaw <bshaw@b-l-n.com>
Cc: Dan Gibson <DGibson@b-l-n.com>; ahuffman@jaycodev.org; Patti Smith <psmith@b-l-n.com>

On Mon, Jun 15, 2020 at 2:57 PM Brian Shaw <bshaw@b-l-n.com> wrote:

Mr. Leonard,
Good afternoon we are working on the Blaine Pike Pedestrian Improvements project in Portland Indiana. The project proposes the milling and resurfacing of Blaine Pike along the entire project corridor and construction of a 10 foot wide, multiuse trail on the east side of the roadway from Bittersweet Lane to approximately 130 feet south of the Norfolk Southern Railroad crossing. The paved shared use path will cross side roads and driveways in which the project proposes to construct crosswalks and curb ramps that meet Americans with Disabilities Act (ADA) compliance. A mid-block crossing with solar-powered flashing beacons will be constructed just north of the intersection of Blaine Pike and Western Avenue adjacent to Memorial Park. Drainage swells on the east side of Blaine Pike, will be relocated between the roadway and the shared use path.
During our evaluation of impacts for the environmental document we conducted a desktop review and site visit of the project area. The Portland Memorial Park is located along the project corridor near Western Avenue. The park does not have direct access onto Blaine Pike, but the facility is located within 100 feet of the Blaine Pike corridor. The project will not impact this resource by taking permanent right of way or restricting access. Also we found that the proposed Portland River Path is approximately 0.01 mile northeast of the project area.

We wanted to coordinate with you to see if you had any comments or concerns about the project in regard to Portland Memorial Park or the proposed Portland River Path or any other resource under jurisdiction of the Parks Department / Parks Board.

I have attached a set of plan and project sheets only and an aerial for review. please email or call if you have questions or need additional information.

Thank you

BRIAN SHAW

Environmental Services Department Manager

o: 317.849.5832

c: 317.709.3440

B-L-N.com

BEAM, LONGEST and NEFF

A Tradition of Excellence Since 1945

AGENDA PRELIMINARY FIELD CHECK

Blaine Pike Pedestrian Improvements
Des No. 1600965

DISCUSSION ITEMS

- Attendee Introductions
- Project Introduction/Brief Scope
- Anticipated Schedule
 - Stage 3 Plans, R/W Secure and Utility Certification by June 5, 2021.
 - RFC September 2021
 - Letting 11/17/21
 - Construction Spring 2022
- **Railroad Restrictions:** No construction traffic within 131 feet of tracks. A barricade will be placed to restrict construction equipment from violating this requirement. Therefore, no coordination with the railroad will be required. Otherwise, to engage the railroad requires the LPA to provide PE money (no federal funds) typically in the amount of \$15,000 just to engage the railroad. As this crossing is inactive, this is seen as unnecessary. Discussion with Katie Olsen at INDOT confirms this is the desired route.
- All utilities must adhere to the restriction or obtain the necessary permit at their cost to work within 131 feet of the closest rail.
- Geotechnical investigation also must observe the 131-foot restriction when obtaining borings
- Utility Coordination – Travis Foerg

All utilities need to provide dates we expect work plans/relocation plans, easement information and reimbursement. Need to determine due date for work plans based on letting. Currently will only need conflict analysis.

Utilities are expected to confirm their presence and if the plans correctly identify their locations.

Electric:

- American Electric Power/Indiana Michigan Power
- Jay County REMC

Fiber Optic:

- Community Fiber Optic

Telephone:

- CenturyLink

Cable:

- Comcast Cablevision

Gas:

- Ohio Valley Gas Corp.

Water:

- Portland Water

Sanitary:

- Portland Wastewater

Dismiss Utilities if they need to leave

- Geotechnical Investigations: (CTL)
 - Railroad Restrictions
 - Mill and resurface (inlay) from CR 150 S to Blaine Pike
 - Shared Use path
 - Possible need for retaining wall or curb near Property near Midblock Crossing. Do we anticipate a need for a boring at this location?
 - Midblock Crossing anticipated to be pedestal type foundation
 - Culvert extension
 - No underdrains anticipated

Dismiss Geotechnical for field review.

- Local Government
 - City of Portland
- INDOT, Greenfield District
 - Development
 - Environmental Services
 - Real Estate Services
 - Operations
 - Traffic
 - Construction
- Review any other comments



8320 CRAIG STREET | INDIANAPOLIS, IN 46250
317.848.5832 | f: 317.841.4260 | 800.362.5206 | WWW.B-L-N.COM

A TRADITION OF EXCELLENCE SINCE 1945

- Site visit, as necessary
 - Additional pictures
 - Trace drainage
- Adjourn

Expected length of meeting: 1.5 hours.

ITEMS TO TAKE

Agenda
Full-sized Plans & Cross Sections
Safety Vests
Camera
Business cards